

A FRAMEWORK FOR THE CONSERVATION OF EĞİRDİR TRAIN STATION
COMPLEX AND ITS INTEGRATION TO THE CITY

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ABSTRACT

A FRAMEWORK FOR THE CONSERVATION OF EĞİRDİR TRAIN STATION COMPLEX AND ITS INTEGRATION TO THE CITY

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Railways are one of the most important factors affecting the geography in social, cultural and economic terms. Station areas and the areas surrounding the stations, where railways come into contact with the city and the citizens, are today an important part of our industrial heritage.

The last station of the İzmir-Aydın line, which is the first railway of Anatolia, is Eğirdir Train Station Complex, and it is an important cultural asset worth investigating in this context. The complex, which was constructed and commissioned by the British in 1912, is located in a valuable area of 360.000 m². The station building maintains its complex integrity with a total of eight buildings, including a station building, cer building, warehouse building, water reservoir, guest house, office (old wagon), bunkhouse, toilet and a steel bridge.

In 2001, Eğirdir Train Station Complex was decided to cease operating by Turkish State Railways accordingly with the transportation policies of the Republic of Turkey. The complex, which was registered as immovable cultural property in 2002 but left to disappear, is one of the 25 stations included in the scope of privatization in 2009. The station, some of which was put up for sale in 2014, was returned to Turkish State

Railways by the privatization administration, together with the actions of the local people.

Most of the Station Complexes built outside the city remain in the center of the city and the land values have increased considerably. This is particularly dangerous for abandoned stations.

The main subject of this study is to carry out researches and documentation studies on the historical, social, economic and political dimensions of Eđirdir Railway Station Complex and its surroundings, which started to operate in 1912, from urban to building scale; also to develop recommendations for the conservation and reuse of the area in order to prevent harmful interference.

Keywords: Railway, Railway Heritage, Railway Station, Eđirdir Railway Station Complex, Conservation, Eđirdir

ÖZ

EĞİRDİR TREN İSTASYONU KOMPLEKSİNİN KORUNMASI VE KENT İLE ENTEGRASYONU ÜZERİNE BİR DEĞERLENDİRME

Sinan, Ali
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Demiryolları bulunduğu coğrafyayı sosyal, kültürel ve ekonomik anlamda etkileyen en önemli faktörlerden bir tanesidir. Demiryollarının kent ve kentli ile temas ettiği noktalar olan istasyon alanları ve çevresi bugün endüstri mirasımızın önemli bir parçasıdır.

Anadolu'nun ilk demiryolu olan İzmir-Aydın hattının son istasyonu Eğirdir Tren Garı Kompleksi de bu bağlamda incelenmeye değer önemli bir kültür varlığı olarak karşımıza çıkmaktadır. 1912 yılında İngilizler tarafından yapımı tamamlanıp işletmeye açılan Eğirdir Tren Garı Kompleksi yaklaşık 400 dönüm büyüklüğünde değerli bir alan içerisinde konumlanmaktadır. İstasyon binası, cer binası, hangar, su deposu, misafirhane, sundurma şefliği(eski vagon), postabaşı binası, tuvalet yapısı olmak üzere toplam sekiz yapı ve bir çelik köprü ile kompleks bütünlüğünü korumaktadır.

Eğirdir Tren Garı Kompleksinin çalıştırılmasına 2001 yılında Türkiye Cumhuriyeti Devlet Demiryolları (TCDD) ve Türkiye Cumhuriyeti ulaştırma politikaları doğrultusunda son verilmesi kararlaştırılmıştır. 2002 yılında taşınmaz kültür varlığı olarak tescil edilen ancak yok olmaya terk edilen Eğirdir Tren Garı Kompleksi, 2009 yılında özelleştirme kapsamına alınan 25 istasyondan biridir. 2014 yılında bir kısmı satışa

ıkartılan ve 600m²'lik alanı satılan gar kompleksi, yerel halkın eylemleri ile birlikte zelleřtirme idaresi başkanlıđı tarafından tekrar TCDD'ye iade edilmiřtir.

Eđirdir Tren İstasyonu Kompleksi gibi kent dıřında inřa edilen istasyon komplekslerinin ođu bugn kentin merkezinde kalmıř ve arsa deđerleri olduka ykselmiřtir. Bu durum zellikle terk edilmiř istasyonlar iin byk tehlike arz etmektedir.

Bu dođrultuda alıřmanın ana konusu; 1912 yılında faaliyete gemiř olan kompleksin, kent leđinden yapı leđine kadar tarihi, sosyal, ekonomik ve politik boyutlarını ieren arařtırmaların ve belgeleme alıřmalarının yapılmasının yanında gelecekte de uygulanma ihtimali var olan tehlikeli giriřimlere engel teřkil etmek amacıyla, alanın korunması ve yeniden kullanılması iin neriler geliřtirmektir.

Anahtar Kelimeler: Demiryolu, Demiryolu Mirası, Tren Garı, Eđirdir Tren Garı Kompleksi, Koruma, Eđirdir

To My Dear Grandmother Huriye Sinan...

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CHAPTER 1

INTRODUCTION

Railways are one of the most influential products of the Western technological development of human and social life. While the new opportunities in transportation have led to the acceleration of industrial development, it has also led to radical transformations in social relations. Therefore, the social, cultural and economic effects of railways on societies have been the subject of a wide audience. Especially the imperialist countries bringing the railroad to the countries that could not keep up with age has led most of the world to be affected by the railways. With the railroad, countries have interacted tremendously in line with their interests and profits, regardless of their development level.

While railways and rural areas entered the meta production process, the labour force needed by the industry was also an important factor in finding solutions for raw materials and market sectors. While raw materials were sent to long distances faster than other transportation methods, the products produced on the machines could be delivered to the markets quickly. With the railway, the labour force could be transferred from less developed regions to more developed regions. Thus, the workers' problem could be eliminated. Therefore, railways are the beginning of the integration process by linking different economies that exist in a wide geography. In other words, railways are one of the biggest factors in the formation of capitalism.

For the first time, the railway was used by the British to remove coal from the mine, the main source of energy in the 18th century, and to deliver it to consumers. The railway, which was started to be used in 1825, started to spread rapidly all over Europe due to its features of being speedy and economical. During this period, developing countries in the industry have become pioneers by providing a breakthrough in the

railway. The first of these countries are the United Kingdom, which started using the railway for the first time.¹

With railways, coal production increases; the needs of the railroad arise; and the demand for railways increases accordingly with the demand for iron and steel. In this respect, railways constitute an important factor in the development of the steel industry. With the expansion and cheapening of steel production, new industries such as shipbuilding, construction, and chemical industry have developed. With the development of the industry, especially the states that searched for raw materials wanted to reach the geographies where they could obtain the raw materials. They preferred railways as the easiest and most economical way of transportation management at that time.¹

In the process of industrialization, the Ottoman Empire could not be in touch with the developments in the world and got behind in terms of technological achievements. In those years, the Ottoman Empire was particularly concerned with the revolts within itself and tried to conserve its territorial integrity.

In this sense, the reason why Ottoman State used the railways is mainly to suppress the revolts and to strengthen the central government by rapidly reaching the different parts of the empire instead of developing its industry. At the same time, Abdulhamid II thought that with the increase in railways, agricultural products grown in the fertile soil of Anatolia could be brought together by other geographies and enriched. However, due to the economic and technological insufficiency of the Ottoman Empire, it could not build its railways. Therefore, it needed foreigners to build the railroad on its territory. European states wishing to benefit from this situation entered the story competition to build railroads in the Ottoman Empire in line with their interests and profits.²

¹ Özyüksel, M. (2000). *Hicaz Demiryolu*. İstanbul: Tarih Vakfı Yurt Yayınları., p.2-4

² Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p. 6

Novichev (1979), who divided the history of railway construction into two periods, calls these periods as phases of capitalism and imperialism. In the capitalism phase, railways' priorities are especially commodities and trade, whereas, in the imperialist phase, it is considered as states dominating the world.³

As we all know, transportation is very important for goods and trade. At that time, the most important points of transportation in Anatolia were harbors. It is because most of the intercontinental transport was done by ships and one of the most important ports of the Ottoman Empire starting from the 17th century was Izmir. Izmir had become an important trade center for Europeans. This situation led to an increase in the population density of Europeans in Izmir. The highest population in Izmir was the British.⁴

1061 British merchants were engaged in import and export in İzmir at that time. Raw materials and transportation are very important for trade. The British, who were aware of the fertile lands in the region, wanted to build a railway between İzmir and Aydın to benefit from these lands. In this respect, they applied for a railway concession between İzmir and Aydın. This concession, granted with the support of the British Embassy, was the first concession granted to the railways in the Ottoman Empire. "Ottoman Railway Company from İzmir to Aydın" was established in 1857 for the construction and operation of the railway. In other words, important steps were taken for a railway in Anatolia.⁵

Among the concessions granted, the concern "the constructor firm will be able to use the mines and forests free from 30 miles (48km) on both sides of the railway free of

³ Novichev, A. D. (1979). *Osmanlı İmparatorluğu'nun Yarı Sömürgeleşmesi* (N. Dinçer, trans.). Ankara: Onur Yayınları., p. 13

⁴ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 30-39

⁵ Özyüksel, Murat.(2000). *Hicaz Demiryolu*. İstanbul: Tarih Vakfı Yurt Yayınları, p. 2

charge” is one of the most important and dangerous matters. In this respect, the railroad investors in the Ottoman Empire aimed to pass the railroad route through fertile soil and mines.⁶

Beginning with the ground-breaking ceremony of Alsancak Train Station on November 16, 1858, the process was completed by reaching Aydın in 1866. However, the British wanted to extend the railway line in this fertile land. In this respect, the concession was granted for Aydın-Sarayköy in 1879 and in 1882 the railway reached Sarayköy. Another concession was granted for Sarayköy-Dinar in 1888 and the railway reached Dinar in 1889. In 1906, the last concession was granted for between Dinar-Eğirdir, and on November 1, 1912, the railway became active in Eğirdir.⁶

In the first world war, the İzmir-Aydın railway of the Ottoman Empire was confiscated by the Ottoman Empire itself and the foreign workers were expelled. In 1915, to operate İzmir-Kasaba and İzmir-Aydın railways, “Şimendifer Mektebi” was established in İzmir, which was the center of railways in Anatolia at that time. This event has an important role in the nationalization of the railways.⁶

After 1950, the transportation policy of the Republic of Turkey was to develop the highways. Therefore, the needed care had not been given to the railways. The ratio of national railway transport in the Republic of Turkey in 1948. After 1950, it decreased from 90% to 6%.⁷

The development of the highways, which started in 1950 with the establishment of the General Directorate of Highways of the Republic of Turkey and gained momentum after the 1960s, achieved a quality that pushed the railways to the second place after the 1970s. No policy was followed to increase the length of the railway line. State

⁶ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 58-180

⁷ Sönmez, Mutlu Güneş. (1997, May 5). Demir Ağlar Pas Tutuyor. *Cumhuriyet Ekonomi*, pp.8-11

appropriations for road infrastructure investments have not only been higher than for railroads but have also grown compared to the rise in oil prices.⁸

Policies implemented after 1980 in the Republic of Turkey is more focused on the commercialization of public services. Besides, reports prepared by institutions such as CANAC along with EU harmonization policies brought “Reconstruction of Turkish State Railways” to agenda. In the 2000s, Turkish Railways were discarded with a justification that it lost money, and privatization and liberalization policies were started to be established. Within the framework of these policies, in 2009, Turkish State Railways handed over to the Privatization Administration to privatize nine immovable and 25 railway stations including Eğirdir Train Station Complex. In this sense, today the Turkish railway heritage is under a serious threat⁹

The railway is a cultural heritage that has a special place in the memories of many people, has played important roles in the history of the country and has given identity to most cities. The symbol of the first war of independence for railway Turkey is one of the cornerstones of the republic. The railway is the primary symbol of the liberation war for Turkey, and one of the cornerstones of the republic. Therefore, railway heritage is an issue that needs to be developed in our country.

Unfortunately, railway heritage is not completely conserved in Turkey, and needed strategies are not developed. In the light of the reactions arising from the lack of use of the railway stations and railways, the railway heritage has come to the agenda and is supported by new researches and articles. These research and articles are very important for the development of our railway heritage. In our country, conservation of moving elements such as locomotive is in the forefront; or, the buildings are treated as singular.

⁸ Altınok, S. (2001). Türkiye’de Ulaştırma Politikaları, Karayolları ve Demiryollarının Mukayesesi. *Sosyal ve Ekonomik Araştırmalar Dergisi*, 1–2, 73–81.

⁹ TMMOB Makina Mühendisleri Odası. (2019). *Ulaşımında Demiryolu Gerçeği*. Ankara: Retrieved from https://www.mmo.org.tr/sites/default/files/gonderi_dosya_ekleri/demiryolugercegiraporu.pdf

However, Article 1 of the Venice Charter of 1964 is important to define better and perceive the railway heritage;

“The concept of a historic monument embraces not only the single architectural work but also the urban or rural setting in which is found the evidence of particular civilization, a significant development or a historical event. This applies not only to great works of art but also to more modest works of the past which have acquired cultural significance over time.”

Although it is not an article written on the railroad, it is an important step for the railway heritage which forms a whole with building groups in general. In the following years, important studies have been done for railway heritage all over the world and are still being done.

There are many train stations and complexes that still maintain their originality and integrity in Turkey. Each of these complexes constitutes our railway heritage today. However, due attention is not paid to our railway heritage. Many of our railway complexes are either abandoned and left to rot or used to destroy their values. In order for our railway heritage to reach the degree it deserves again, Turkey is obliged to develop strategies in terms of protecting this heritage by taking into account national and international declarations, specifications and laws.

Eğirdir Railway Station complex is one of the abandoned railway complexes in Turkey. The Eğirdir Railway Station complex, which had witnessed very important events since 1910 and contributed much to the country and region in social and economic cultural aspects, was closed in 2002 and left to decay. In the process, his land was torn down and some of it was sold in such a way as to damage its integrity and authenticity. Despite the entire process, the station complex stands in all its glory and retains most of its value. However, there are no conservation strategies developed on the site. Therefore, the Eğirdir railway station assembly is in danger of losing all its value in the future, as are many other stations in Turkey.

In this context, conservation strategies for Eđirdir Train Station Complex which is one of the outstanding railway heritage of Turkey should be developed with respect to the related national and international declarations, charters and laws and in consideration of understanding and knowledge revealed by organizations related to railway heritage. By this means, the conservation approach constituted for the Eđirdir Train Station Complex would be a model for other railway heritages especially for other 24 stations that were transferred to the privatization administration.

1.1. Aim and Scope

As it is authorized by the transport policies of Turkey, many railway stations are closed, and they are out of use today. Although unused railway stations were planned outside the city at the time of construction, they became central with the expansion of cities. Therefore, land values have increased considerably.

These unused areas in the center of the city are in danger today. Station complexes are described by most people as a huge void, ignoring their natural beauty and qualified units. Today, the greatest danger is the unqualified attempt to fill these areas, which people or communities who do not define these areas as cultural heritage, instead, are regarded as for vacancies.

Today, Eđirdir Train Station Complex is under the same threat. Some parts have even been sold for housing purposes in the past. Although the area maintains its integrity, there is still a danger of fragmentation. Against these threats, Complex have been chosen as the research area.

The written sources on the documentation and protection of railway and railway heritage in our country are very diverse. Many sources have been used in this thesis. It was first observed that most researchers interested in railway history were political scientists or economists. Of these, Earle (1972), Kurmuş (1982), Noviçev (1979) are publications examining railways from the framework of imperialism. Also; Atilla (2002), Can (2000), Gürel (2011) Özdemir (2001), Özyüksel (1988), Özyüksel (2000),

Yıldırım (2001) are the publications which are referenced in the thesis and which examine railway lines in Anatolia. Bozkurt (2007) is one of the valuable sources describing the history and importance of the railway in the Lake District.

Hudson (1979), Madran & Özgönül (1999), Madran & Kılınç (2008), Saner (2012), Trinder (1981) are among the important references in industrial heritage. In addition, Akyıldız (1992), Burman (1997), Cossons (1997), Coulss (1999), Nisser (1987) Railway Heritage, Kösebay & Ahunbay (2008), Koşgeroğlu (2006) Yıldız (2017) are some of the important publications that have been referenced in the issue of railway heritage.

Many doctoral and master's theses written about railway heritage in Turkey. Cihangi-roğlu (2019), one of these publications referenced in the thesis, did not focus directly on railway heritage, but it was very useful to describe Ankara Garı as a place of memory and to examine the value perception and change of this place. On the other hand, Kösebay (2007) carried out a study approaching from the upper scale by taking the architecture and conservation developed in the anatolian railway environment. Ekizoğlu (2012) and Köşgeroğlu (2005) are some of the studies on the conservation of İzmir-Aydın railway.

In line with the investigations and researches, no studies have been observed for the conservation of the Eğirdir Train Station Complex. Therefore, this study will contribute to the transfer of our important cultural heritage, Eğirdir Train Station Complex to future generations.

This thesis aims to document and analyze Eğirdir Train Station Complex from urban scale to building scale, to evaluate the values, to examine the problems of the area, to identify potentials and to develop conservation strategies against ongoing destructive initiatives.

The scope of the thesis is limited to defining the basic principles of conservation of railway heritage and while focusing on the conservation of Eğirdir Train Station Complex at different scales.

1.2. Methodology

The methodology of this thesis was developed during both field study and literature review. In this sense, the scope and methodology of the thesis have been advanced together. During the literature review, a conceptual framework was tried to be established. Considering all the factors affecting the conservation of railway heritage, the architectural features and urban relations of the railway stations come into prominence. In this context, data collection has been carried out in two stages, as literature review and field research.

Field research is mainly aimed at gathering information about buildings and open areas. The literature review begins with railways, the railroad history, and politics in the Ottoman Empire and the Republic of Turkey Period, and then continue with the urban changes in the context of station complexes to understand the content and format of the study better. Afterward, the literature review focuses on the history of Eđirdir and Eđirdir Train Station Complex. Besides, theses and academic publications on the subjects relating to the railways in the Periods of the Ottoman Empire and the Republic of Turkey, railways, station complexes were examined thoroughly.

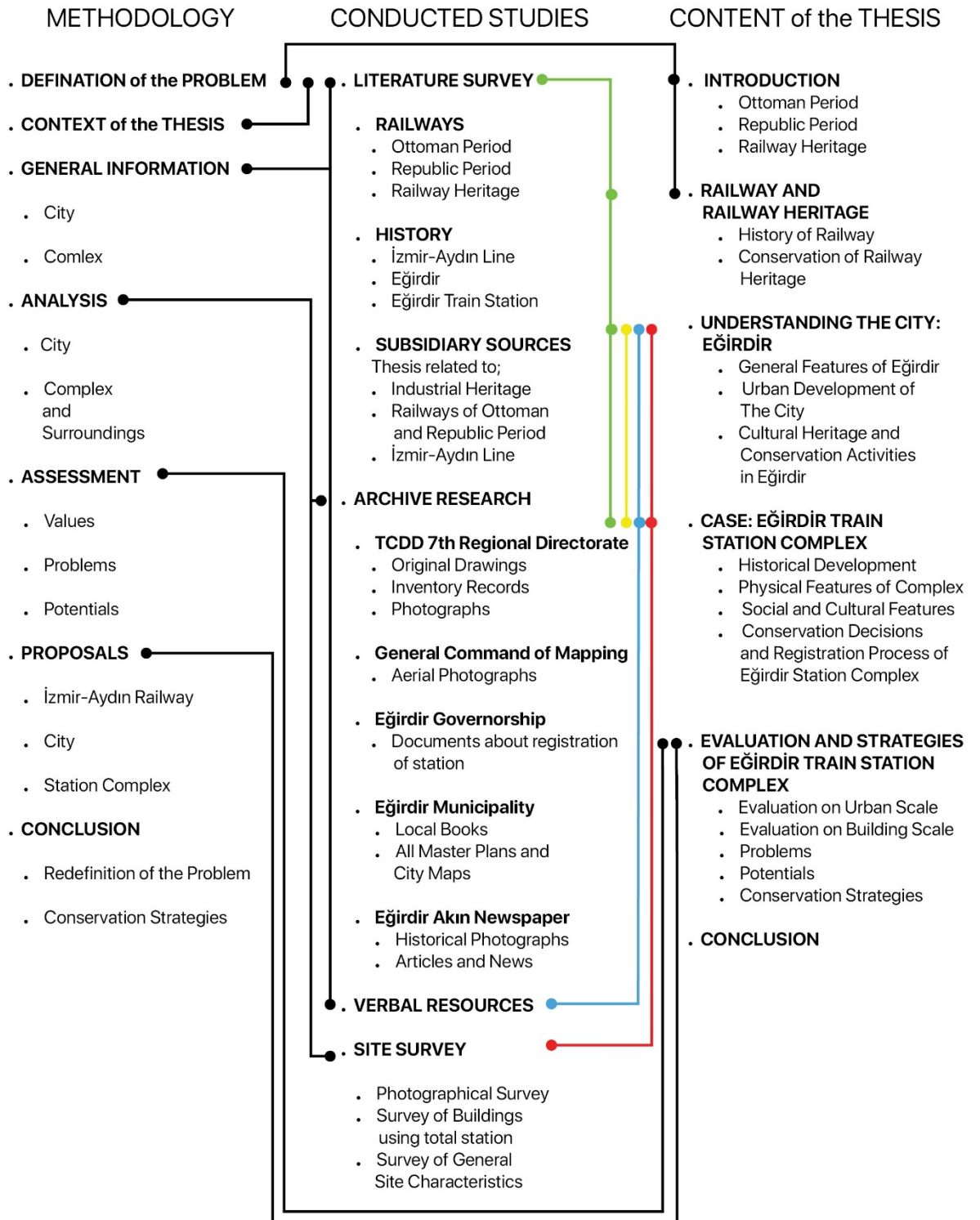
Secondly, the archive research was carried out through the examination of archives of Eđirdir District Governorate, Eđirdir Municipality, Eđirdir Akin Newspaper, TCDD (Turkey State Railways) Afyon 7th regions directorate, libraries of METU, İller Bank, Air Force Command, General Directorate of Mapping. During the archive examination, drawings and photographs of the buildings belonging to Eđirdir Train Station Complex, which are dated from 1910 to 1960, were taken from Turkey State Railways archives. Also, the current map of the city was taken from the Municipality of Eđirdir; the construction plans of urban development were referenced from the provincial bank and the archives of METU city planning department; and finally, aerial photographs were taken from the General Directorate of Mapping. In order to understand and analyze the memory of the city, the writings and photographs in the archive of Eđirdir Akin Newspaper were examined.

Thirdly, to obtain information about the situation of the area and the city during the active time of the station, communication was established with the people living in the city at that time

Finally, various researches and studies have been carried out on the building scale and the urban scale. The façades of the buildings were measured using the total station. Plan drawings were made by entering into the buildings. In the light of the information obtained from field research and archival surveys, analyzes such as land use, current status and original function, construction date, registration status, construction technique, and structural situation and architectural interventions were laid out. Moreover, previous studies conducted by various scientists and organizations within the scope of the value assessment of cultural heritage were reviewed to evaluate the values of the station and its surroundings.

Besides, in order to assess the values of the site, previous studies conducted by various scholars and organizations in the scope of value assessment of cultural heritage were reviewed.

Table 1.1: Methodology flowchart of the thesis (Sinan, 2019)



1.3. Structure of the thesis

This study is structured in five parts; the first is the introductory section, which includes the purpose and methodology of the study.

In the second part, the development of railway history and policy is first mentioned and then the history of the railways in the Republic of Turkey Period and the Ottoman Empire Period is explained. The entire process of the railways built in Anatolia as a colonial tool from the past to the present is another point that is mentioned in the second part of the thesis. In the second section, industrial and heritage was also conducted under the conservation of heritage and examples from the world and Turkey were examined.

In the third part; Eğirdir, the last stop of the İzmir-Aydın line, has been examined under its urban scale. Eğırdır is examined in detail within the scope of historical, geographical, economic, tourism, and archeological fields. In addition, the process of urban development, cultural heritage and conservation activities of the city is mentioned.

The fourth part contains information about Eğırdır Train Station Complex. The historical process of İzmir-Aydın railway and Eğırdır Train Station Complex is mentioned. In addition, the physical features of the complex are mentioned. Respectively, the access roads, the location of the station complex and its surroundings, and then the current situation of the station complex are explained. Subsequently, features on a lower scale, such as buildings, open areas, and roads, are described in detail. Analyses are in terms of construction date, registration status, construction technique, structural situation, and architectural interventions with the help of the information obtained through the field research and archival researches. Also, an overview of social structure of the complex is summarized. Lastly, conservation decisions and registration process are examined.

The fifth part of the chapter includes evaluation, examining the problems, identifying potentials of complex and strategies. Eğırdır Train Station Complex is evaluated with the help of evaluation formations defined in the literature.

Finally, in the sixth and last part of the thesis, the importance of Eđirdir Train Station Complex and the urgent need for the conservation of the area are re-emphasized as a result.

CHAPTER 2

RAILWAY AND RAILWAY HERITAGE

When humankind was in nomadic life, he carried out the act of displacement with very few things that his body could carry. Therefore, there was no need for a vehicle. Together with the settled life, people started having belongings, producing products, and even constructing their buildings and cities, and this evolution in human life has led the act of relocation to gain a different meaning. Human beings had to develop some techniques in line with these efforts and needs.

One of the most important discoveries in the history of transportation and even humanity is the wheel. The invention of the most important vehicles of our time was laid with the wheel, which was found in the years 5000 – 3000 BC. While the wheel became easier to access from one place to another, human beings were able to transport at sea with the help of muscle strength and wind. These systems, which require muscle strength, were exhausting and unsustainable, especially on long journeys, although transport on land and sea was provided in this way. However, we know that transportation has been done with muscle strength between the continents since ancient times and many places were discovered in this way.

The industrial revolution that has been influential in Europe since the second half of the 18th century; it has brought many innovations that affect human and social life. Perhaps the most important innovations for transportation is the use of the thrust of steam inland and sea vehicles. With the replacement of muscle power with steam

power, the distances have increased, especially the trade has been extended to the unreachable parts of the world and the volume of world trade has reached unimagined diameters. One of the most effective areas where steam is used is surely the railway.¹⁰

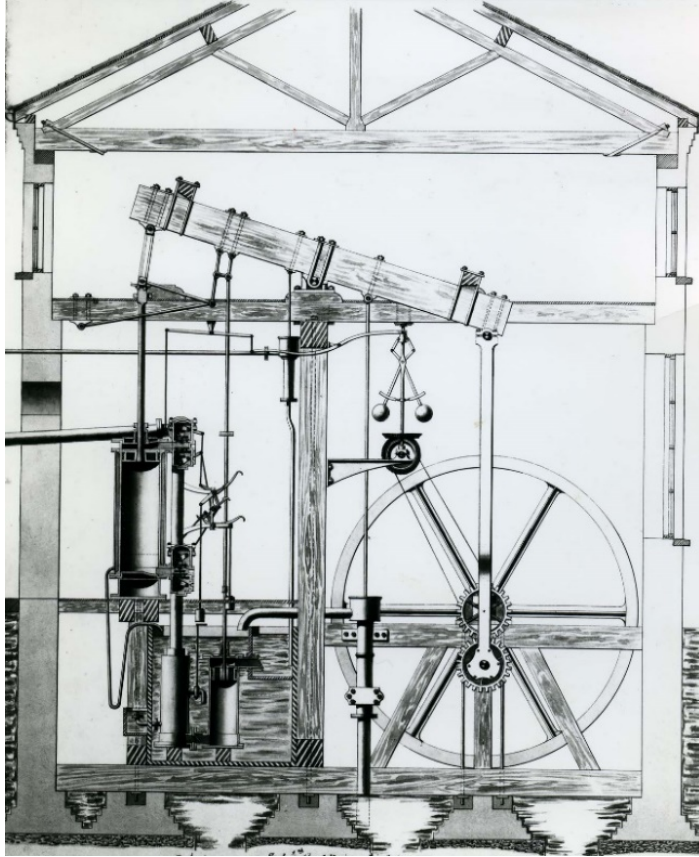


Figure 2.1: First Steam Engine Drawing. (Kingsford, 2019)

¹⁰ Earle, E. M. (1972). *Bağdat Demiryolu Savaşı* (1st ed.; K. Yargıcı, trans.). İstanbul: Milliyet Yayınları.

2.1. History of Railway

It will not be wrong to state that the history of the railway starts with the use of rails. The rails were used in European mines, and the mines were transported on railways by wagons drawn by humans or animals.¹¹

In the 18th century, iron was used by Reynold, an iron industrialist in England, to transport iron bars in mines. Reynold laid the foundations of the first railroad; whose main purpose was to cover the wooden planks on which the carriages were towed and prevent them from eroding. Easier movement of vehicles on a metal surface and the construction of steam-powered cars have contributed significantly to the development of the railway. With all these developments, 19th century. The biggest development in transportation is the use of a steam engine in land and sea vehicles. In 1803, British engineer Richard Trevithick ran a steam-powered car on iron rails. As can be expected, this tool is quite far from today's locomotive, but the basic logic is the same.

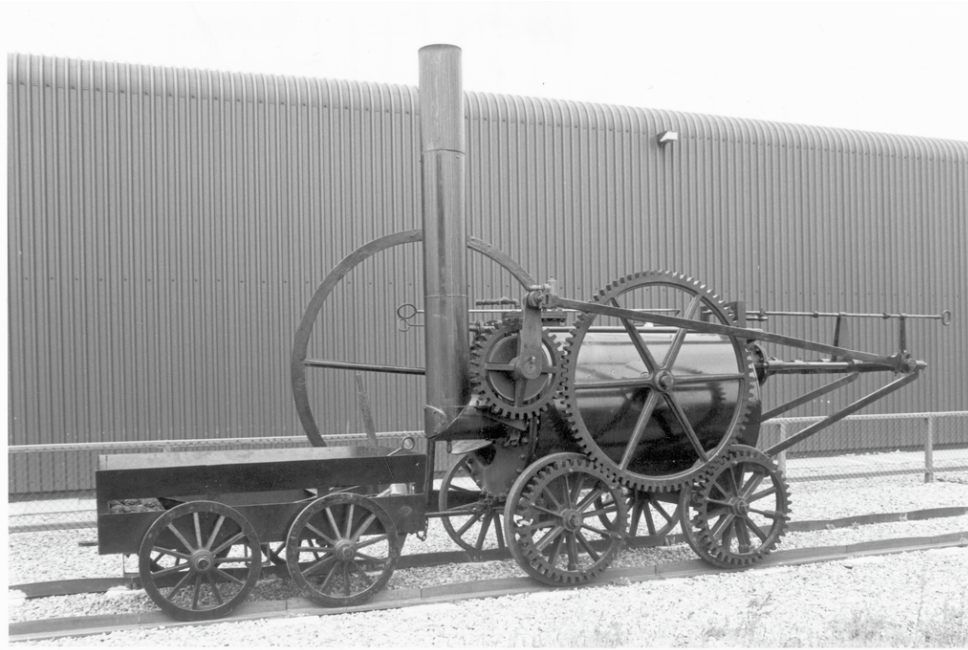


Figure 2.2: Trevithick's Locomotive - Richard Trevithick's Steam Locomotive (<https://museum.wales/articles/2008-12-15/Richard-Trevithicks-steam-locomotive/>, last visited on June 2019)

¹¹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 17

In 1814 the British engineer George Stephenson made a locomotive which could not be continuously operated and could not be very efficient but was powered by steam power. In 1823, Stephenson planned the first railway line and a 61 km long railway was built with the funds provided by a wealthy person named Edward Pease. In 1825, with the invention of tubular boilers, he developed the George Stephenson logotype. It is September 27, 1825, one of the most important days in railway history. Stockton-Darlington Company, founded by Pease, sold its first train ticket and made its first rail expedition. As of 1830, regular trains started between Liverpool and Manchester. After a certain period, this line, which initially proceeded with an average number of passengers, started to be highly demanded, and railways are now highly preferred in the world transportation system.¹²

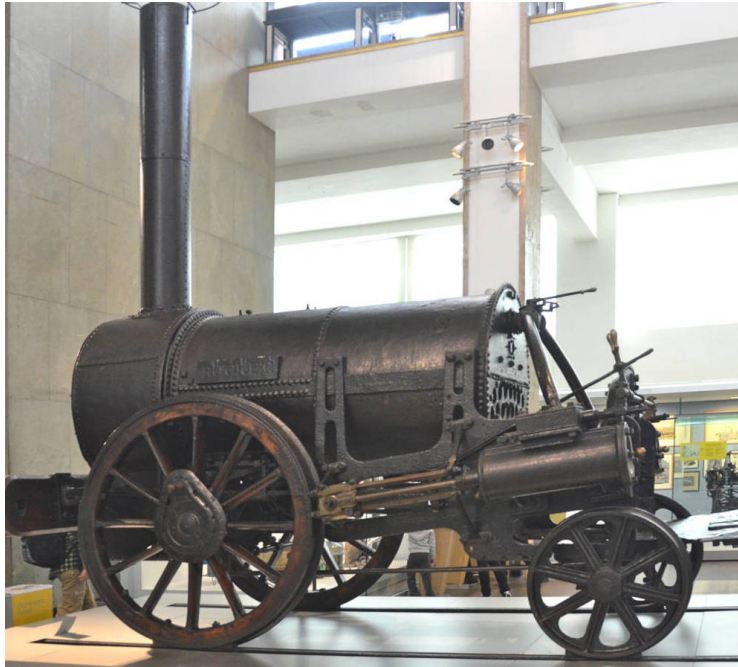


Figure 2.3: George Stephenson' locomotive - Robert Stephenson's Rocket the first modern steam locomotive. (www.victorianweb.org/technology/railways/locomotives/22.html, last visited on June 2019)

¹² Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 19

Since 1825, the railways have been built at great speed all over the world. In 1830, the first railway operation was opened in France and on August 24, 1837, the Paris-Saint-Germain Line was opened. By 1840, the railway line was laid at 1348km in England, 549 km in Germany, 497 km in France, 366 km in Belgium, 144 km in Austria-Hungary, 26km in Russia, 17 km in the Netherlands and 8 km in Italy. In 1850, railways that were developed radically in ten years were in length of 11,000 km in the UK, 6,000 km in Germany, 3,000 km in France, 2,000 km in Austria-Hungary, 176 km in Italy, 32, 27, 28 km in Denmark, Sweden and Spain respectively. When we look at the period between 1860 and 1914, there is a significant increase in railway transportation all over the world. In 1860, while there was a railway of 9,500 km length in France, this figure increased to 51,600 km in 1914. In 1860, It was 51,800 km of the railway was built across Europe and 108.000 km in the world, while in 1914, the figures increased to 390.000 km of railways throughout Europe, and 1.339.000 km in the world in total. The developments in the USA are no different from Europe. In 1851-52, railways were opened in Chile, Peru, Brazil, and Argentina.¹³

This development of the railroad has also radically affected the economic, political, social and intellectual structures of societies. In particular, railways and rural areas, which are one of the most important dynamics of the Industrial Revolution, are introduced into the process of commodity production and contribute to the solutions of labour, raw materials, and market problems. The railway is important in terms of connecting rural areas within the borders of an industrial country as well as underdeveloped countries to its market. In this sense, the railroad is one of the main factors in the formation of the capitalist system.¹⁴

As can be understood from this information, the railway had not developed at the same speed and extent everywhere. While the developments in the industrialization process which started to build a railway with the internal dynamics of their economies were

¹³ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 22-23

¹⁴ Özyüksel, M. (1988). *Anadolu ve Bağdat Demiryolları*. İstanbul: Arba Yayınları, p. 1-4

extremely rapid, the same situation did not apply to the countries that did not participate in the industrialization process, but they did not remain outside the development. Indian railways, for example, were built by the colonialist states as part of the imperialist order. The main goal here is to use India's natural resources. Apart from India, England had also invested in rail investments in countries such as Belgium, Denmark, Austria, and Argentina. Railway investments were both profitable and supported by political authorities. At the same time, it brought the most guaranteed economic infiltration opportunities to an underdeveloped country, as well as ensuring that the region through which the lines pass became a sphere of influence by the relevant country.¹⁴

Table 2.1: Chronological Development of Railways. (Sinan, 2019)

1796	the iron rails were used
1804	first steam power was designed
1814	first steam power locomotive was designed
1814	first railway line was planned
1825	first railway ticket was sold first train service
1830	begin of the Liverpool–Manchester train service
1835	beginning of the German Railways
1837	beginning of the French Railways
1839	beginning of the Italian Railways
1844	beginning of the Denmark Railways
1847	beginning of the Sweden Railways
1848	beginning of the Spanish Railways
1851	beginning of the American Railways
1866	beginning of the Ottoman Railways

2.1.1. Development of Railways During Ottoman Empire

Towards the end of the 19th century, the political and economic influence of the Ottoman Empire in the world was diminished. The basic industry was not established. Therefore, the country couldn't use its wealth of agriculture and minerals in line with its interests. Most of the agricultural products were imported. While the free trade conditions were arranged accordingly with the 1838 Trade Agreement, 1839 Tanzimat Edicts led to the emergence of the administrative, financial, and so on reforms which required dependence on foreign trade.

The Ottoman Empire, which started to borrow heavily in the 1850s due to the wrong policies applied, declared in 1875 that only half of its foreign debts could be paid, and in 1881 the Duyun-u Umumiye Administration was established to control the repayment of debts to European creditors. Concessions began to be given to foreign states such as England and France which became colonial with the industrial revolution due to unpaid debts and political events. With these developments, foreign capital inflow into the Ottoman Empire accelerated. Foreign investments remained constant between 1860 and 1880, while they increased almost threefold between 1890 and 1914. Foreign capital has especially focused on railway investments.¹⁵

In the last years of the Ottoman Empire, it had lost a significant amount of land, and the borders of the country spreading to a very large area created problems in terms of controlling the people. For this reason, the importance of the railways in those years can be said for the Ottoman Empire, mainly for the administration, military, and strategic reasons. Abdülhamit II was aware of the fact that he could intervene quickly in the rebellions by railways and planned to increase his wealth by selling agricultural products to Europe. The Ottoman Empire, which had a completely different railway strategy with Europe, would become a semi-colonial state in the future. The imperialist states that had a say in the period were in constant struggle with each other in the

¹⁵ Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p.1-3

railway investments they would make in the Ottoman country. In the course of receiving the concessions, each country was looking after the other. When one state received a concession, the other pressured the Ottomans for the concession and they received a concession as well. The imperialist states were trying to fulfill their wishes by putting pressure on the determination of the lines and general strategies.¹⁶

The United Kingdom, the largest imperialist state of the time, had shown serious interest in the Ottoman Railways because the Ottoman Empire was a treasure trove of industrial raw materials and rich agricultural products that constituted the source of the British capital. Besides, the British would be able to provide a transportation system that could provide trade through the Ottoman lands and the most important part of British imperialism would provide land transportation to India instead of sea transportation.¹⁷

The railways were first introduced in the Ottoman Empire in the 1830s by a British officer named Chesney when Britain was on its way from the Mediterranean to India via the Persian Gulf. The journeys from Cape of Good Hope was very long and the cost was quite high. The transportation from the Red Sea to India was considered as dangerous as transportation by steamships was not very developed at that time. Accordingly, Chesney first said that India could be reached via the Euphrates by using steam vessels over Syria and Mesopotamia. However, as a result of the researches, it was decided that this route would not be safe and when Chesney introduced a railway project that started from the Mediterranean Sea to Baghdad and reached the Basra Gulf, the idea of railway construction was put forward for the first time in the Ottoman Empire¹⁸

¹⁶ Novıçev, A. D. (1979). *Osmanlı İmparatorluğu'nun Yarı Sömürgeleşmesi* (N. Dinçer, trans.). Ankara: Onur Yayınları

¹⁷ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 42-43

¹⁸ Özyüksel, M. (1988). *Anadolu ve Bağdat Demiryolları*. İstanbul: Arba Yayınları, p. 7-9

Due to the economic situation of the Ottoman Empire, there was no chance of establishing the railway in its territory. However, they were aware that railways were necessary to keep up with the developments of the age. Therefore, they found the solution in foreign capital. To convince foreign capital, the Ottoman state gave concessions to the construction of railways and gave investors the right to have a say on the raw materials, agricultural products and historical artifacts surrounding the railways. With the help of this desirable offer, foreign capital started to build railways in Anatolia.

The first line coming to Anatolia is the İzmir-Aydın line. The concession of this line made by the British was given in 1856 and construction started in 1858. After this line, the Europeans in Anatolia entered into a conflict of concession to build a railway and built different railways. While the Ottoman Empire granted concessions to foreigners, it aimed to pass the railroads through their strategic routes, but established routes were arranged according to the interests of the investing company or the states.

The railways constructed in the Ottoman Empire were established by foreign capital except for the Hejaz line. Originally built by British capital, the railways were later built with French and German capital. In 1890, 18.8% of the railway investments belonged to French, 50.6% to British, 22.4% to German and 8.2% to other countries, while in 1914, 49.6% of the investments belonged to French, 9.8% to British, 36.8% to German 3.8 to other countries.¹⁹

¹⁹ Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p. 8

The main railways built within the borders of the Ottoman Empire until the War of Independence are as follows;

1. Alexandria-Cairo Railway Line 1856
2. İzmir-Aydın Railway 1860
3. Constanta-Chernavoda (Boğazköy) Line 1860
4. İzmir-Kasaba Railway 1866
5. Varna-Ruscuk Line 1866
6. Rumeli Railways 1873
7. Anatolian-Baghdad Railway 1892
8. Hejaz Railway 1900

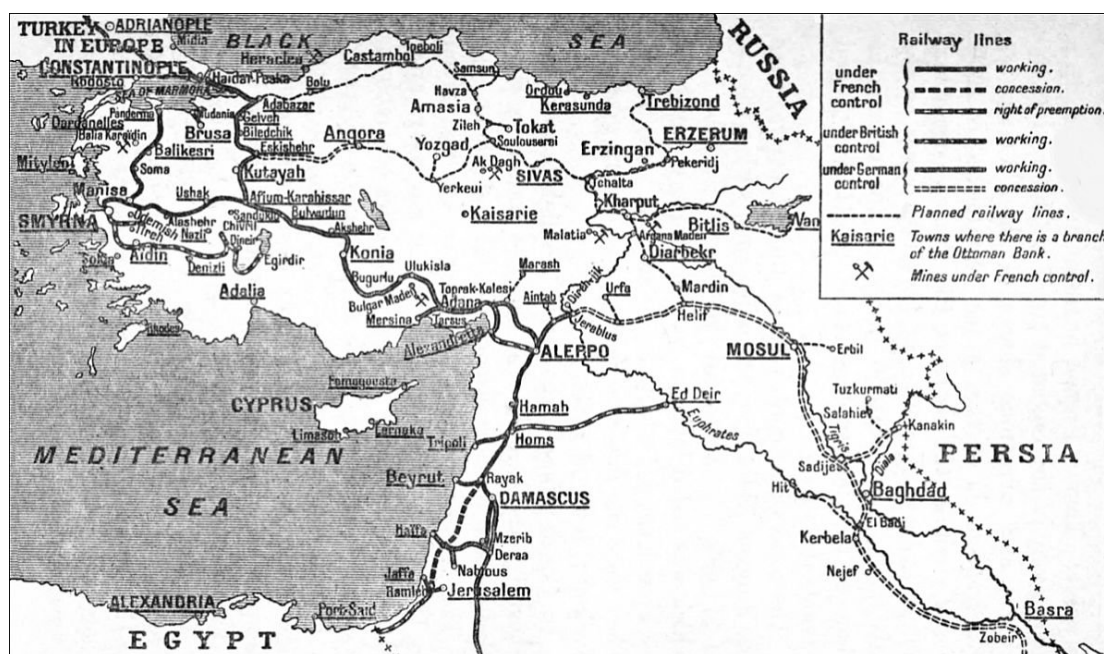


Figure 2.4: Ottoman Railway Lines
 (<http://wowturkey.com/forum/viewtopic.php?t=158112>, last visited on June 2019)

1. Alexandria-Cairo Railway Line: With the establishment of a ferry company between England-Egypt-India in 1842, the Persian Gulf railway plan was prepared by Chesney was abandoned. Although the railway project between Alexandria and Cairo, which was built by Egyptian Hidvi Abbas Pasha, which was supported by England, was a very short line, it was the first line laid in the Ottoman Empire²⁰



Figure 2.5: Alexandria-Cairo Line
(<https://nzhistory.govt.nz/media/photo/nzef-egypt-1914-16-mapp.32>, last visited on June 2019)

²⁰ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 42

2. İzmir-Aydın Railway Line: Eğirdir Train Station is the last station of this line. It has an importance in the sense that it is the first line of the Anatolian. In 1856, the construction of this line was given to the British for the concession between İzmir and Aydın. With the railroad reaching Aydın, the British demanded a second concession to extend the line. In the ongoing process, the British asked four more concessions to extend the line and the last concession was given for Dinar-Eğirdir line. In 1912, the last station Eğirdir was opened.²¹



Figure 2.6: İzmir-Aydın Railway Line

(<https://rayhaber.com/2015/01/anadoludaki-ilk-demiryolu-hatti/izmir-aydin-demiryolu-haritasi/>, last visited on June 2019)

3. Constanta-Chernovada (Boğazköy) Line: This line, which is 66 km long, is the first line of the Ottoman Empire in Europe. The construction of the line started in 1857 and was opened for use in 1860.

²¹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 89

4. İzmir-Kasaba Line: One of the most important characteristics of the line is that it is the second railway line of the western Anatolia with rich resources. In 1863, the British took over the privilege of this line, which the British and French were competing for. The first target of the line, the town railway, was completed in 1866 and reached Alaşehir in 1875. İzmir-Kasaba line which was transferred to France reached Uşak in 1887. In the 1890s, the Ottoman State opposed the merger of the İzmir-Kasaba line with the Anatolia-Baghdad line.²²



Figure 2.7: İzmir-Kasaba Railway Line (<https://upload.wikimedia.org/wikipedia/commons/d/d6/ChemineferdeSmyrne-Cassaba-etProlongements.png>, last visited on June 2019)

5. Varna-Ruse Line: This line was signed in 1859 and opened in 1866.²²

²² Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 46-107

6. Rumeli Railways: Yenikapı-Florya in 1871, Florya-Hadimköy and Sirkeci-Yenikapı in 1872, Hadimköy-Çatalca-Hudur-Edirne lines in 1873 and finally the Mandıra-Kırklareli line was completed and put into use in 1912.

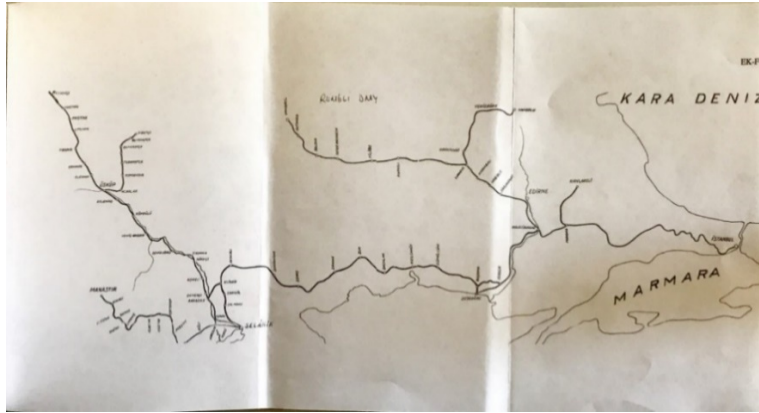


Figure 2.8: Rumelia Railways Rumelia Railways (Özdemir, 2001)

7. Anatolian-Baghdad Railway: The process of building a railway within the borders of the Ottoman Empire starts with the Anatolian-Baghdad Railway. This line, which started in 1871, reached Ankara in 1892. Afterwards, the Germans completed the Eskişehir-Konya railroad in 1899 under the Anatolian Line. Konya-Yenice railway, which was included in the Baghdad Line, was completed in 1912. The Cenup Railway of Baghdad line, which started in 1912, was completed in 1917.

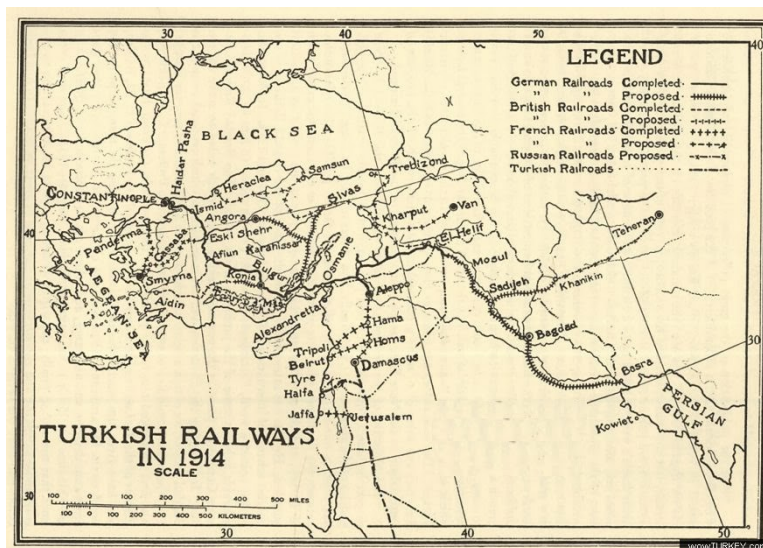


Figure 2.9: Anatolian-Baghdad Railway (http://wowturkey.com/t.php?p=tr773/Ahmed_Bayezid_Anadolu_Bagdat_Demiryolu.jpg, last visited on June 2019)

8. Hejaz Railway: The unique feature of the Hejaz Railway that makes it stand out among other roads is that it was built with the use of the resources of the Ottoman Empire. This line, which started on September 1, 1900, brought along important military, political, religious, economic and social consequences.

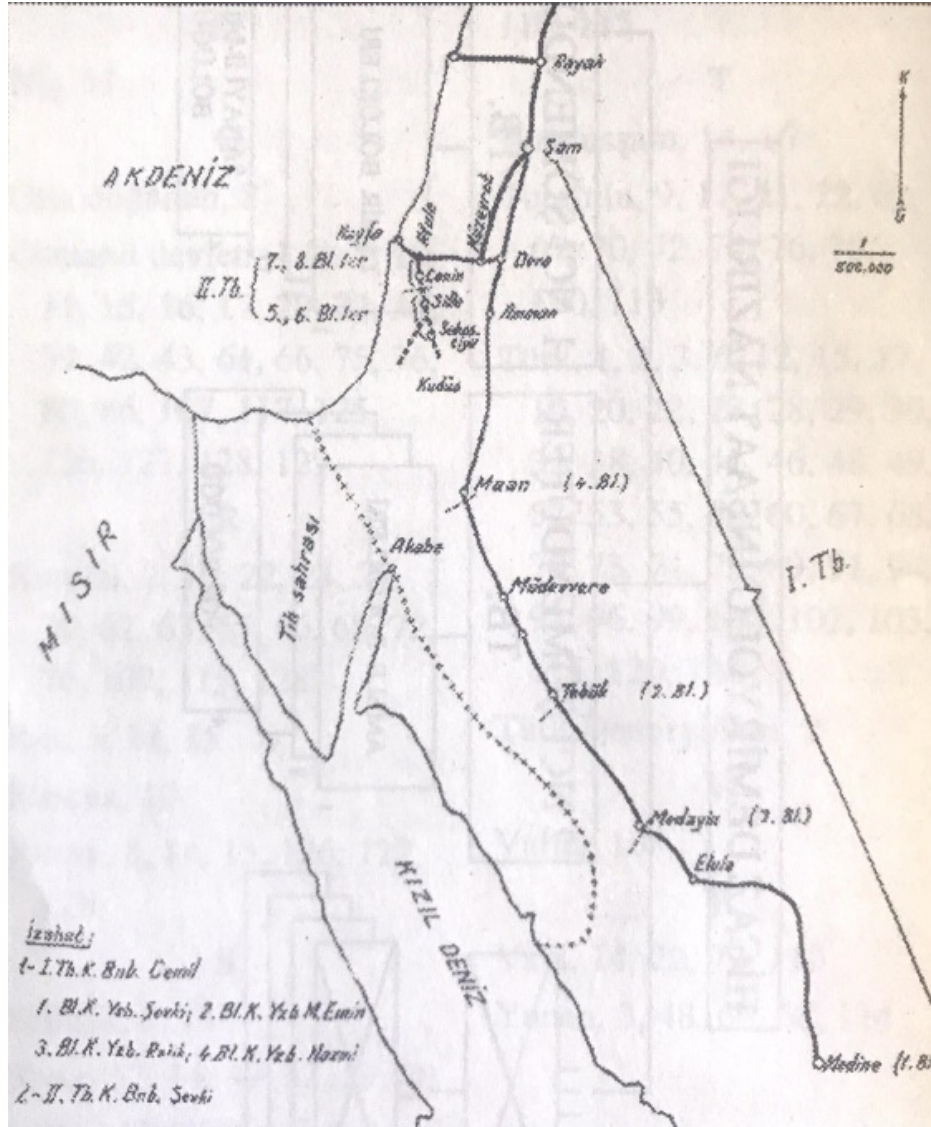


Figure 2.10: Hejaz Railways (Özdemir, 2001)

There were also efforts to recapture the remaining railways in the Ottoman Empire. In this direction, many railways were looted and occupied at that time.²⁴



Figure 2.12: Portion of Turkey by The Sevres (Bozkurt, 2007)

2.1.2. Development of Railways During Republic of Turkey

The policy of the Ottoman Empire on the railways was completely changed as the country entered the first world war. Following the outbreak of the First World War, the railroads of Aydın and Kasaba were seized by the Ottoman Empire and these railroads were decided to be bought in 1916. With the help of the school of Şimendifer that was opened in İzmir in 1915, a large number of Turkish railroaders were trained. Furthermore, during the war years, especially Western Anatolia and Baghdad railways became the main means of transportation within the country.²⁵

Considering the war of liberation, railways are both an element to be captured and an important tool to be used against the enemy for the Turks. Following the opening of the Grand National Assembly (B.M.M.), the railways outside the occupied regions were confiscated. In order to operate the seized lines, directorates were established in cities starting with Eskişehir such as Ankara and Konya accordingly with the situation

²⁵ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 158

of the war, during the national struggle, the destroyed lines were repaired as the enemy retreated, while the new lines were built by the local people according to war strategies. These included the parts of the Ankara-Sivas line, from the Azarıköy near Çay (Afyon) to the Cihanbeyli (Konya) side and the Erzurum-Sarıkamış-Borderlines.²⁶

Especially during the First World War, the lack of railway in the military area was apparent. Mustafa Kemal Atatürk expressed this deficiency in his words: “Railways are a more important safety weapon of the country than the rifle.”²⁷

In the years of national struggle, besides the military preparations, railways are the most important means of transportation for the establishment and support of the Western front. Until the foundation of the Republic of Turkey, the struggle of establishing sovereignty over the railways in Anatolia had continued between the European States and the Turks. Another struggle between the Turks and Europeans was included in the fight to obtain the operation of the railways.

The New Republic of Turkey, which emerged victorious from the war of independence, was awaiting tough economic troubles. Even if the physical war ended, the economic war had begun. Republic of Turkey founder Mustafa Kemal Atatürk's priority was to ensure peace in the world. For this reason, in 1922, the Izmir-Aydın Railway, which was seized by the Ottoman Empire during the war, was handed over to the British and the Izmir-Kasaba line was transferred to the French. There is a 4112 km railway line built by foreigners at the time of the establishment of the Republic of Turkey and the right of the Republic of Turkey is very limited.²⁸

²⁶ Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p. 23-27

²⁷ Atatürk Kültür, Dil Ve Tarih Yüksek Kurumu, & Atatürk Araştırma Merkezi. (1997). *Atatürk'ün söylev ve demeçleri 1-3*. (5th ed.). Retrieved from <https://atam.gov.tr/wp-content/uploads/S%c3%96YLEV-ORJ%c4%b0NAL.pdf>, p. 301

²⁸ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 169



Figure 2.13: Turkish Trains Ornoment During Republic Day (Atilla, 2009)

Despite all the economic difficulties since the first years of the Republic, the 19th century was marked by the importance of railways. Until the death of Atatürk, the country's rail policy was handled with a great earnest. While “one more span of the railway” is one of the main slogans, marches written like the 10th Year help to understand the country's railway policy. The main purpose of the Republic with railways was to establish an economic balance by linking production and consumption centers with each other, to ensure the development of social life as well as economic life in the regions where the railway passes, and to use the railway as one of the important propaganda tools of the new Republic.

Executives aiming at the development of all aspects of the country are taking steps to improve all transportation opportunities, especially railway, by the decisions of Izmir Economy Congress. The 1923 Public Nafia Program is one of the most important of these steps. Within the scope of the program, railways connecting to the ports and lines passing through the east-west direction were designed. Within the scope of the program, railways connected to the ports by lines passing through the east-west direction were designed. For the development of these railways, foreign capital was allowed without damaging the independence of the country as it was during the Ottoman Empire. 4112 km long railway line of the Republic of Turkey was not enough to meet their transportation needs. Therefore, increasing the length of railways and developing

in line with the interests of the country was an important point. Besides, the nationalization of the İzmir-Aydın-Denizli-Isparta-Eğirdir railway line operated by the British and the İzmir-Kasaba-Uşak-Afyon and İzmir-Manisa-Soma-Bandırma railway line operated by the French were among the most important targets. As it is understood from the speeches of Atatürk, the founder of the Republic of Turkey, most railways in Anatolia were repaired and repaired before the establishment of the Republic. Yahşihan was the first concrete product of the government's railway policy during the republican period.²⁹

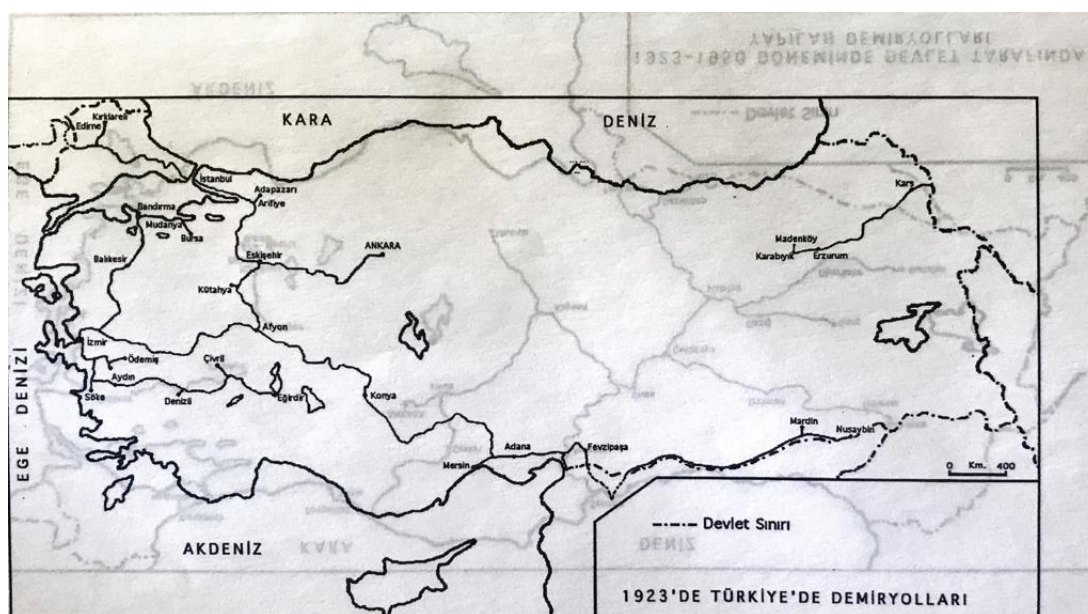


Figure 2.14: Railways in Turkey in 1923 (Yıldırım, 2001)

As it is seen in the map above, the railway İzmir-town line in the early years of the Republic was reaching from İzmir to Afyon and from Soma via the branch line to Bandırma. The İzmir-Aydın line extended from Denizli and Dinar to Eğirdir. The Anatolian line reached Ankara and Konya via Eskişehir. Starting from Konya, the Baghdad line was passing through Adana to Iraq. The Orient railways were also connecting

²⁹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 160-173

the country with Europe. There is no railway in the east of Ankara except for the Erzurum-Sarıkaş-border railway line, which is left over from the Russians. It is very important for Turkish Railways between the years 1924 and 1948. During these years, two important policies were introduced on the railways. The first was the construction of new railways to form a network structure that will meet the needs of national integrity and national economy, not in the interests of foreign states but the interests of the country, and the other was the nationalization of the railways held by foreigners.³⁰

On 23 May 1927, Turkish state railways were established by Law No. 1402. The administration of National Railways, excluding foreign Railways, was united under a single institution.³⁰

The Republic of Turkey by the provisions of the Treaty of Lausanne, the only lines available to nationalize lines as necessary to maintain its privileges given by the Ottoman Empire could do it by sales. In this case, it was very difficult compared to the economic conditions of that period, but it was one of the priorities. Between the years 1924-1930, the construction of railways in Turkey in 1929 started the new world is not quite beside the railway construction slowdown due to the economic crisis. With the effect of the depression in 1933, railway construction accelerated again.³⁰

In the meantime, the Nationalization process began after the 10th anniversary of the Republic. The İzmir-Kasaba line was purchased and nationalized in 1934 and Aydın Railway was purchased in 1935. In 1937, due to the approaching World War II, the Eastern Railways were nationalized and great importance was given to the nationalization processes.³¹

In addition to all these developments, a Turkish company was granted the right to construct a railway for the first time as a result of the tender bid opened for the Sivas-Erzurum line in 1934. Thus, it led to the beginning of another period on the railway.

³⁰ Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p. 40-140

³¹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 180

In this period, instead of the slogan “One more Span of Railway”, “Turkish knowledge, the Turkish capital, Turkish contractor and Turkish worker and railway” started to be used. Between the years 1933-1940, Turkey has experienced perhaps the fastest time in Turkish railways. Mustafa Kemal Atatürk, who thought that the desired goals had been achieved on the railways, said on 9 May 1935: “We have tied the Mediterranean Sea to the Black Sea by iron”. During the years 1940-1950, especially due to the economic burden brought by the Second World War, railway constructions were slowed down. Even though railroad construction started after the war, its old construction speed could not be reached.³²



Figure 2.15: Railways in Turkey between 1923-1950 (Yıldırım, 2001)

³² Yıldırım, İ. (2001). *Cumhuriyet Döneminde Demiryolları 1923-1950*. Ankara: Başak Matbaacılık ve Tanıtım Hiz. Ltd. Şti., p. 44

Table 2.2: Turkey's Transportation Statistics between 1950-2000. (Evren and Dündar, 2016)

Yıllar	Karayolu (%)	Demirvolu (%)	Denizvolu (%)	Havavolu (%)	Toplam (milyar ton-km)
1950	17,1	55,1	27,8	-	5,6
1960	38,0	48,0	14,0	-	9,7
1970	65,7	21,6	12,7	-	28,2
1980	73,6	10,4	15,9	0,1	49,6
1989	81,6	9,5	8,8	0,1	81,4
1995	92,5	7,1	0,2	0,2	121,7
1996	93,0	6,8	0,0	0,2	133,0
1998	94,6	5,3	0,0	0,2	161,0
1999	89,9	5,0	4,9	0,2	167,9
2000	89,9	5,5	4,4	0,2	179,7
2001	90,5	4,5	4,8	0,2	167,4
2002	91,9	4,4	3,5	0,2	164,1

(DİE Ulaştırma İstatistikleri ve Türkiye Karayolları Ulaştırma İstatistikleri 2001-2002)

After 1950, priority was given to the highway as the country's transportation policy. It is possible to understand this change through the Transport statistics of Turkish highways. In that period, the biggest investment in Western Anatolia was the Port of Izmir considering the railway investments which were slowed down. İzmir Port, whose foundation was laid with great enthusiasm in 1955, has an important place for our country.³³ Unfortunately, no other major investment has been seen outside the port of Izmir.

³³ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 195



Figure 2.16: Railways in Turkey in 1950 (Yıldırım, 2001)

As a result, to summarize the Railways of the Republic period, the average 40% of our total railway lines were built before the Republic and 37% were built between 1923 and 1950. In the last period, in the 1950s, the transportation policy of Turkey gave importance to highways. For this reason, the railway has not been invested except for a few high-speed rail projects.³⁴

³⁴ TMMOB Makina Mühendisleri Odası. (2019). *Ulaşımında Demiryolu Gerçeği*. Ankara: Retrieved from https://www.mmo.org.tr/sites/default/files/gonderi_dosya_ekleri/demiryolugercegiraporu.pdf, p.8

2.2. Conservation of Railway Heritage

From 1950 to the present day with many lines of railways and stations in Turkey, emphasizing the many highways were abandoned to their fate. Although almost all of these stations are cultural assets worth conserving, these stations have not been given the necessary consideration due to various reasons.

Today, Railway Heritage is referred to along with concepts such as industrial archaeology and industrial heritage. The buildings of the historical railways that survived to the present day are the biggest representatives of the industrial heritage and it is more accurate to examine them under a sub-title under the name of railway heritage. In this respect, it would be right to go over the industrial heritage first in order to understand the railway heritage.³⁵

2.2.1. Railway Heritage

In the aftermath of the Second World War, in the face of the rapid construction and rapidly developing technology that started with the aim of eliminating the destruction caused by the war, the industrial buildings that were inadequate started to be demolished. In the leading industrialized countries such as England, France and Germany, which started and developed the industrial revolution, the conservation of these buildings as they were the symbols of the development of the industry came to the fore and firstly a concept called “Industrial Archeology” was started to be discussed in England.³⁶

Research, exploration and recording of old industrial buildings have been called “industrial archeology”, while the buildings uncovered has been described as “industrial monuments” In the case that the conserved entity is not a singular industrial building

³⁵ Kösebay Erkan, Y., & Ahunbay, Z. (2008). Anadolu Demiryolu mirası ve korunması. *İTÜ Dergisi A: Mimarlık, Planlama, Tasarım*, 7(2), 14–25, p. 16

³⁶ Ekizoğlu, G. (2012). *Demiryolu Yerleşkelerinin Endüstriyel Miras Olarak Korunma Sorunları: İzmir- Aydın Hattı Üzerindeki Demiryolu Yerleşkeleri Örneği* (Msc Thesis). The Graduate School of Natural and Applied Sciences of Dokuz Eylül University, p.10

but all of the buildings and facilities collected in a given area, the area is referred to as a whole and defined as “industrial site”. “Industrial heritage” concept came to the forefront with the international approach to the conservation of old industrial buildings, and the idea that industrial monuments and sites are actually components of an international heritage as well as national heritage has been adopted in this process.³⁷

The first known conservation approach for the industry was conducted by the author L.T.C. Rolt as a personal response in 1940 and it was on the canals in England and later on railways. By the 1960s, the mill, mine, quarry, iron casting workshop, such as old industrial facilities were considered under the scope of conservation.³⁸

The concept of industrial archaeology was first introduced by Donald Dudley in the 1950s as a topic that could be discussed and studied in academic circles, but in an article written by Michael Rix, although it was not precisely defined, the subject was stated as the factories, locomotives, steel buildings and bridges constructed in XVIII. and XIX. centuries.³⁹

After The Second World War, to eliminate the destruction caused by the war, the industrial buildings that were inadequate in the face of rapid construction and developing technology started to be demolished. Especially the instructional buildings of the XIX. Century that started to disappear in the 60’s have begun to attract public attention.³⁹

Inventories for industrial archeology, committees and industrial monuments began to be established within England, one of the pioneers of the industrial revolution from 1960 to 1970. Telling people why industrial buildings that do not have a good place

³⁷ Saner, M. (2012). Endüstri Mirası: Kavramlar, Kurumlar ve Türkiye’deki Yaklaşımlar. *Planlama*, 2012/1-2, 53–66, p.53

³⁸ Trinder, B. 1981. “Industrial Archaeology in Britain”, *Archaeology*, vol. 34, no. 1, s. 8-16, p.10-12

³⁹ Hudson, K. (1979). *World industrial archaeology*. Cambridge: Cambridge University Press, p.1

in people's memory should be conserved is one of the biggest challenges of the period.⁴⁰

In 1973, with the FICCIM-First Industrial Congress, the Industrial Heritage was once again moved to the international level in the UK. In 1975 the SICCIM Second International Conference was held in Germany on The Conservation of Industrial Monuments. In 1978- the third Conference on The Conservation of Industrial Heritage was held in Sweden. With the use of the name of this conference as an “industrial heritage” instead of “industrial monuments”, it has been considered that industrial buildings have been considered within the concept of heritage, and this area has reached a wider scope.⁴¹

The biggest development on industrial heritage after Ticcih in the 80s was the work of the Council of Europe. In the meetings held in 1985 and 1987, it was aimed to determine the industrial heritage status in Europe and determine the policies to be followed. In addition, on 13 September 1990, the Council of Europe issued Recommendation No. R (90) 20, titled Recommendation on the Conservation of the Industrial and Technical and Civil Engineering Heritage in Europe.⁴²

By the 2000s, TICCIH continues to keep its industrial heritage on the agenda in the international order. With the cooperation agreement signed between TICCIH and ICOMOS in 2000, TICCIH was appointed as ICOMOS ' expert committee on the study and conservation of industrial heritage. In this way, TICCIH gained the opportunity to be incorporated into a much wider network of international organizations.⁴¹

⁴⁰ Nisser, M. (1987). *Aspects of international co-operation. The industrial heritage: what policies? Architectural heritage reports and studies, no.6*, Strasbourg: Report of the Lyons colloquy (21- 27). 08.11.2019. <https://rm.coe.int/090000168093ea5f>, p.22

⁴¹ Saner, M. (2012). Endüstri Mirası: Kavramlar, Kurumlar ve Türkiye'deki Yaklaşımlar. *Planlama, 2012/1-2*, 53-66, p.55

⁴² Madran, E., & Özgönül, N. (2005). *Kültürel ve Doğal Değerlerin Korunması*. Ankara: Ankara Tmmob Mimarlar Odası.

The most recent and most important regulation on industrial heritage is the Nizhny Tagil Regulation, which was published at a meeting of TICCIH in Russia in 2003. In the Nizhny Tagil Charter, the industrial heritage consists of the remnants of industrial culture of historical, technological, social, architectural and scientific value. Industrial buildings and equipment, workshops, mines, warehouses, energy production areas, all kinds of infrastructure related to transportation, social spaces that constitute residential area in industrial zones constitute the industrial heritage.⁴³

In addition to all these developments, there are organizations operating in the field of industrial heritage internationally or including industrial heritage in their fields of activity. These organizations can be listed as mentioned above, TICCIH, the Council of Europe, ICOMOS, the European Union (Culture 2000 project, ECOC-European Capital of Culture), ERIH-European Route of Industrial Heritage, E-FAITH-European Federation of Associations of Industrial and Technical Heritage, DOCOMOMO- Documentation and Conservation of Modern Movement (Buildings, Sites and Neighborhoods).⁴⁴

All developments examined as the industrial heritage also apply to railway heritage. However, it would be more accurate to examine the development of the railway heritage in detail. “Railway Heritage” can have different connotations for different occupants. It may remind the engine of an old locomotive for an engineer working on the railroad while reminding a historical building for an architect.

However, according to Burman, the term “railway heritage” along with railway archives are in relation with buildings such as railway stations, engine sheds and railway works, lesser buildings such as boxes, technical equipment of many kinds (major of innovative and the product of highly original minds.), major buildings in the landscape

⁴³ The International Committee for the Conservation of the Industrial Heritage (TICCIH) (2003). *The Nizhny Tagil Charter for the Industrial Heritage*. Retrieved from <http://ticcih.org/wp-content/uploads/2013/04/NTagilCharter.pdf>

⁴⁴ Saner, M. (2012). Endüstri Mirası: Kavramlar, Kurumlar ve Türkiye’deki Yaklaşımlar. *Planlama*, 2012/1-2, 53–66, p.55-59

such as bridges, viaducts and minor artefacts that nevertheless convey the local distinctiveness of the various companies such as clocks, benches and other furniture.⁴⁵

Until today, the railroad heritage has not fully received the attention it deserves all over the world, including England. In the process of modernization, an awareness has begun to emerge with the endangered railway buildings that became dysfunctional due to many railway lines that were closed in the 1960s. The exhibition 'Off the Rails' which was organized in 1977 within the scope of "SAVE Britain's Heritage" increased the interest of the railroad in England, and after that date, the conservation of architectural or historic buildings and railways came to the fore. For example, in the 1980s, the transformation of Manchester Central Station into an exhibition hall began to conserve the railway buildings instead of letting them collapse.⁴⁵



Figure 2.17: Aerial View of Manchester Center Station.
(http://www.disused-stations.org.uk/m/manchester_central/, last visited on June 2019)

As a pioneer of the railroad in the early 19th century, England was also a pioneer in the development of the railway heritage. With the help of developing technology and

⁴⁵ Burman, P. (1997). *Philosophies for Conserving the Railway Heritage*, Conserving the Railway Heritage (18 – 33). London: E & FN Spon, p. 18-19

changing strategies, an independent company “Railway Heritage Trust” was established in 1985 to support the conservation and maintenance of unused railways. The company had 630 registered buildings in 1985 and the number increased to 1256 by 1997.⁴⁶

The Athens Charter, signed in 1931, does not contain any special advice for railways, but in general, it is important considering that it highlights continuous maintenance and the conservation of historic buildings. The concept of international cooperation which is another important issue in the conservation of the railway heritage is emphasized in this agreement. In 1964, the Venice Charter states that the need for the conservation of buildings that bear the messages of the past and which are living witnesses are concepts that can be adopted for railways.⁴⁷

The Venice charter (1964) says that “the concept of such a building or monument embraces not only the single architectural work but also the urban or rural setting in which is found the evidence of a particular civilization, a significant development or a historical event. This applies not only to great works of art but also to more modest works of the past which have acquired cultural significance with the passing of time.”

Ruskin and Morris emphasized the constant maintenance and conservation of buildings. According to the Venetian Regulation, buildings should be conserved as much as possible in their original places, and it is stated that decorations and sculptures of great artistic value can be carried exceptionally. Another important issue mentioned in the Venice Regulation is the documentation and publication of historical buildings. The Royal Commission on Historical Monuments of England has important studies on this subject.⁴⁸

⁴⁶ What is the RHT | Railway Heritage Trust. (2009). Retrieved September 10, 2019, from Railwayheritagetrust.co.uk website: <http://railwayheritagetrust.co.uk/what-is-the-rht/>

⁴⁷ Burman, P. (1997). *Philosophies for Conserving the Railway Heritage*, Conserving the Railway Heritage (18 – 33). London: E & FN Spon, p. 23-27

⁴⁸ Burman, P. (1997). *Philosophies for Conserving the Railway Heritage*, Conserving the Railway Heritage (18 – 33). London: E & FN Spon, p. 23-27

The Burra Charter, adopted in Australia in 1979 and updated in 1981 and 1988, expanded the definition of conserved areas and formed it as “sites, areas, buildings or other work of art, their contents and their surroundings and buildings or other groups of works” Such a definition may include a hotel, canopy, entrance courtyard, platforms, ticket office and other buildings of original texture. However, the condition of having cultural importance (aesthetic, historic, scientific or social value for past, present and future generations) in the next article is of great significance.⁴⁸

An event, held at King’s Manor in York on May 11-13, 1994, brought together people and organizations who have a say in conservation of the heritage of the railroad and expressed important views. Since then, the University of York and the National Railway Museum have created a master's program under the name of the Institute of Railway Studies and carried out important studies in the conservation and exploration of railways. In order to conserve the heritage of the railroad, first, it is necessary to have information about these works, to recognize and to introduce them. In this respect, the graduate program undertakes a very important mission⁴⁹

A defensible intellectual approach is needed on national and international platforms to help conserve the heritage of the railway. Peter Burman (1997) listed the views that are valid in the case of England but can be easily taken as examples all over the world as follows:

⁴⁹ Cossons, N. (1997). An Agenda for the Railway Heritage. In P. Burman & M. Stratton (Eds.), *Conserving the Railway Heritage* (pp. 3–17). London: E & FN Spon.

“1. Continuous maintenance, emphasized from Ruskin to the Burra Code, should be encouraged both for railway buildings that are still in use (likely to be out of use in the future), and for buildings that are out of use. It is of great importance to prepare a handbook in which their current situation is documented and conservation recommendations are presented.

2. It will be useful to determine the cultural value of each building. Buildings face the threat of being out of use today or tomorrow. When such a situation arises and re-use is necessary, the presence of a document stating the importance of the building will be very useful and will prevent many possible discussions.

3. Relevant institutions should publicize and promote a railway building in the same way that they conserve any cultural property.

4. Promotion and publication are very important. Publications should be made available to the public and to scientists.

5. A map showing the railway heritage will be very useful.

6. The Institute of Railway Studies in York has a major role to play in publications and research.

7. The promotion of registered railway buildings needs to be conventionalized digitally.

8. When the railway buildings are out of use, research shows that new functions can be given very favorably.

9. The work of the Railway Heritage Trust should be better advertised.”⁵⁰

⁵⁰ Burman, P. (1997). *Philosophies for Conserving the Railway Heritage*, Conserving the Railway Heritage (18 – 33). London: E & FN Spon, p. 30-31

The interest in railways is maintained not only by academics but also various groups. In addition to many foundations and organizations around the world, there are many groups interested in railways in social media and various blogs. These masses interested in railways aim to include the railways in the World Heritage List and thus increase the prestige of the railway lines. Therefore, it is very important that ICOMOS examine the railways under a separate title, although it is included in the industrial heritage.⁵¹

In 1999, ICOMOS published an article entitled “Railways as a World Heritage Site” to draw the attention of the World Heritage Committee. The main topic of this article is on concerns and recommendations for the determination of the international railway heritage. In this article, railways are considered as socio-technical systems. Socio-technical systems, on the other hand, are defined as systems in which the social and technical qualities cannot be separated from each other, one of which can be both the outcome and the influence of the other.⁵²

Accordingly, the following criteria are set forth in the evaluation of railways;

- “1. Outstanding creative products: important railways, important engineers.*
- 2. To be influenced and influenced by creative technology: rail bearings, construction of embankments, engineering and architectural buildings and construction techniques, technology transfers and so on.*
- 3. To be a striking or typical example,*
- 4. To be an indicator of economic and social development”⁵²*

Each of the mentioned criteria is complementary to each other.

⁵¹ Kösebay Erkan, Y. (2007). *Anadolu Demiryolu Çevresinde Gelişen Mimari Ve Korunması* (PhD Thesis). The Graduate School of Natural and Applied Sciences of İstanbul Technical University, p.144

⁵² Cossons, N. (1997). An Agenda for the Railway Heritage. In P. Burman & M. Stratton (Eds.), *Conserving the Railway Heritage* (pp. 3–17). London: E & FN Spon, p. 5

2.2.2. Conservation Efforts of Industrial and Railway Heritage in Turkey

It is also known for about the last thirty years in Turkey, and it is becoming increasingly common. Since the beginning of the 1990s, conservation approaches of the old industrial buildings in Turkey have begun to take shape. One of the first practices for the conservation of industrial heritage is the registration of some industrial buildings as the projects for the cleaning and rehabilitation of the estuary shores of Istanbul at the end of the 1980s predicted the demolition of some old production structures in the Golden Horn. In addition, attempts have been made to conserve the elements such as cylindrical gas meters and cranes with the danger of collapsing of the gas plants during the same periods. At that time, the main agenda of these processes was the concern of preserving the old work or collective memory rather than industrial heritage.⁵³

After these processes, the concept of industrial archaeology or heritage began to be used in Turkey and even started to be offered as a course in some programs. There are few conservation projects that have been successfully implemented, such as Power Plant Istanbul in Istanbul, CerModern in Ankara, and the conversion of the Adana National Textile Factory into a museum.⁵³

Turkey has a rich railway heritage with its railway lines and stations. However, it has unfortunately failed to develop in the sense of preserving the railway heritage. While the criteria of railway heritage are being discussed and developed all over the world; Turkey, unfortunately, does not have a law on this special subject concerning the conservation of the railway heritage.

The Law on the Conservation of Cultural Heritage No. 2863 did not mention the railroad heritage as in the definition of a scientifically important immovable object. With the amendments regarding this law in 2004, it becomes easier to register the industrial heritage, while demolition and sales and registration are not sufficient. Even though

⁵³ Saner, M. (2012). Endüstri Mirası: Kavramlar, Kurumlar ve Türkiye'deki Yaklaşımlar. *Planlama*, 2012/1-2, 53–66. p.60-61

many station buildings in Turkey have been registered through the establishment of conservation, it is questionable whether they have been registered through necessary criteria and consideration. These studies are always carried out for individual elements and buildings and do not cover all the items included in the definition of railway heritage. In addition, most of the small stations in many cities in Anatolia are not registered. These buildings and their surroundings, which are not conserved by any law, are in great danger.

Finally, while four institutions (the National Railway Museum, English Heritage, Railway Enthusiast Movement, Railway Heritage Trust) is responsible for the conservation of railways in England, the railway conservation and repair responsibility belong only to Turkey State Railways in Turkey. When the need for maintenance or unused railroad heritage is left to the initiative of a single institution, inaccurate decisions can often be made and our railway heritage is damaged.⁵⁴

2.2.3. Examples for The Conservation of Railway Heritage

Since the 20th century, the work on preserving the Railway Heritage has been focused on singular elements such as wagons and locomotives. One of the most important milestones in this process is undoubtedly the publication of criteria by ICOMOS in 1999 for the identification of major international railways as candidates for the World Heritage List. In this way; in the following period, railway lines reflecting important engineering structures and innovative technologies and affecting social and economic developments in the regions it passes through were included in the World Heritage List. When examining the samples, it would be correct to start with these lines and continue with lower scale samples.⁵⁵

⁵⁴ Coulls, A. (1999). Railways as World Heritage Sites. *ICOMOS*. Retrieved from <https://www.icomos.org/studies/railways.pdf>, p.13-17

⁵⁵Yıldız, A. (2017). Demiryolu Mimarisinin Korunma Sorunları ve Kırklareli Tren İstasyonu Yapılarının Yeniden Kullanımı için Bir Öneri. *Çukurova Üniversitesi Mühendislik-Mimarlık Fakültesi Dergisi*, 32(4), 39–54. p.42

2.2.3.1. Examples from The World

Since it was built in the mountain area, special engineering techniques were used.

Baltimore & Ohio Railroad, United States of America: The United States of America: It is the first railway built in USA. It was built simultaneously with the Liverpool & Manchester Railway. It is one of the best examples of technology transfer from one country to another. It also holds a very important place in terms of socio-economic criteria. When the railroad reached Ohio in 1857, it covered 600 km and had a key role to the country's development. The benefits of this line should not only be perceived as regional. Baltimore is considered the birthplace of American railways.⁵⁶

The Great Zig Zag, Australia: The Zig Zag Railway was the first railway venture to extend into the interior of Australia, crossing the mountain ranges 150 km west of Sydney in 1868. Due to financial inadequacy, the difficult terrain was replaced with a zig-zag route instead of a tunnel. Since Railway Technology has not advanced much, traditional but robust structures have been built, and many are still standing. The railway later became popular as a tourist attraction, and in 1881 a 23 hectares area was declared a wildlife reserve. 250,000 people visit this region every year.⁵⁶

The Darjeeling Himalayan Railway, India: The Darjeeling Himalayan Railway is a line with many outstanding features, such as social, economic and political influences and the relationship of the route with the land. It is a great success that Darjeeling, which is 2000m high, has been able to climb the steep slopes in a short time with cheap but effective techniques. It accomplished this with four rounds and four zig zags around it. The railway, which is in good condition today, has not lost its value. Stations other than Darjeeling station retain their originality. The fact that some of the original locomotives are still in use increases the cultural heritage of the railway.⁵⁶

⁵⁶ Coulls A. (1999). Railways as World Heritage Sites. *ICOMOS*. Retrieved from <https://www.icomos.org/studies/railways.pdf>, p.13-17

The Liverpool & Manchester Railway, United Kingdom: Liverpool & Manchester railway is the first railway to operate locomotives used in passenger and freight transportation. Certain parts of the route are still as in the 1830s. Liverpool Road station is part of the Manchester Science and Industry Museum. Some stations and other buildings on the line are registered as buildings with historical value. The fact that many parts of the line are in use shows how well they are built. With these qualities it meets most of the World Heritage criteria.⁵⁷

The Shinkansen, Japan: What the Liverpool & Manchester line was for train travel in the sense of technological creativity when it was first built, the Shinkansen railway is the same for modern high-speed trains. This line is the product of the creative genius of the 20th century, and its effects have spread all over the world. This is the application of the technology transfer criterion in the 20th century.⁵⁷

In addition, sometimes railway lines or railway settlements or railway buildings may be out of use. In such cases, the re-functionalization pathway is preferred. Although it is not on the list of world rail heritage, there are some examples of re-functionalization in the world and in our country.

New York-Highline: In the same year, Congress passed the Trail System Act, allowing people to circumvent complicated land rights issues in order to transform old rail lines into recreational areas. However, the unused line was thought to be not appealing by many people over the years and was ordered by the mayor to demolish it. In 1999, a non-profit group founded high line friends and, together with their supporters, became the only group responsible for the maintenance and operation of the high line. In 2003, an idea project competition was held with 720 participants from more than 36 countries. This situation attracted the attention of the high line. Between 2004 and 2006, with the strong support of the mayor and the council, a development plan was proposed for the use of the high line as a public park. Then High Line Art was founded

⁵⁷Coulls A. (1999). Railways as World Heritage Sites. *ICOMOS*. Retrieved from <https://www.icomos.org/studies/railways.pdf>, p.13-22

in 2009 and continues annually to build and produce art works in and around the High Line. Completed in 2014, the project has become one of the important recreation areas of the country today.⁵⁸

Poughkeepsie bridge: The bridge, whose construction was completed in 1889, was badly damaged by a fire in 1974 and closed to use. In 1993, a nonprofit group called Walkway over Hudson was formed to convert the bridge into a pedestrian bridge. Together with funds provided by the established group and the Dyson Foundation, the bridge functioned as a public hiking trail, becoming an example of a successful conservation project.⁵⁹

2.2.3.2. Examples from Turkey

Unfortunately, there are very few examples of qualified conservation in Turkey. Particularly with the modifications made by the users who rented empty station buildings by TCDD, the buildings are damaged seriously. Gazi station in Ankara and the station turned into Mudanya hotel in Bursa can be cited as examples of bad practices. In addition, there are Istanbul Railway Museum in Sirkeci station in Istanbul, logotype museum in Ankara and Izmir Camlik, and Tulomsas Museum in Eskisehir. However, these examples focus on singular elements such as the locomotive instead of a holistic approach.⁶⁰

Although it is not possible to approach the railway heritage with a holistic approach due to the existing institutions and laws, CerModern is one of the most important and qualified conservation projects in Turkey.

⁵⁸ History | The High Line. (2019). Retrieved August 10, 2019, from The High Line website: <https://www.thehighline.org/history/>

⁵⁹ Poughkeepsie-Highland Bridge | ASCE. (2019). Retrieved November 10, 2019, from Asce.org website: <https://www.asce.org/project/poughkeepsie-highland-bridge/>

⁶⁰ Köşgeroğlu, E. F. (2005). *An Approach for Conservation of Railway Heritage; Assessing and Experiencing the İzmir- Aydın Railway Line* (Msc Thesis). The Graduate School of Natural and Applied Sciences of Middle East Technical University. p.26-30

Cermodern: The construction of the ateliers, which form the basis of the buildings on which the modern Art Center, which became operational under the name CerModern in 2010, was established in 1927, shortly before the opening of the Ankara-Kayseri railway line. The fact that Ankara Station, having an important connection such as Kayseri Line, not only increases the work capacity, but is also designed to meet maintenance and repair activities in draw frames. With the end of the locomotive period, the building lost its function and in 1995 it was decided to transform the building into a center of contemporary art. The complex, which has been made re-functional as CerModern in partnership with the Ministry of Culture and Tourism and the Association of Travel Agencies of Turkey (TÜRSAB) in 2010, is built on an area of 11,500 square meters and consists of exhibition area, conference hall, library, store, artist rooms, studios, cafeteria, sculpture park and parking lot.⁶¹

The CerModern provides a very important example in Turkey in terms of discussing the view on railway heritage. As emphasized in the previous paragraph, while it is a very successful conservation project on the building scale, its relation to the context has been severed. However, the railway is a big organization and as mentioned above, it should be handled in a holistic way with all its parts. Understanding of heritage railway in Turkey unfortunately coincides with CerModern instance. None of the examples examined approached the railway heritage in a holistic way.

⁶¹ Çekiç, C. E. (2016). CerModern: Bir Mavi Tren Bizi Sanatın Kalbine Götürecek. *Şehir Araştırmaları Dergisi*, 1, 68.

CHAPTER 3

UNDERSTANDING THE CITY: EĞİRDİR

The main research subject of the thesis is of great importance for the town of Eğirdir, where the train station complex is located. It is also interesting that the district of Eğirdir, which is geographically, politically, and economically highly affected by the station, has been sustained by the station. In the second part, with the help of the researches on the railway in the Ottoman Empire and Turkish Republic periods, it can be considered as the freshwater edge and fertile land as the reason why the railway came to Eğirdir. The article “agricultural products, mines, raw materials, which can be left and right of 20 km on the route passed by the railway, belongs to the owner of the railway” from the Ottoman Empire appears to be an important reason. However, in order to better understand both Eğirdir Train Station Complex, it would be correct to look at the whole of the city.



Figure 3.1: Eğirdir Aerial Photo (Eğirdir Akın Newspaper Archive)

3.1. General Features of Eđirdir

3.1.1. History

It is understood from the findings and records in the vicinity of Lake Kovada that Eđirdir and its surroundings would have been settled during the period of the Kingdom of Arzava (2000-1200 BC). During the Hittite period (1800-1200 BC) it is known that the lands where Eđirdir was located were referred to as “Pitařsa”. However, the Lydians are the civilization that was established in the center of Eđirdir District and the settlement continues to this day. The estimated years of the settlement range from 560-547 BC. The city of Eđirdir was founded by Kroisos, the last ruler of Lydia, and its first name is thought to be Krozos. It is claimed that the castle of Eđirdir, one of the most important buildings of Eđirdir, was built during this period. Today this area coincides with Ađa, Cami, Hamam, Hacıřeyh, İmaret, Kubbeli, Poyraz and Seydim neighborhoods.

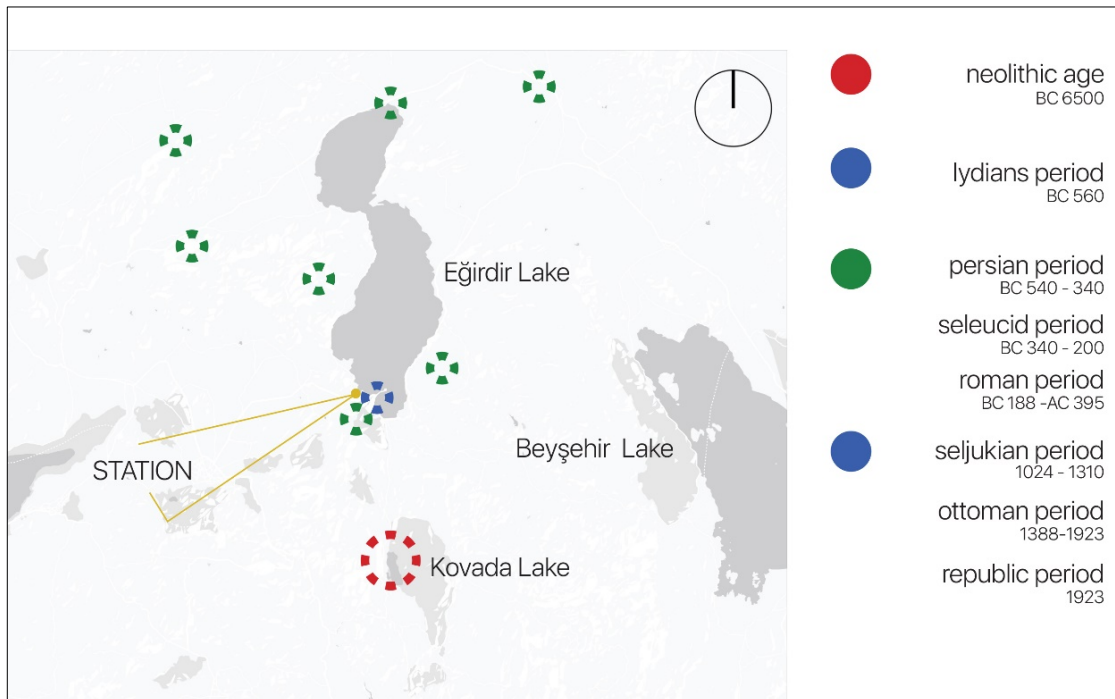


Figure 3.2: Historic Period of Eđirdir (Sinan, 2019)

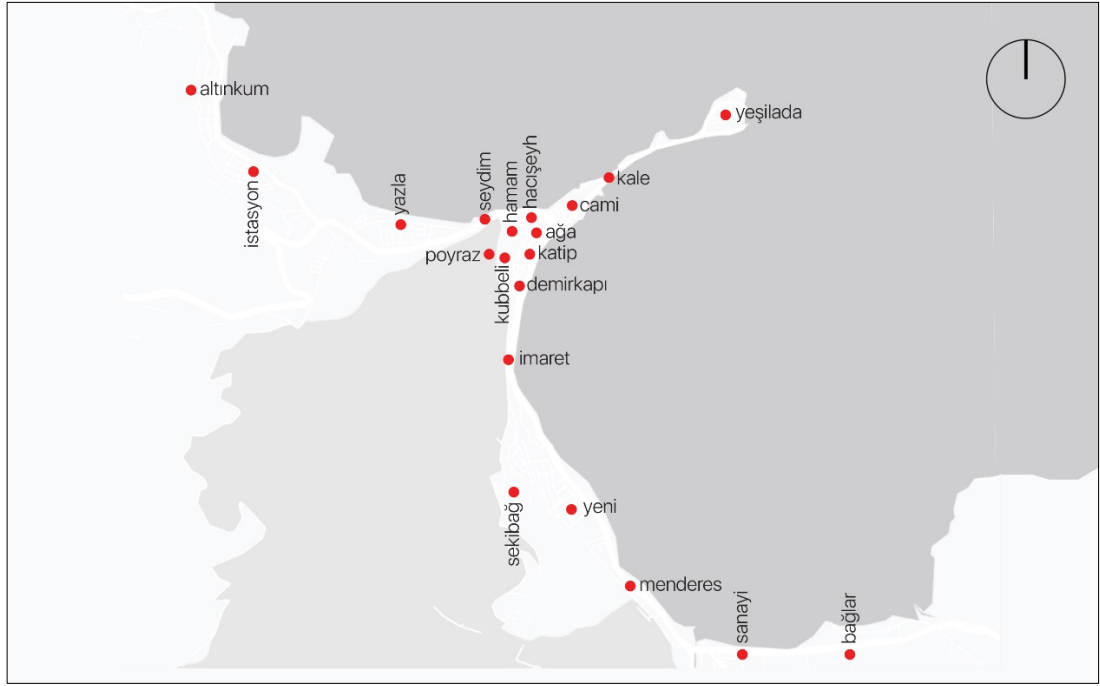


Figure 3.3: Neighborhoods of Eğırdir (Sinan, 2019)

The region was under the control of Alexander the great between 334-323 BC, and after the death of Alexander the Great in 323 BC, the region fell into the hands of the Seleucids as a result of the Kurupedion War (281 BC) between Seleucids' and Lysimachus, one of his successors. In 188 BC, the Seleucids, who had defeated the Roman army and signed the Peace of Apameia (Dinar), withdrew from the section up to the Taurus Mountains and the region was left to the Bergamans by the Romans. During the Hellenistic period, the cities of Prostanna, Parlais near the town of Barla and Malos near the town of Sarıdris were established on the outskirts of Viarus.

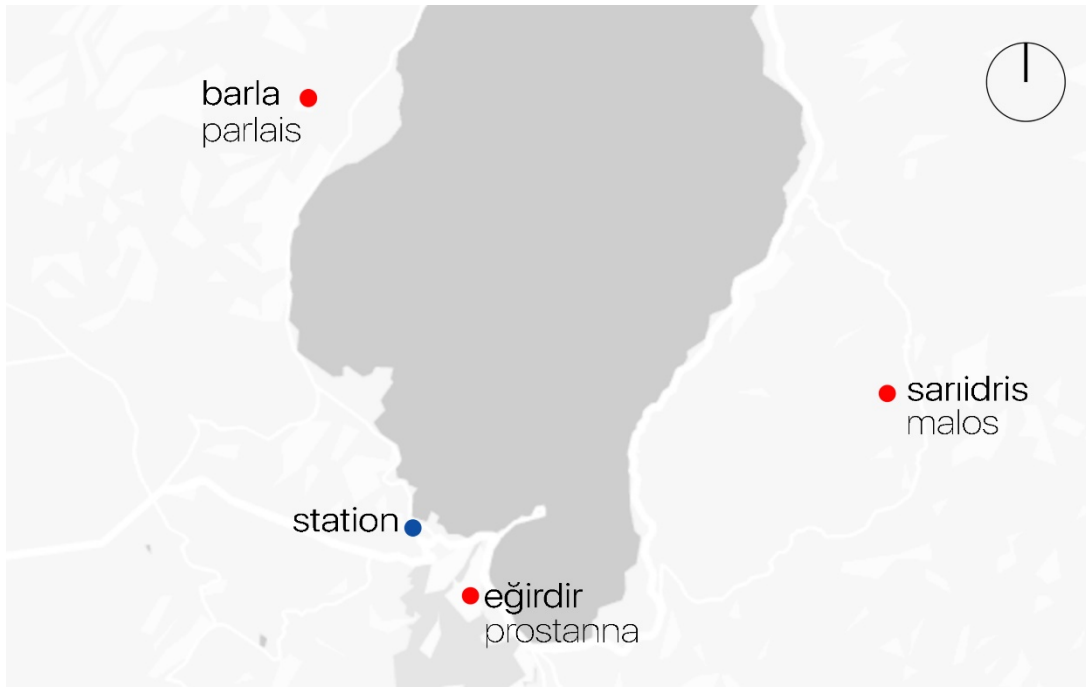


Figure 3.4: Cities which was established in Hellenistic Period (Sinan, 2019)

Eğırdir and its environs came under the reign of the Anatolian Seljuk State together with the surrounding cities in 1204. Eğırdir was quite developed in terms of urbanization during the Seljuk Period. The castle fortifications have been repaired, and many of the monumental buildings that now stand have been built. It also became an important commercial center during the Seljuk period. Antalya port has become an important accommodation and trade center on the way to Anatolia. Eğırdir Inn, one of the important Anatolian Seljuk caravanserais, was founded in 1237 by the Seljuk Sultan II. It was built by Gıyaseddin Keyhüsrev. Today, this inn has become an archaeological ruin and is located next to the Caravanserai Cemetery.



Figure 3.5: Eğırdir Caravanserai
(<http://www.selcuklumisasi.com/architecture-detail/egirdir-hani>, last visited on June 2019)

On the other hand, looking at the buildings such as madrasah, dervish lodge and so on which were built on the XIII. century and the last half of XII. century, it can be said that Eğırdir is a very lively city in terms of science and social aspects. With the collapse of the Anatolian Seljuk state, the period of principalities in Anatolia began and the Principality of Hamitoğulları was established in 1301 by Feleküddin Dündar Bey. Feleküddin Dündar Bey, a member of the Teke Turkmen longitudinal Iğdir, founded the Principality of Hamitoğulları and first announced Uluborlu as the center of this Principality, yet, later in 1310, he announced Eğırdir as the center of the Principality.⁶²

Eğırdir is referred to as “Felekabad” in this period and a serious development has been seen in the urban sense. The city was heavily attacked during this period. The city did not change hands until 1390, although Osmanogulları, which supported these attacks, granted some land. It came under the Ottoman rule in 1390.

⁶² Eğırdir AB Proje Bürosu. (2019). Eğırdir’in Tarihi. Retrieved September 10, 2019, from Eğırdir.gov.tr website: <http://egirdir.gov.tr/egirdirin-tarihi>

Eğirdir experienced its worst years during the Ottoman period. Until that date, the town of Eğirdir, which has a population greater than the population of Isparta, has started to shrink. However, it continued to be an important marketplace. According to Konya province, Salnamesi dated 1305 (1888), there are thirty-two mosques, seventeen mosques, two madrasas, a library, four baths, as many as 150 shops, three inns and two churches in Eğirdir at this time, four of which are minarets.

The biggest development for Eğirdir during the Ottoman period was the extension of the İzmir-Aydın railway line to Eğirdir. With this line, Eğirdir was on its way to regaining its old commercial importance. During the first world war and national struggle, Eğirdir was of critical importance. It became an important factor of the western front with the fact that it was the last station of İzmir-Aydın line during these dates. After the establishment of the Republic, Eğirdir maintained its district status as in the Ottoman period.

By the law enacted in 1930, the names were changed and the name of the district was changed to “İğirdir”. However, to resolve the confusion in the post office department, the initials were changed to E and used as Eğirdir. Due to public reactions, it gained its present name as Eğirdir by the decision of the assembly of 1985.⁶³

⁶³ Eğirdir AB Proje Bürosu. (2019). Eğirdir’in Tarihi. Retrieved September 10, 2019, from Eğirdir.gov.tr website: <http://egirdir.gov.tr/egirdirin-tarihi>

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Isparta Milletvekili İbrahim Fevzi Yaman ve 13 Arkadaşının,
Eğirdir İlçesi Adının Eğirdir Olarak Değiştirilmesi Hakkında
Kanun Teklifi ve İçişleri Komisyonu Raporu (2/227)

TÜRKİYE BÜYÜK MİLLET MECLİSİ BAŞKANLIĞINA

Isparta İline bağlı Eğirdir ilçesinin adının «Eğirdir» olarak değiştirilmesi hususunda hazırlanan kanun teklifi ve gerekçesi ilişikte sunulmuştur.

Gereğine emirlerinizi saygılarımızla arz ederiz.

10 . 4 . 1985

<i>Ibrahim Fevzi Yaman</i> Isparta Milletvekili	<i>M. Kemal Toğay</i> Isparta Milletvekili	<i>Haydar Özalp</i> Niğde Milletvekili
<i>Ahmet Turan Soğancıoğlu</i> Sivas Milletvekili	<i>Togay Gemalmaz</i> Erzurum Milletvekili	<i>Birsel Sönmez</i> Niğde Milletvekili
<i>İsmet Ergül</i> Kırşehir Milletvekili	<i>Cengiz Tuncer</i> Kayseri Milletvekili	<i>Ahmet Altıntaş</i> Muğla Milletvekili
<i>Abdullah Cengiz Dağyar</i> Antalya Milletvekili	<i>Nabi Sabuncu</i> Aydın Milletvekili	<i>F. Mihriban Erden</i> Isparta Milletvekili
<i>Mehmet Özalp</i> Aydın Milletvekili	<i>Metin Ataman</i> Isparta Milletvekili	

GEREKÇE

Eğirdir M.Ö. 546 yılında Lidya Hükümdarı Krezüs tarafından kurulmuş uzun zaman bu hükümdarın adını taşımıştır.

Pars (İran) Hükümdarı Keyhüsrev'in Lidya Kralı Krezüs'ü mağlup ederek Anadoluyu hâkimiyetine geçirmesi sonucu Eğirdir'de Keyhüsrev'in idaresine girmiştir.

Anadolu'daki Roma ve Bizans hâkimiyeti sırasında önemli bir merkez durumunu korumuştur. Selçukluların Anadolu'yu Bizanslılardan almaları sonucu Selçuklu Sultanı 2. Kılıç Arslan zamanında 1156 yılında Eğirdir Selçuklu Devleti idaresine girmiştir.

Selçuklu İmparatorluğunun yıkılması üzerine 1297 yılında Uluborlu'daki Uç Kumandanı Hamit Bey Hamitoğulları Beyliğini kurmuş ve ölümünden sonra 2. Hükümdar olan Dündar Bey tabii güzelliği ve merkezi vaziyette oluşu itibarı ile Beylik Merkezini Uluborlu'dan Eğirdir'e nakletmiş ve ufak bir kasaba olan Eğirdir'i yeni baştan imar ve ihya ederek büyük ve mamur bir şehir haline getirmiştir. Dündar Bey imara ve ilme büyük önem vermiş ilk icraatı olarak Selçuklu İmparatoru Alaattin Keykubat'ın 1238 yılında han olarak yaptırdığı, kapısının güzelliği ve mimari üstünlüğü ile şöhret bulmuş halen taş medrese diye anılan binayı 1301 yılında medreseye çevirmiştir.

Eğirdir adının Eğirdir'i Beylik merkezi yapan Hamitoğullarının Oğuzların «Eğirdir» boyundan gelmelerinden dolayı verilmiş olduğu tahmin edilmektedir. Dündar Beyin Eğirdir'de bastırduğu gümüş paraların üzerinde «Eğirdir» yazılıdır. Hamitoğulları Beyliğinin güçlü ve ihtişamlı devrinde 1330 yılında Eğirdir'e ge-

Figure 3.6: Petition for Name Change (Eğirdir Akın Newspaper Archive)

Besides, due to its nature which is favorable for fire disasters, Eđirdir has survived important fire disasters. The most important and well-known fires in 1814, 1908 and 1959. “Cami” one of the oldest neighborhoods, including the administrative units, was destroyed with this fire. With this event, a lot of data loss occurred in Eđirdir.



Figure 3.7: Cami Neighborhood Fire in 1959 (Eđirdir Akin Newspaper Archive)

region, especially in summer. Northwest; Barla Mountain (2,798 m); Davraz Mountain (2,635 m); Dedegöl Mountain (2.998 m) intersects this section in the north-south direction. The average altitude of Eğirdir is 918 m.

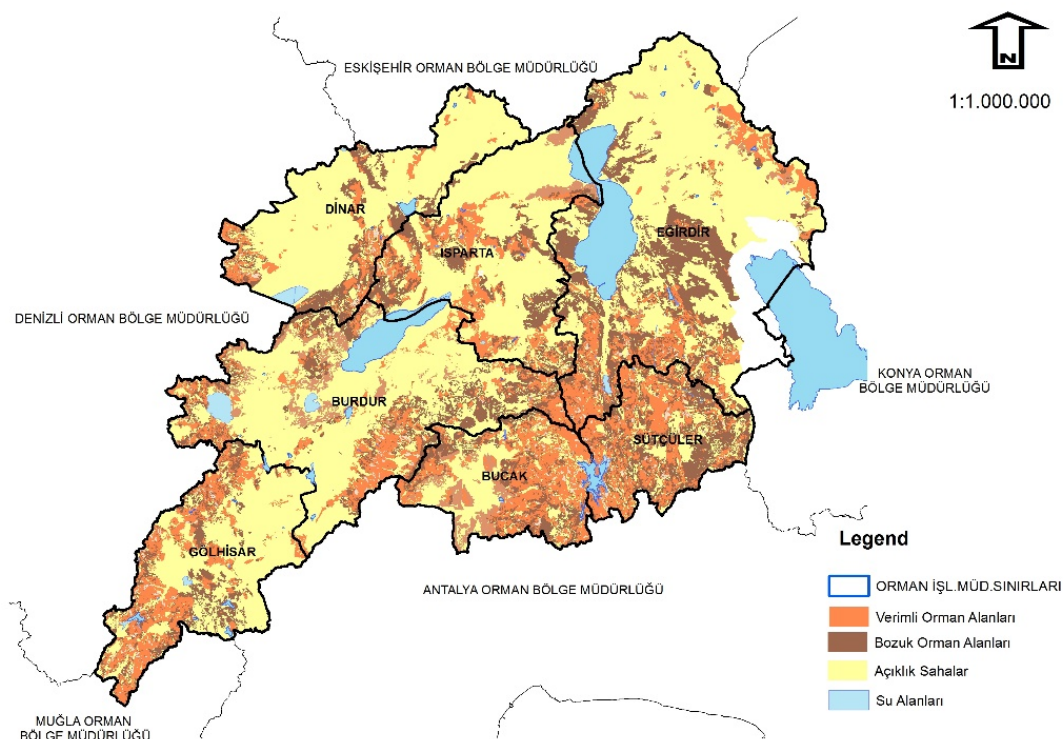


Figure 3.9: Geographical Map of Isparta
(<https://ispartaobm.ogm.gov.tr/Documents/OBM%20foto/fiziki.jpg>, last visited on June 2019)

Inclination within the boundaries of Eğirdir district increases up to 40%. There are some important plains between the mountains. The most important of these is the plain of Boğazova. The mountains are partly forested. In general, pine, larch is dominant; oak, juniper, cedar, fir communities are also encountered. The most important plateau of the district is the Barla Plateau. The most important river is the Aksu.⁶⁴

⁶⁴Eğirdir AB Proje Bürosu. (2019). Eğirdir'in Tarihi. Retrieved September 10, 2019, from Egirdir.gov.tr website: <http://egirdir.gov.tr/egirdirin-tarihi>



Figure 3.10: Boğazova
(https://www.egirdirakingazetesi.com.tr/App_Themes/all/dataimg/haber/buyuk/8406.jpg, last visited on June 2019)

The district is located in a transition area between Mediterranean and Central Anatolia climates. Depending on this climate type, the district is neither rainy in the Mediterranean nor arid climate in Central Anatolia. The average annual temperature is 11.9°C and the average rainfall is around 705 millimeters. A large part of Lake Egirdir, which is the largest lake in the Lakes Region, is located within the boundaries of the district. Another lake in the district is Lake Kovada.



Figure 3.11: Lakes Region
(earth.google.com, last visited on June 2019)

One of the most important lakes of the Lakes Region as well as within the boundaries of Isparta Province is Eğirdir Lake. Lake Eğirdir is located between the Sultan and Karakuş Mountains and in the middle of the provincial area. With 517 km² surface area is Turkey's 4th largest lake. Eğirdir Lake, whose north-south length is 50 km and whose east-west width varies between 3 and 15 km, collects the waters of a basin of approximately 3,309 km². The karstic structure has a large share in the formation of the lake. It was formed as a result of the collapse of the sediment troughs on the main limestone foundation. The lake has an altitude of 916 m and an average depth of 12 m. The maximum depth is 16.5 m near Eğirdir.

North-east winds on the lake create dangerous waves. In the south of Hoyran, there is a rapid flow towards Eğirdir. Lake Eğirdir is divided into two parts. The smaller lake section in the north is called Hoyran Lake and the southern section is called Eğirdir Lake. Both lakes are connected by Hoyran Strait. The edges of the lake are generally steep. In the Gelendost and Hoyran regions where this steepness disappears, there are swamps on the lakeside.



*Figure 3.12: Hoyran Lake
(earth.google.com, last visited on June 2019)*

There are two small islands in the lake, like an extension of the peninsula on which Eđirdir is located. One is Can Ada and the other is Yeşilada (Nis). There are over 100 houses in Yeşilada. In recent years, these islands have been connected and Eđirdir by taking advantage of the decrease in lake waters. There are many fishes in the lake. The best breeds are burr, shiraz, sea bass, freshwater lobster, and zander. The fishery in the lake is mostly made by residents of Yeşilada.



Figure 3.13: Aerial Photo of Yeşilada

(<https://seyyahdefteri.com/wp-content/uploads/2019/04/E%C4%9Firdir-Ye%C5%9Filada-2.jpg>, last visited on June 2019)

Lake Eđirdir, on 17.05.1996 due to its natural beauty and features, was been declared I. and III. Degree Natural Conserved Area. There are many natural beauties around Eđirdir. These are the Kovada Lake National Park, which was a national park in 1970 and declared as a first-degree natural conserved area in 1992, located on the Eđirdir-Barla road, and Gelincik Mountain, which has an important biological diversity and significant biodiversity. This mountain has 218 varieties of plant species including the pulley, hair, macedonia, thuja oak, additionally including tree species such as Lebanon cedar, larch, and red pine was declared as a nature conservation area in 1987.

3.1.3. Economy

The Eđirdir economy is very versatile due to its geography. As in all of Anatolia, agriculture and animal husbandry is in the foreground in Eđirdir. Also, mining and tourism are the elements that feed the other economy.

Agriculture

One of the most important livelihoods of Eđirdir is agriculture. The region is very rich in fruits and vegetables, especially apples. However, the main framework of Eđirdir's agricultural activities is Apple. According to TUIK figures for 2014, approximately 2.500.000 tons are produced in Turkey, 650.00. tons are produced in Isparta and 250.000 tons are produced in Eđirdir. One out of every four apples grown in Turkey Isparta known as the capital of apples, and 1 out of every three apples grown in Isparta is produced in Eđirdir. With its climate and soil quality, Eđirdir is quite developed in apple cultivation. With the development of apple growing, it has become one of the most important centers of the region with its 150.000 tons cold storage capacity. Eđirdir Sapling (Fruit Research Institute), founded in 1952 in Eđirdir, which is the center of the apple, makes significant contributions to agriculture and regional employment. Today, Eđirdir has become a big apple center that needs the apples it produces abroad. In addition to apples, agriculture can be made with different varieties such as cherry, peach, and fungus.⁶⁵

Fishery Industry

Along with agriculture, Eđirdir and other economic activity that is common in the vicinity is animal husbandry. Animal husbandry in Eđirdir is mainly carried out in surrounding villages. On the other hand, the other economic activity integrated with the identity of Eđirdir is fishing. Eđirdir Lake is the fourth largest lake and the second-largest freshwater lake in Turkey with an area of 487 square kilometers and a volume of 4005 hectometre cube. Fishing in Lake Eđirdir, which is one of the most important

⁶⁵ Eđirdir AB Proje Brosu. (2019). Eđirdir Elması. Retrieved September 1, 2019, from Egirdir.gov.tr website: <http://www.egirdir.gov.tr/egirdir-elmasi>

wetlands with its biodiversity, is an important source of livelihood for those living in its vicinity. Although the recent decline in fish population has negatively affected the economy, fishing is integrated with the city identity. Commercial aquaculture production in Eđirdir Lake started in 1950 and now constitutes 4.4% of Turkey's freshwater production. Lake Eđirdir, which was traditionally hunting before, attracted the attention of foreign states with the potential of its aquatic product. In this sense, in 1913, England rented Eđirdir and Beyşehir Lakes enterprises from the Ottoman Empire for 99 years. ⁶⁶

About ten species have been known to grow in Lake Eđirdir since the past. Today, there are freshwater crayfish which are preferred by foreign countries especially in addition to the fish species such as zander, carp, and eđrez. Especially between 1970 and 1980, zander and crayfish hunting had its golden days. In those years, products were shipped to countries such as France, the Netherlands, Belgium, Sweden, Norway, Denmark, and Germany. ⁶⁷

However, in recent years there has been a serious decline in fisheries and the economy of Eđirdir has become different from its old days. In 1986, the Directorate of Eđirdir Fisheries Research Institute, located at an important point in aquaculture, was established in August 1986 to research the inland waters of Turkey, directly affiliated to the Ministry, and was put into operation in April 1987. In 2011, Eđirdir Fisheries Research Institute (Eđirdir Unit) was united under the umbrella of Mediterranean Fisheries Research Production and Education Institute.

In 2013, as a provincial organization directly connected to the center, Isparta Province, Eđirdir District was restructured in the form of Fisheries Research Station Directorate. In 2015, the name was re-established as the Fisheries Research Institute. Aquaculture

⁶⁶ Zaman, S., & Çetin, B. (n.d.). Eđirdir Gölü'nde Su Ürünleri Avcılığı. *Eastern Geographical Review*, 16, 273–304, p.273

⁶⁷ Bolat, Y., Aksoylar, M.Y. (1997) Eđirdir Gölü Kerevitlerine (*Astacus leptodactylus salinus* Nordman 1842) Genel Bir Bakış. IX. Ulusal Su Ürünleri Sempozyumu, SDÜ Eđirdir Su Ürünleri Fak. ve Palmar Su Ürünleri End. A.Ş. 17-19 Eylül 1997, 257-269.

Research Station in Eđirdir District. The Directorate today makes significant contributions to national and international research and activities and to the employment of the region with the help of national and international research and activities it has conducted⁶⁸

Mining

Although Isparta and Eđirdir are said to be in a position with various mineral reserves, only marble can be shown with economic counterpart throughout Turkey. In recent years, the increase of marble quarries in the vicinity of Eđirdir can be easily observed. Mustafa Kūçūkyaman, President of the Isparta Marble and Miners Association, on this issue: “Marble is the first item when it comes to the exportation in Isparta. While Isparta's apple exports worth 2 million dollars, marble worths 80 million dollars,” he said. In this sense, the marble quarries seen in Eđirdir constitute a great economy for Eđirdir. The association chairman Kūçūkyaman emphasized that there are 134 active marble quarries in the province, 14 in the center of Isparta, 71 in Eđirdir and 49 in Sūtçūler, and 3500 people employed in this area indicate the importance of marble quarries for Eđirdir.⁶⁹

3.1.4. Tourism

Its historical and natural beauties, climate and sociological status make Eđirdir a tourism paradise. Water and mountain sports such as paragliding, windsurfing, camping caravan tourism, bird watching, mountain hiking, canoeing, orienting, fishing, triathlon can be performed in and around the lake. Davraz Ski Resort, which is 15 km long from the district center, has an important place in winter sports in Turkey. It is also noted for its archaeological areas and historical buildings which it holds in and around.

⁶⁸ Tarihçe. (2019). Retrieved August 10, 2019, from Tarimorman.gov.tr website: <http://arastirma.tarimorman.gov.tr/sarem/Menu/9/Tarihce>

⁶⁹ hurriyet.com.tr. (2017, October 17). “Isparta ihracatında mermer ilk sırada.” Retrieved September 11, 2019, from Hürriyet website: <http://www.hurriyet.com.tr/isparta-ihracatinda-mermer-ilk-sirada-40613123>

The biggest development of Eđirdir, in particular, is the adoption of the Cittaslow movement in 2017, which became widespread among Italian cities in the early years of its establishment and has 209 members in 30 countries. Eđirdir, which is the twelfth most peaceful city of Turkey, has become well-known in the tourism of the country.



Figure 3.14: Cittaslow Signboard on Entrance of The City Center (Sinan, 2019)

Accordingly, with the Cittaslow criteria, Eđirdir improves its tourism strategy. Today, the cittaslow criteria (conservation of traditional buildings, removal of vehicles from the city center, use of local products, use of renewable energy, lack of supermarket and fast-food restaurants, support of local product sellers, conservation of urban fabric, conservation of old buildings, conservation of old crafts, increasing the quality of air and urban life) gives importance to the development of Eđirdir in many areas.⁷⁰

In addition to this information, Mountaineering Festival, Eđirdir motorcycle festival, Eđirdir Triathlon Turkish championship races, Bridge Festival, and classical music festivals are held within the scope of tourism activities in Eđirdir. Besides, people from the surrounding provinces and districts come to events such as conferences and seminars. The district provides 8 hotels, 12 pensions, 660 beds and 18 restaurants for these tourism activities.

⁷⁰ Tarhan, Z. (2018, February 9). Başkan Őengöl'le Sakin Őehir Cittaslow Eđirdir'in Öyküsü. *Akın Gazetesi*.

3.1.5. Archaeology

The city, which dates back to ancient times, has quite a lot of archaeological sites in and around it. The ancient city of Prostanna, which is within the city, Isparta province, about 1.5 km from Akpınar village of Eğirdir District. it is located to the east of the military area in Camili Plateau, to the northwest. The ancient city of Prostanna, which was first established in the form of a defensive structure in the foothills south of the ancient Viarus Mountain (Spire Mountain), entered the process of urbanization in the Hellenistic period. As a result of the Northern Pisidia surveys conducted under the direction of Assoc. Prof. Dr. Fikret Özcan in the ancient city of Prostanna, it was observed that the construction in the city presented data far ahead of what is known and predicted. The town was discovered in 1948.⁷¹

Apart from Prostanna, the other Antic cities that remained within the borders of Eğirdir District during the Hellenistic period were Malos and Parlais. Parlais, whose present-day modern settlement is located on the town of Barla is about 30 km from Eğirdir as far north. During the Hellenistic period, Parlais was built as Colonia Iulia Augusta Parlais during the reign of Emperor Augustus. The settlement has been completely destroyed for today.⁷² Malos, on the other hand, is located on a hill in Göynücek, Sarıdris town of Eğirdir District.

⁷¹ Kızılyalçın, F., & Özcan, F. (2016). Prostanna Antik Kenti. *Journal of Süleyman Demirel University Institute of Social Sciences*, 1(23).

⁷² Baz, F. (2015). Merkezi Pisidia daki Bazı Roma Kolonileri ndeki İmparator Kültünün Varlığına İlişkin Değerlendirme. *PHASELIS*, (1), 261–275. <https://doi.org/10.18367/pha.15013>, p.269



Figure 3.15. A View Of Prostanna

(<http://gezilecekyerler.com/prostanna-harabesi/>, last visited on June 2019)

There are many antic settlements around Eğirdir district. The most important of these places is Pisidia Antiokheia, which is located in Yalvaç district around Lake Eğirdir. The town is located in the northernmost part of the Lake District. Although it is thought by researchers that the city was founded in 3rd century BC by Seleucids from the Hellenistic Kingdoms, Pisidia is understood to have been inhabited continuously since the Paleolithic period according to research.

The discovery of Antiokheia dates back to ancient times. In the 19th century, many European scholars and travelers visited Antiokheia Pisidia. Francis Vyvyan Jago Arundell, who served as a priest in İzmir between 1822 and 1834, was the first person to define the city and the Great Basilica in Antioch. In 1836 the British William Hamilton visited the Afyon Sultan Mountains and published his observations again in the 19th century in the French archaeological year. Between 1912-1914 and 1924-1927, W.M. Ramsey excavated most of the city. The city excavations are continuing today.

As can be seen in the study conducted by METU Faculty of Architecture, City Regional Planning students, there are many antic cities around the city district. Especially the fact that Pisidia Antiokheia, which has a very important place at the moment, was discovered and published in many publications in 1836, can be considered to increase the awareness of the region.



Figure 3.16. A study for antic cities around the city district. (METU/CRP Archive)

3.2. Urban Development of The City

The cities are formed by combining the social, economic, and physical layers with their unique qualities, and by adding the human factor, they gain a living, dynamic characteristic. The urban study is an independent field of study that analyzes the city with its past, present, and future. Urban history and urban morphology hold an important place in urban studies.

Historical sources, ancient plans, and maps were used to understand the historical settlement process of Eđirdir and the change in historical periods of the current urban macroform.

The historical events in the development process of the city are examined because they can create significant effects. As mentioned in historical research, the first settlement of the present city center is based on the Lydians. Since then, the Center has remained unchanged. It is known that the first zoning plan of Eđirdir was made in 1952. There was no plan before 1952. After 1952, development plans were prepared in 1966 and 1981. In this direction, it would be appropriate to examine the development of urban Macroform in 4 different periods as before 1952, 1952-1961, 1961-1981, and 1981.

As Eđirdir is a city wedged between the lake and the mountain, it had to spread linearly. Since the flatlands are also agriculturally productive, they have continued to be squeezed in the same way until today. Besides, the city has had to be rebuilt many times with the great fires it has experienced. These fires occurred in 1814, 1908 and 1959.

Before 1952, the city's settlements were Yeřilada, Kale, Seydim, Domed, Demirkapı, Katip, Hamam, Ađa, Poyraz and Hacıřeyh neighborhoods. Among these neighborhoods, Yeřilada and Kale Quarter are the oldest known settlements. With the fire that occurred in 1814, the mosque quarter was completely burned. Looking at the aerial photograph of 1941, it is observed that the burning mosque quarter has a grid layout. Assuming that it was very similar to the post-fire planning system of the Ottoman period, it could be considered that it was overhauled at that time. The most important event in urban development in those years was the arrival of the railway in 1912. As it is understood from the 1941 aerial photograph, the railway was built quite far from the city and there is no direct relationship with the city center.

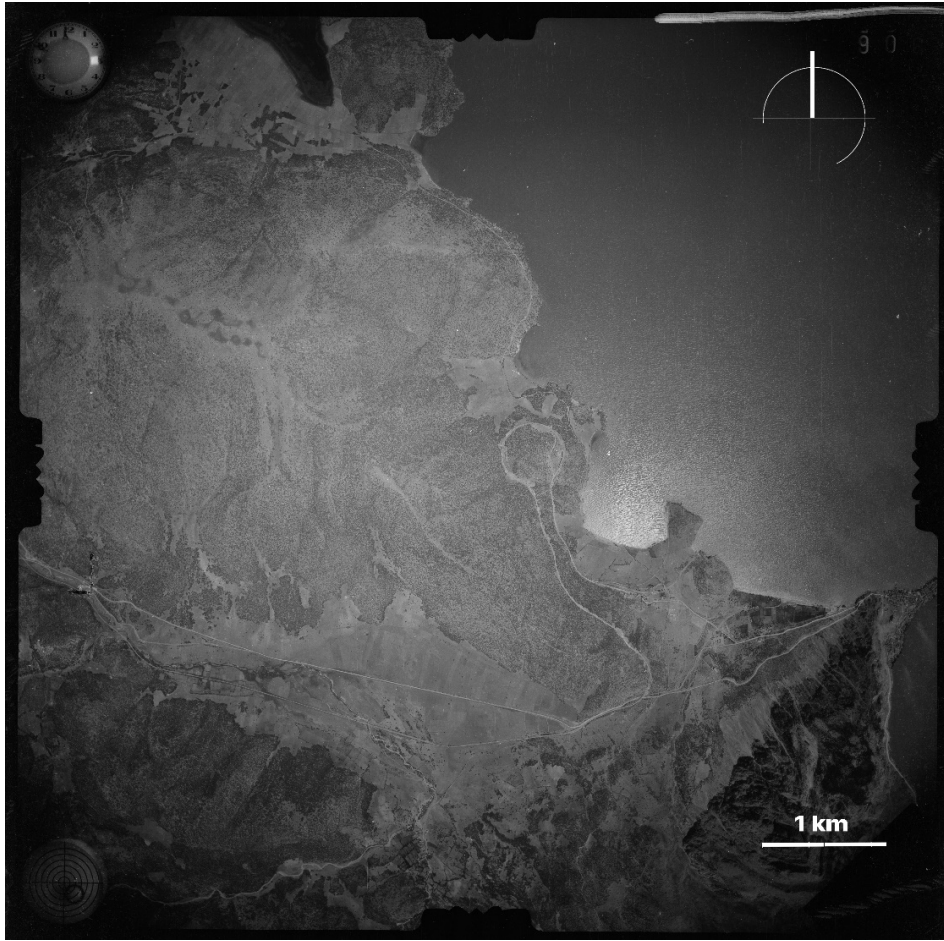


Figure 3.17: 1941 Aerial Photo (General Directorate of Mapping Archive)

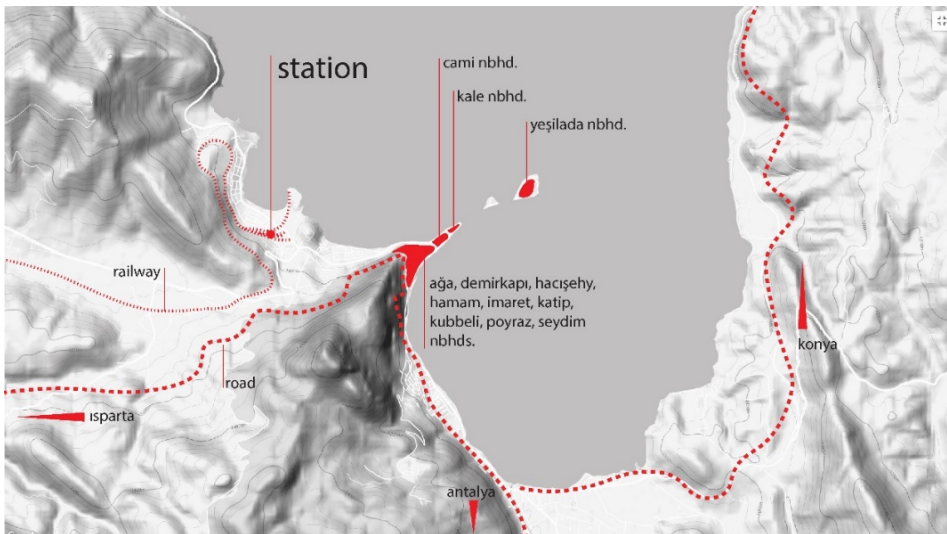


Figure 3.18: City Map of Eğirdir before 1950 (Sinan, 2019)

For the first time between 1952 and 1966, the city moved out of the historic center. In 1952, the building which was used as the former government building was turned into a hospital and in 1959 the mosque quarter was destroyed with the great fire and the new neighborhood was opened for reconstruction. The city started to move towards the northwest and northeast. As can be read from the 1960 aerial photography, summer settlement and construction began in Yenimahalle and Yazla neighborhood. There are units similar to the warehouse in the area where the railway is located.



Figure 3.19: Egridir between the years 1952-1966. (Akin Newspaper Archive)



Figure 3.20: 1963 Aerial Photo (General Directorate of Mapping Archive)

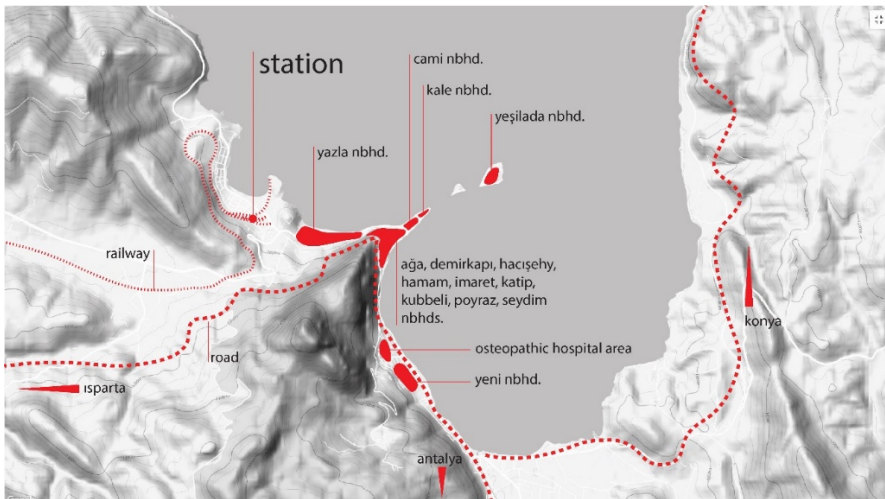


Figure 3.21: City Map of Eğirdir in 1952 (Sinan, 2019)

One of the most important changes between the years 1966-1981 is that, the green island and the island is connected to the city by filling in 1975. With the fillings, the city has moved away from its original texture. With the 1966 zoning plan, the density was kept the same in most of the central neighborhoods, housing was proposed to the west of the “Yenimahalle” and osteopathic hospitals, vineyard houses were proposed to the vineyards of the vineyards, touristic and camping areas were proposed between Altinkum beach and the railway, and the Isparta-Konya road was proposed. As seen in the 1975 aerial photograph, there was an increase in housing in new neighborhoods. In addition, the new Isparta road was opened and buildings increased in the military areas.



Figure 3.22: Egirdir between the years 1975-1980. (Akin Newspaper Archive)

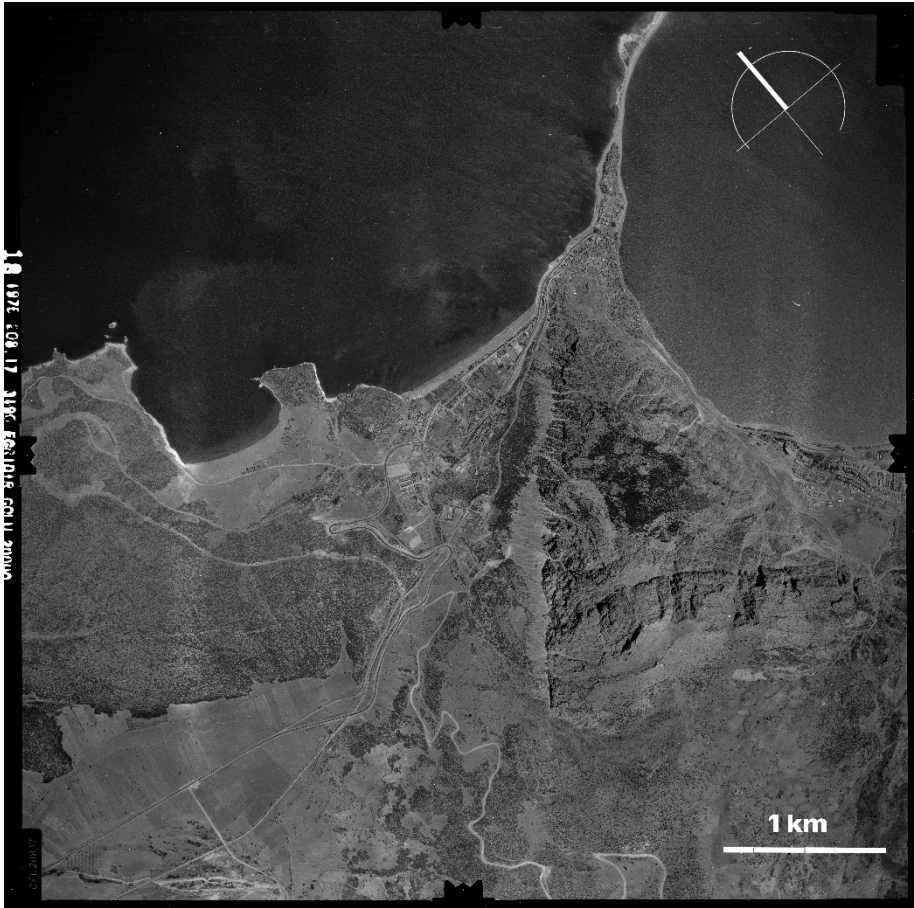


Figure 3.23: 1975 Aerial Photo (General Directorate of Mapping Archive)

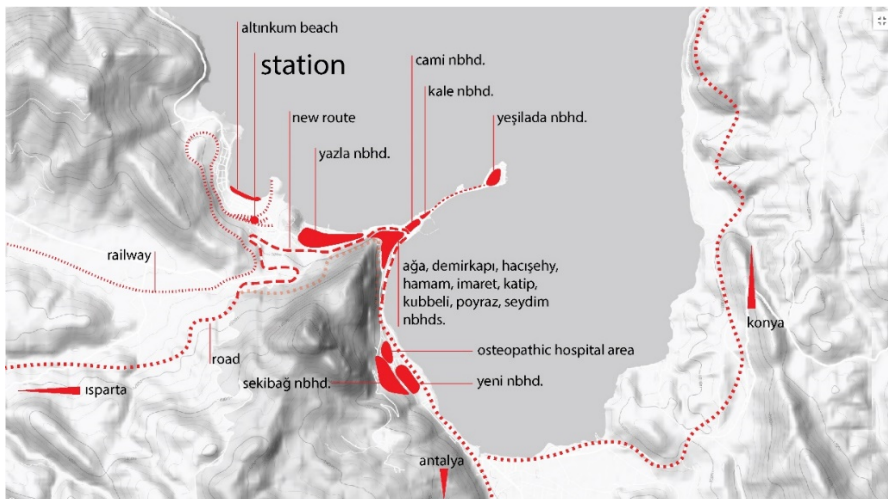


Figure 3.24: City Map of Eğirdir in 1960 (Sinan, 2019)

With the development plan of 1981, the city is expected to grow significantly. In order to preserve the historical texture in the central neighborhoods, new residential areas have been proposed. Boğazova, which is located to the east of the city, is designed as an agricultural area and Altınkum beach area to the west is designed as a residential area. In addition, the lakeside (military area) of the Altınkum region is equipped with cultural and recreational and tourist functions. As seen in the aerial photograph of 1981, the military grew and developed quite rapidly. The number of houses in Yazla neighborhood and “Yenimahalle” is increasing compared to previous years. There is no construction except the touristic facilities on the beach of Altınkum.



Figure 3.25: Eğırdır in 1980s. (Akın Newspaper Archive)

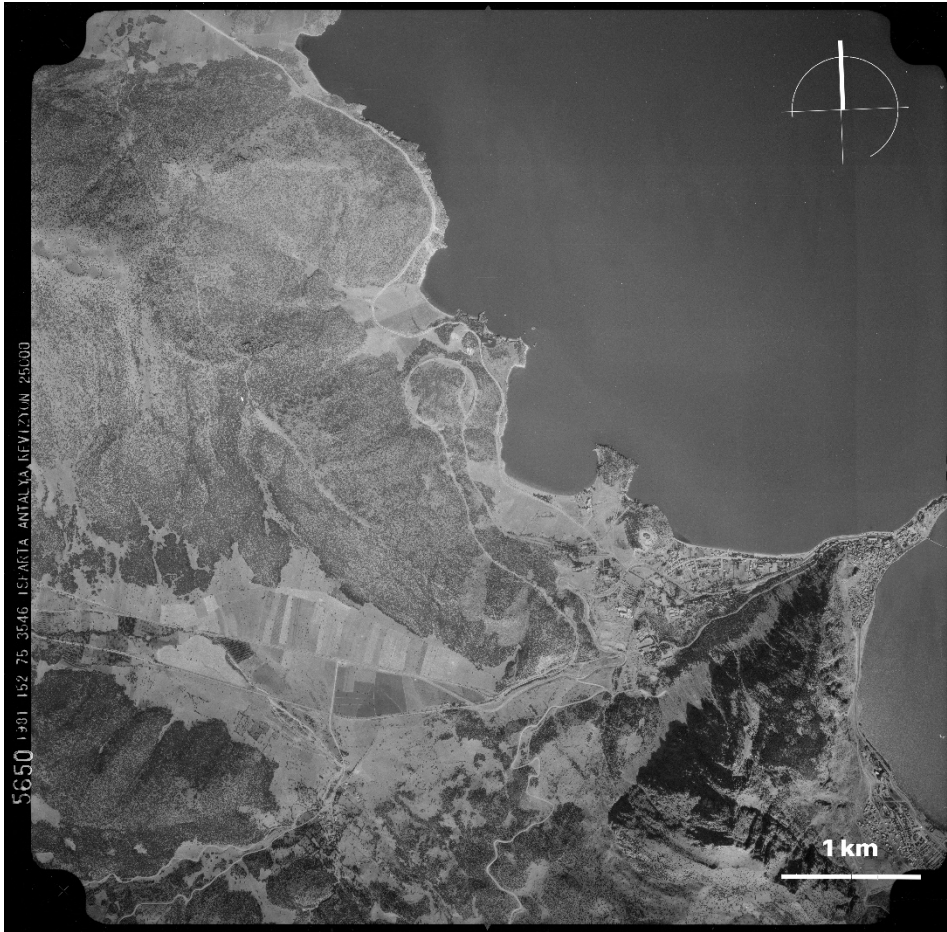


Figure 3.26: 1981 Aerial Photo (General Directorate of Mapping Archive)

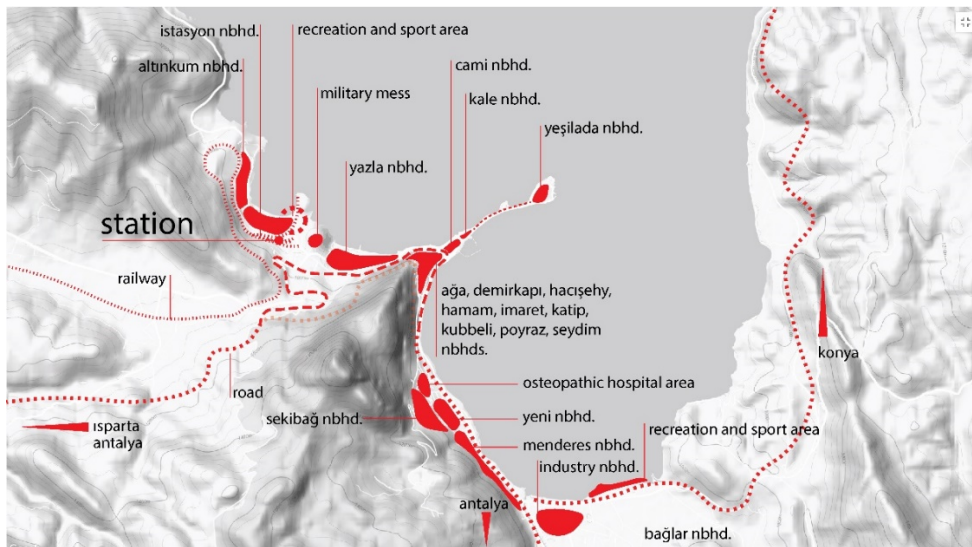


Figure 3.27: City Map of Eğirdir in 1981 (Sinan, 2019)

A serious change is understood from the 1992 aerial photograph, which is taken three years later. Housing density has increased very rapidly in the region of Altinkum. Multi-story apartment buildings were also started to be built around the station. Lodgings were also made in the area where the military had been overrun for training purposes. In addition, the roads that determine the parcels and between the Eđirdir-Barla have also become prominent. A 1992 aerial photo shows the situation closest to the city's current form.



Figure 3.28: Aerial photo of Eđirdir after 1990s. (Eđirdir Akın Newspaper Archive)

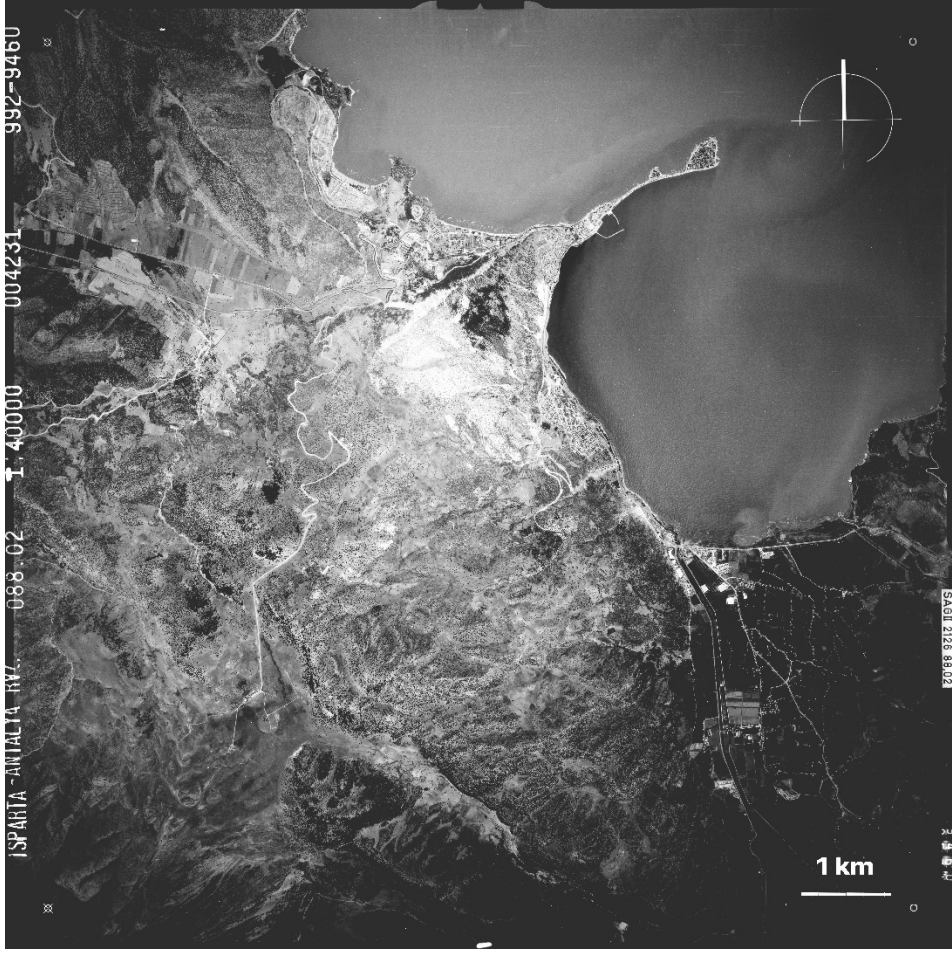


Figure 3.29: 1992 Aerial Photo (General Directorate of Mapping Archive)

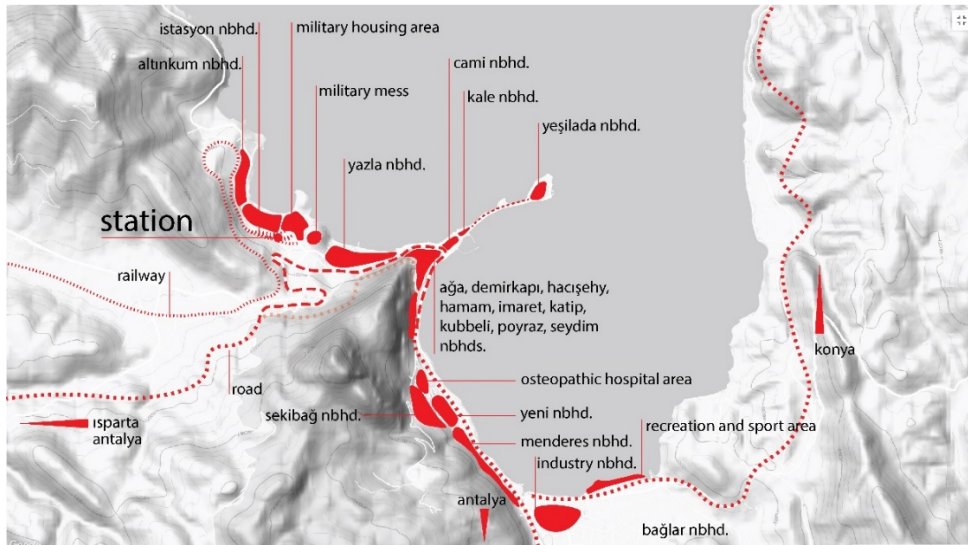


Figure 3.30: City Map of Eğirdir in 1992 (Sinan, 2019)

3.3. Cultural Heritage and Conservation Activities in Eğirdir

There are many cultural assets around Eğirdir. The oldest building in the city is the castle. Apart from the castle, there are monumental buildings built in different periods and dates located in various parts of the city.

Table 3.1: Cultural Heritage in Eğirdir (Sinan, 2019)

Name	Location	Year of Built And Registration	Name	Location	Year of Built And Registration
Mosques			Bridges		
Center Ağa Mosque	Center	? / 2007	Ottoman Bridge I	Barla	? / 1997
Ada Mosque	Yeşilada	? / 1975	Ottoman Bridge II	Barla	? / 1997
Çeşniğir Paşa Mosque	Barla	1376 / 1997	Roman Bridge	Barla	2nd. c. / 1997
Hızırbey Mosque	Center	1327 / 2009	Archeological Monumental Buildings		
Tombs			Church	Gökçehöyük	19th c. / 1997
Baba Sultan	Yazla	1358 / 1989	Temple	Sanidris	? / 1997
Şeyh Müslihittin	Yeşilada	? / 1975	Monumental Trees		
Şeyh Mehmet Çelebi	Yazla	? / 1989	Plane Tree I	Barla	19th c. / 1997
Churches			Plane Tree II	Barla	? / 1997
Aya Georgios Church	Barla	18th c. / 1997	Plane Tree III	Barla	? / 1997
Aya Stefanos Church	Yeşilada	8th. c. / 1975	Plane Tree IV	Barla	? / 1997
Madrasahs			Ancient Cities		
Dündar Bey Madr.	Center	13th c. / 1984	Prostanna	Akpınar	b.c. 1st c. / ?
Caravansaries			Mallos	Sanidris	b.c. 1st c. / 1985
Keyhüsrev Cara.	Yenimahalle	13th c. / 1990	Castles		
Fountains			Eğirdir Castle	Center	? / 1975
Fountaination I	Center	? / 2007	Ruins		
Fountaination II	Center	? / 2007	Kalatepe Church	Gökdere	- / -
Fountaination III	Center	? / 2007	Bağören Church	Bağören	- / -
Fountaination IV	Center	? / 2007	Ancient Road	Serçpil	- / -
Ahırcı Foun.	Barla	18th c. / 1997	Ancient Settlement	Akbenli	- / -
Akmescit Foun.	Barla	18th c. / 1997	Natural Protected Areas		
Baş Mahalle Foun.	Barla	? / 1997	Canada		- / 1975
Pazar Mahalle Foun.	Barla	? / 1997	Kovada Lake		- / 1975
Baths			Eğirdir Lake		- / 2012
Esmâ Sultan Bath	Center	? / 2007	Yeşilada		- / 1975
Bath 1	Barla	? / 1997	Kasnak Oak Natural Park		- / 1987
Bath 2	Kale	? / 1996			
Bath 3	Yeşilada	? / 2006			
Public Buildings					
Eğirdir Train Station	İstasyon	1907 / 2002			



Figure 3.31: Photos of some cultural assets in Eğirdir (Sinan, 2019)

Eğirdir Castle (1): There are two castle ruins, internal and external. Although the date of construction is not known exactly, it is thought that it was built by the King of Lydia Croesus at the beginning of BC. 4th century. Unqualified repairs and additions have been made to some parts of the ruined castle today.⁷³

Hızırbey Mosque (2): It is thought that the mosque, located opposite the Dündarbey Madrasa, was built during the Hamitoğulları period. Today, it continues to be used as a central mosque. It was registered in 2009.⁷³

Dündarbey Madrasah (3): It was built in 1302 during the Hamitoğulları period. The madrasah was used as a bazaar in 1959 and repaired in 1979 and 2018. Today it is used for cultural activities. It was registered in 2009.⁷³

Minaret (4): The minaret, built without foundation on the city wall between the Medrese and Hızırbey Mosque, is a very unique example with a two-meter-wide arch for passing the road. The minaret built during the Hamitoğulları period is still in use today. It was registered in 2009.⁷³

Eğirdir Caravansary (5): Caravanserai, which was built on the Caravan Road, which was built during the Seljuk period, is in ruins today. It was registered in 1990.⁷³

Ayastefanos Church (6): It was built in the second half of the XIX. century. Within the scope of the Church Lakes Region Research Project, it underwent the restoration, then the roof covering was renewed, the outer walls were made and the internal wooden parts were renewed. It was registered in 1975.⁷³

Baths: Although it is stated that there are five Turkish baths in total, there is only one Turkish bath that is standing and working today. It is thought that the Hamam of Dündarbey remained from the Hamitoğulları period.⁷³

⁷³ T.C. Isparta Valiliği İl Kültür ve Turizm Müdürlüğü. (2009). *Isparta Kültür Envanteri (1) (1st ed.)*. Konya: Bahçıvanlar Basım Sanayi A.Ş.

Babasultan Tomb (7): It was built in 1358 by Hamitoğulları. It has an orthogonal body and a conic roof.⁷⁴ It was registered in 1989.⁷⁴

Civil Architecture Examples: Despite the destruction of the city, it is possible to find examples of civil architecture. Traditional houses built of stone and wood are usually two-storey. Although some of the traditional residences in Yeşilada, Kale, and Ağa neighborhoods have survived to the present day, they are not in good physical condition.⁷⁵

Studies on the conservation of cultural heritage in Eğirdir started for the first time in 1975. In 1975, it was decided to register Yeşilada as a SIT site that needs to be conserved. Besides, 90 civilian architectural examples that need conservation and 7 different monumental buildings have been identified in Yeşilada. Ten of the residences were identified as the first group. Moreover, 4 trees were registered as natural monuments. Meanwhile, Canada was also registered as a natural site in 1975.⁷⁴

The Conservation Plan of Yeşilada was submitted to the Board on 22.07.1983 and on the same day, it was decided that 1/500 scale conservation plan and construction conditions were appropriate with the re-determination and registration studies in Yeşilada. The resolution also mentioned the importance of "realizing the Conservation and Tourism Development Zoning Plan as soon as possible by taking into account the island's tourism potential". In the conservation plan, concrete new functions have also been proposed for some buildings that need to be conserved.

With the change of the political order in 1984, the conserved site area was removed in Yeşil Ada and Can Ada, also the registration decision on 79 of the 90 immovable cultural assets was abolished. The number of immovable cultural properties in Yeşi-

⁷⁴ T.C. Isparta Valiliği İl Kültür ve Turizm Müdürlüğü. (2009). *Isparta Kültür Envanteri (1) (1st ed.)*. Konya: Bahçıvanlar Basım Sanayi A.Ş.

⁷⁵ Büyükdığan, İ. (2002). Kontrolsüz Gelişen Turizmin ve Fiziksel Planlama Yetersizliğinin Tarihi Dokulara Olumsuz Etkisi- Eğirdir Örneği. *Mimarlık Dergisi*, 303, 57–63, p.58

lada has decreased to 11 and the decision to register 5 monumental buildings has remained. In 1996, the Antalya Cultural and Natural Heritage Conservation Board declared the lake water surface as a first-degree natural site and a 300-meter shoreline on the land side as a third-degree natural site. However, the declaration of a 3rd degree natural site, which permits all kinds of construction in a controlled way, did not prevent the destruction of the most important beach of Eđirdir, especially in Yazla and İstasyon region, and transforming it into a dense and multi-storey second residential area. Intense construction and unconscious exploitation of lake water also destroy natural life.⁷⁶

In 2004, the Eđirdir Train Station and its land, which was closed for use in 2002, was declared as a cultural asset to be conserved first degree by the decision of Antalya Culture Conversation Regional Council.

In 2007, Yeşilada, which was previously abolished from its status as a conserved area, was declared as a 3rd degree natural site again with the decision of Antalya Culture Conservation Regional Council. There is a registration decision for 11 immovable cultural assets and 5 monumental buildings. In 2012, special provisions and conservation areas of Eđirdir Lake were determined. The Yeşilada and Canada Conservation Master Plan prepared in 2013 is pending unapproved.

⁷⁶ Büyükdıđan, İ. (2002). Kontrolsüz Gelişen Turizmin ve Fiziksel Planlama Yetersizliğinin Tarihi Dokulara Olumsuz Etkisi- Eđirdir Örneđi. *Mimarlık Dergisi*, 303, 57–63, p.61-62

CHAPTER 4

CASE: EĞİRDİR TRAIN STATION COMPLEX

In order to understand and analyze the main subject of the thesis, The Eğirdir Train Station complex, it is necessary to examine the Izmir-Aydın railway line, which is the last station next to the city. No doubt the railways are a big organization and it would be wrong to focus on its parts without addressing the whole organization. In this respect, the Eğirdir Train Station complex has been taken into consideration at different scales, but in a holistic manner, from the railway line where it is the last station to the buildings it contains.

4.1. Review on İzmir-Aydın Railway

Western Anatolia is one of the regions where trade between England and the Ottoman Empire is most intense. Money-commodity relations in the region were more developed than in other parts of the Empire, thanks to domestic and foreign trade, which had its roots back to the old times. Western Anatolia, where fertile arable land covers a wide area, was running through the Port of Izmir, where the Empire had relations with other cities and the outside world. Since antic times, Izmir was the starting point of the caravan route connecting the Mediterranean and Anatolia. Therefore, western Anatolia and Izmir were an important region for the capitalist states.⁷⁷

⁷⁷ Kurmuş, O. (1982). *Emperyalizmin Türkiye'ye girişi* (3rd ed.). Ankara: Savaş Yayınları. (Original work published 1974), p. 15

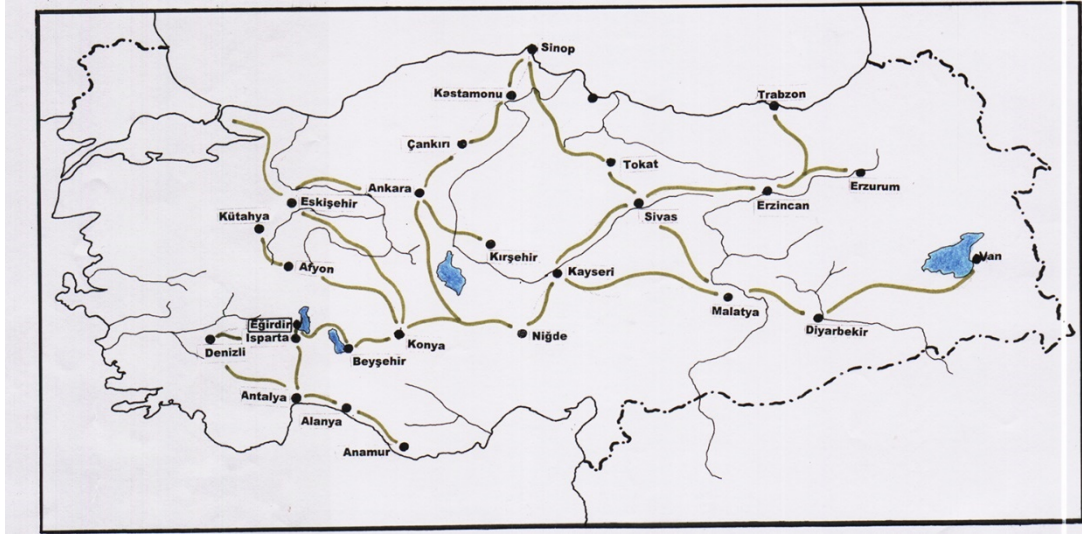


Figure 4.1: Caravan Road in Seljuq Period (METU/CRP Archive)

England was conducting its commercial activities in Western Anatolia in the 19th century through the Levantine companies established in Izmir. Charlton Whittall came to Izmir in 1811 and founded the company Whittall and its Partners. As a result of their contacts with the British capital, a strong commercial network was established in the region.⁷⁸

The region was aware that the connection of Western Anatolia to Izmir by rail by existing British Merchants would provide a great commercial advantage. However, at that time, the Ottoman Empire was in a difficult situation economically and it was impossible to invest in railways. Since railway investment requires a large amount of capital, it is very difficult for traders to make such an investment themselves. They, therefore, sought support from the British capital. James Whithall, who was awarded the title of “Sir” by the Queen for his services to Britain's foreign trade and had a very important position with the 48 branches he opened in Anatolia, said about the construction of the railway.

⁷⁸ Kurmuş, O. (1982). *Emperyalizmin Türkiye'ye girişi* (3rd ed.). Ankara: Savaş Yayınları. (Original work published 1974), p. 17

*“The first step should be to make the railways. These railways would be built by the British, operated by the British and would become the property of the British. They will be very profitable and they will make areas that have not been opened to agriculture so far very productive. The railway companies would become small independent republics.”*⁷⁹

This prediction of Whithall was realized by the Izmir-Aydın railway. The Aydın railway represents the most modern technology of its era, along with the fact that it was invested on a scale not seen in the Ottoman Empire until then. It has fulfilled a prerequisite for the development of agricultural labour force among the production areas, connecting İzmir to its rich hinterland and increasing the production of exported agricultural products. Although the Mersin-Adana and Haydarpaşa-Izmit railways were constructed by the British along with the İzmir-Aydın line, these lines were sold to the French. The only way in Anatolia under British rule is the Izmir-Aydın line. According to Orhan Kurmuş (1982), author of the book *The Introduction of Imperialism in Turkey (Emperyalizmin Türkiye’ye Girişi)*, the British boast the technical excellence and effectiveness of this line. They also see the Izmir-Aydın line as the superiority of the British Empire over the “ignorant and underdeveloped eastern countries” and call it the great work of English genius and skill.⁸⁰

On 23 September 1856, the Ottoman Empire granted the British a concession to “*create the company by will and build the railway*”. Later, it was stated that the contract signed between the Ottoman state and the company to be established would last for 50 years and that the company to be established must be an Ottoman company, which was bound by Ottoman law. Also, the important articles in the contract are as follows;

⁷⁹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 55

⁸⁰ Kurmuş, O. (1982). *Emperyalizmin Türkiye’ye girişi* (3rd ed.). Ankara: Savaş Yayınları. (Original work published 1974), p. 21

“Article 3: The Company will be able to construct 30 miles (48.28km) of railway and roadway on both sides of the railway.

Article 4-5-6-7: The necessary buildings for the railway, such as a pier, customs buildings and the like, will be constructed within four years.

Article 9: The company will lay a telegraph line along the railway and allocate one line to the Ottoman State.

Article 10: The land required for the railway and buildings will be loaned to the company for free, with savings being owned by the state.

Article 11: Free State-owned land in the places where the road will pass, those with the property the value will be appreciated and given to the company.

Article 12: The coal mines released from the land in the distance of 30 miles (48.28km) on both sides of the line will be given free of charge, while the those that are property will be sold.

Article 13: State-owned forests and homes that are in a distance of 30 miles on both sides of the line can be used out of charge.

Article 14: The company will state the two percent of its fund that worth 1.200.00 sterling as bail money, and if it fails to complete the road in 4 years this money will be left to the Ottoman Treasury.

Article 16: If the company halts construction and passes for a period of 6 months, the railway will remain with the government along with all coal mines and the company will not demand compensation. However, in case of difficulties in drilling existing tunnels along the road route, additional construction time will be given to the company.”

In addition to these articles, after the opening of the railway, the Ottoman Government would have the right to purchase the railway in agreement with the company in the

first 50 years and then every 20 years. A year later, the group that signed this agreement sold the company's concession right to another British group. Founded in 1857, the company of the new group was named as the “*Ottoman Railway Company from Izmir to Aydın*”.

The second company, which did not accept the plans of the first group and the British had more credit, brought three road engineers from London to Izmir and drew new plans. Most notably, James T. Wood, one of the three engineers from London, was a smuggler of historical monuments. The newly drawn route consists of 3 sections. These sections are respectively 70 km road between İzmir-Selatin Mountain, between Selatin Mountain and Aydın. The foundation of the line was laid on 16 November 1858 in front of today's Alsancak Train Station.⁸¹



Figure 4.1: Alsancak Train Station in İzmir (https://photos.wikimapia.org/p/00/05/06/81/86_full.jpg, last visited on June 2019)

The first section of the İzmir-Aydın railway was Izmir-Torbalı line in 1860. After six years, on 7 June 1886, the railroad reached Aydın and the first big target of the line was completed. A new concession contract was signed on 1 October 1866 to take the

⁸¹ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 56-60

railway further, which was completed as long as ten years. The accounts of the Ottoman government, which supported the Aydın Railway with the belief that production and exports would increase during this time, were correct and the regions through which the railway passed did not begin to develop and prosper in a serious sense. Along with this development, three important centers of the old caravan routes that followed Aydın attract attention. These centers are Nazilli, Denizli, and Dinar. Since the revenues between İzmir and Aydın were plenty, the company line, which received two more concessions in 1879 and 1888, managed to lead it to the inner parts of Western Anatolia. Nazilli in 1881, Dinar in 1889 and Denizli in 1891 to the last concession for the railway was given on October 6, 1906, for Dinar-Eğirdir⁸²

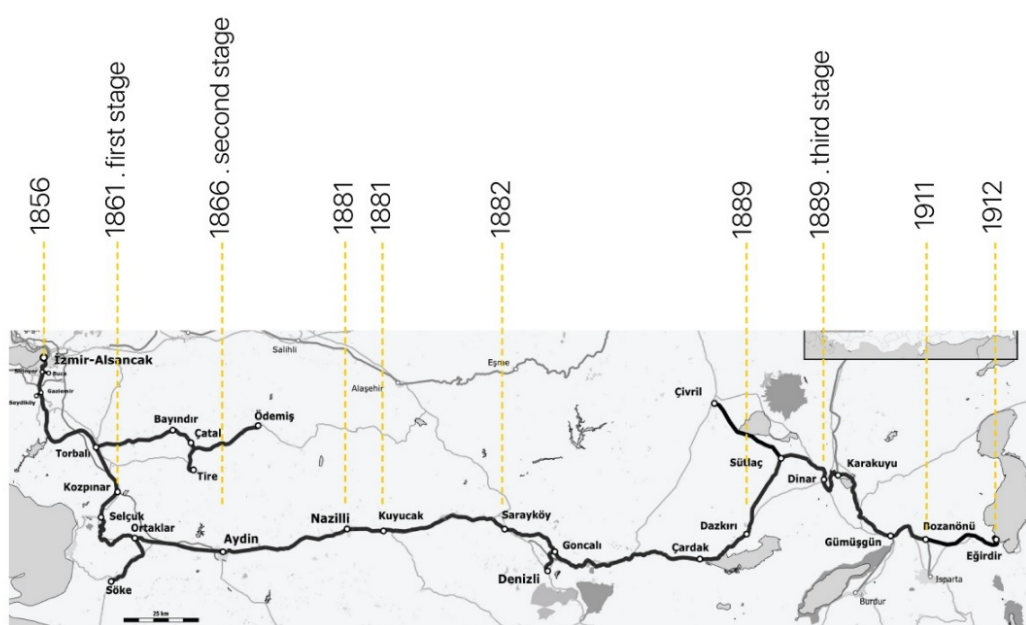


Figure 4.2 Chronological Development of İzmir -Aydın Railway (Sinan, 2019)

Apart from agricultural and mineral products, the railway routes of the stations are often archaeological sites of sites where the railways are laid. It is also known that the İzmir railway was used for the smuggling of historical monuments, although it is not clear whether this was planned in advance. 1856 İzmir-Aydın and the British engineers

⁸² Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 63-89

who came to Izmir to furnish Temdidi were the ones who started the real archaeological work in Anatolia. 1856 British engineers who came to İzmir to lay İzmir-Aydın and Temdidi are the ones who started archaeological works in Anatolia. British engineer Charles T Wood, who is also known as the initiator of the excavations of Ephesus, began to work with the railway passing by the antic city of Ephesus. Many artifacts found in that shed were taken away.

The railway relationship with other important places is as follows;

Kolophon Ancient City:	12 km from Develiköy station
Ephesus Ancient City:	3km from Selçuk station
Magnesia Ancient City:	Between the Stations of Reşadiye and Morali.
Priene Ancient City:	27km from Söke station
Miletos Ancient City:	27km from Söke station
Didyma Ancient City:	43km from Söke station
Tralleis Ancient City:	1 km from Aydın station
Nysa Ancient City:	2 km from Sultanhisar station
Pamukkale Ancient City:	1 hour with animal haulage at Goncali station it can be reached in minutes. ⁸³

The Izmir-Aydın Railway, which was confiscated by the Ottoman Empire during the First World War, was given back to the British again in 1922. It was nationalized in 1935. With the construction of the Izmir port in 1955, it becomes more important. After 1950, a lot of stations including the Eğirdir Railway Station were closed to use in addition to the fact that no major investment was made on railways due to the emphasis being placed on the highways.

⁸³ Atilla, A. N. (2002). *İzmir demiryolları*. İzmir: İzmir Büyükşehir Belediyesi Kültür Yayını, p. 63-89

4.2. History of Eđirdir Train Station Complex

Eđirdir Station Complex is located on the shore of the lake with its name, and with the roads and bridges built to reach the train, it has an important value today. 5.5 km before the station, the train descends on a slope of 28 per thousand and reaches the lake. This route is perhaps one of the most beautiful railway sceneries in the world.



Figure 4.3: A view of Station (Eđirdir Akin Newspaper Archive)

The location of Eđirdir Train Station constitutes a whole as regards the open spaces and the buildings in it. Built by the British in 1910, the station complex retains its area integrity and structural nature, although it is not used today. Therefore, the research on Eđirdir Train Station should be considered in a way to support this integrity.

Eđirdir Train Station is the last station of İzmir-Aydın Temdidi Railway line. On October 6, 1906, the British were granted concessions for the construction of the railway between Dinar and Eđirdir. Passengers coming from Eđirdir to Yalvaç, Akşehir, Antalya, and Konya frequently use Eđirdir Train Station. However, since 1914, the

French have acquired the "Eğirdir Shimendifer Enterprise Company" and the concession for transportation on Lake Eğirdir, and have made 24-hour transportation on the lake. Thus, Eğirdir has become exactly "transport and transportation point" but has also been the focus of Colonial States. At the beginning of 1921, 180-200 tons of transportation and shipment are carried out between the piers of Eğirdir-Höyük (Gelendost), Eğirdir Aşağıkaşıkara (Yalvaç), and Höyük-Aşağıkaşıkara. Today, dozens of warehouses have started to be built in the Stations and Altinkum neighborhoods to protect and protect the materials that come to Eğirdir.⁸⁴



Figure 4.4: Transportation on Lake Eğirdir (Bozkurt, 2007)

⁸⁴ Bozkurt, R. (2007). *Ulusal Kurtuluş Savaşı'nda Eğirdir: 1919-1922*. Eğirdir: Eğirdir Belediyesi, p. 180

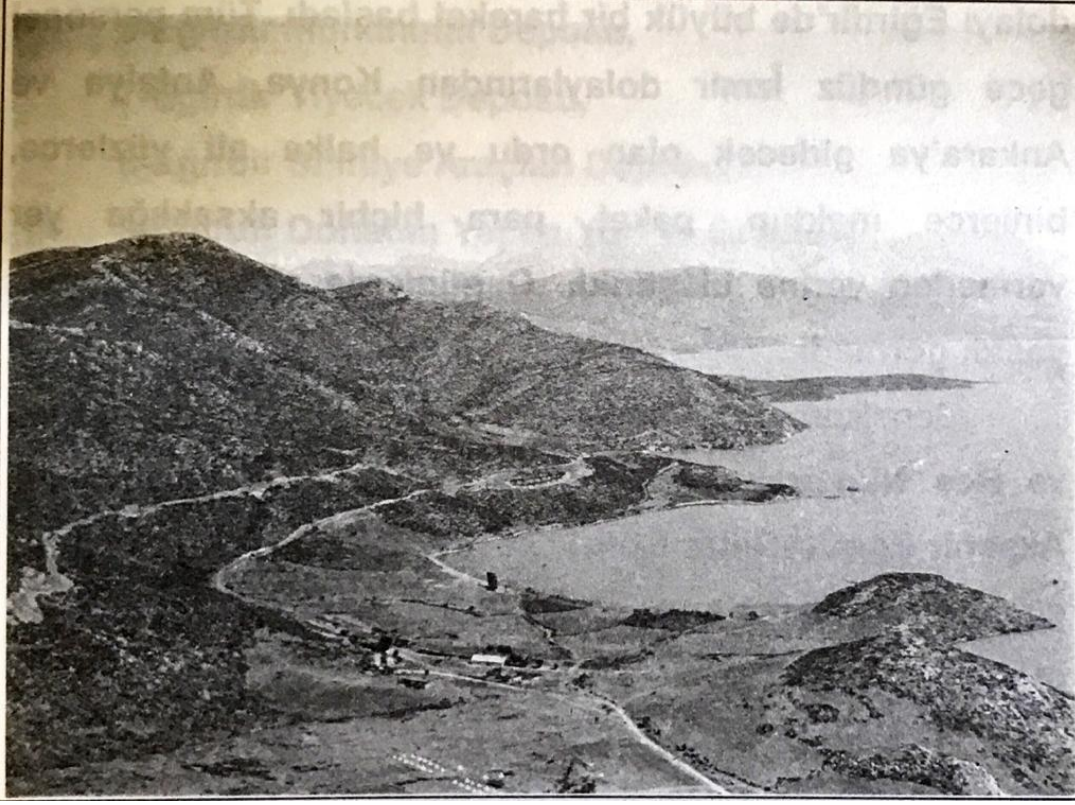


Figure 4.5: A view of Station in 1922(Bozkurt, 2007)

The Ottoman Empire joined the First World War that started in 1914 and took many precautions. These measures include the seizure of railways. The Ottoman Empire seized the management of Izmir-Turgutlu and extension lines, Izmir-Aydın and extension lines, Beirut-Damascus-Aleppo railways and extension lines, Mudanya-Bursa, Jaffa-Jerusalem Railways. The Ministry of Military Affairs of the Harbiye Ministry decided that those in Syria would be operated by the Hejaz railway administration and that the İzmir-Aydın and İzmir-Turgutlu railways would be operated by a military administration.⁸⁵

Although the Ottoman state confiscated the railways, foreign states wanted to continue their investments. Apart from the British and French in the region, the Italians also wanted to establish dominance in the region. Italians view the two port cities Antalya

⁸⁵ Özdemir, M. (2001). *Mütareke ve Kurtuluş Şavaşı'nın başlangıç döneminde Türk demiryolları : (Yapısal ve ekonomik sorunları)* (1st ed.). Ankara: Kültür Bakanlığı, p. 28-29

and Izmir as the entrance gates to Anatolia. Its main aim was to expand its trading grounds by turning the Mediterranean into an “Italian lake”. For this reason, on October 10, 1913, they were granted concessions for the construction of railroads in Antalya and its surroundings. Though they had concessions on the lines of Antalya-Burdur, Antalya-Alanya, Antalya-Kızılkaya, and Fethiye-Muğla, they aimed to extend the concession by adding the Antalya-Konya line to the list as well.⁸⁶

For the Italians who occupied Antalya on 28 March 1919, Konya Station on 24 April 1919, and Burdur on 28 June 1919, only Isparta and its surroundings are located in the area. On the other hand, after the British brought the railway line to Eğirdir in 1912 and the French started operating the Eğirdir Lake Shimendifer Operating Company in 1914, four states in the region started a struggle for interest and profits.⁸⁷

When the Greeks landed soldiers in İzmir with the date of May 15, 1919, the people whose houses and properties were plundered started to migrate in the masses in the direction of Aydın-Denizli-Isparta. They also came to Eğirdir by rail, which was one of the main means of transportation and communication at that time. In this way, the news of the occupation started in Izmir reached the region of Eğirdir and reactions began to occur. As a result of the reactions, a group of people living in Eğirdir went to the Nazilli front⁸⁷

During the years of national struggle, Eğirdir has become very important transportation and meeting place for the western front since the railway stopped at Eğirdir without going to Isparta. For this reason, Kuvai Nationalists in Eğirdir were given great duties and the people of the district were asked to do the first things. Besides, with the help of the Kuvai Nationalists who would be sent from Yalvaç, Karaağaç and Avşar

⁸⁶ Bozkurt, R. (2007). *Ulusal Kurtuluş Savaşı'nda Eğirdir: 1919-1922*. Eğirdir: Eğirdir Belediyesi, p. 8-17

⁸⁷ Özdemir, M. (2001). *Mütareke ve Kurtuluş Savaşı'nın başlangıç döneminde Türk demiryolları : (Yapısal ve ekonomik sorunları)* (1st ed.). Ankara: Kültür Bakanlığı, p. 28-29

and nearby places, all kinds of supplies from Gelendost-Höyük pier was loaded and brought to Eğirdir Station and finally sent to the front.

During the years of national struggle, Niğde, Antalya, Burdur and Isparta Mutasarrıflık (sancak) were connected to Konya between 1914-1919. With the railroad reaching Eğirdir, Konya-Eğirdir relationship became stronger and Eğirdir has always been under the influence of Konya.⁸⁸

During the war of independence, the Turkish army, which has been moving from defense to attack, has made a great “sad taaruz plan”, which includes Eğirdir.

(*) - The SAD symbol was a secret code which means that our army units were gathered in the vicinity of Sandıklı and that a fatal blow would be hit on the enemy. This password name was taken from the name of Sandıklı written in Arabic.⁸⁹

In line with this operation, the Chief of General Staff, on 15 October 1921, and the Western Front commander of the Western Front Commandership commanded to the First Army Commander in particular, "Akşehir, Eğirdir, Dinar Range should be established, and all the precautions should be taken against any possibility of the enemy to pass or destroy the Eğirdir-Baladız-Dinar Railway Line. In line with this decision, on 18 November 1921 “Eğirdir Range Line Command” Eğirdir Naval Detachment, Eğirdir Firewood Detachment, Eğirdir Range Hospital, Eğirdir Menzil Animal Hospital, Eğirdir Cleaning Station, Eğirdir Ammunition Warehouse, Eğirdir Food Warehouse, Eğirdir Equipment Construction Site, and Eğirdir News Receiving Center, consisting of units began to serve.⁹⁰

⁸⁸ Bozkurt, R. (2007). *Ulusal Kurtuluş Savaşı'nda Eğirdir : 1919-1922*. Eğirdir: Eğirdir Belediyesi, p. 57-186

⁸⁹ Belen, F. (1983). *Türk Kurtuluş Savaşı : Askerî, Siyasi ve Sosyal Yönleriyle*. Ankara: Başbakanlık Basımevi, p. 367-368

⁹⁰ Bozkurt, R. (2007). *Ulusal Kurtuluş Savaşı'nda Eğirdir: 1919-1922*. Eğirdir: Eğirdir Belediyesi, p. 57-186

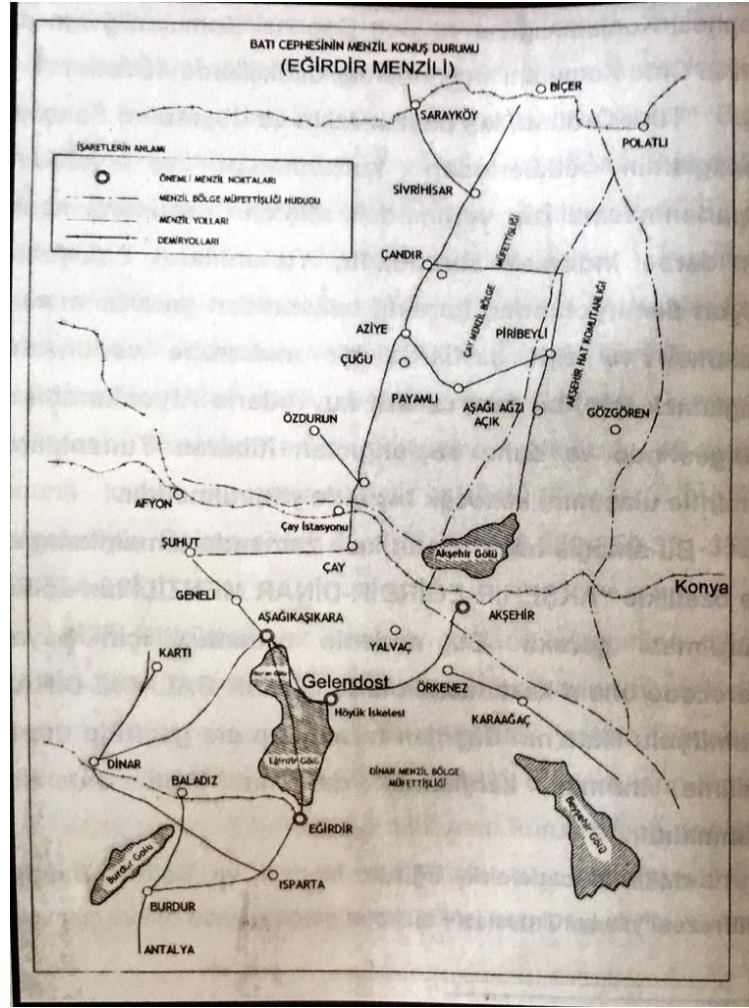


Figure 4.6: Eğirdir Range (Bozkurt, 2007)

The importance of Lake Eğirdir and Eğirdir Train Station has become more important in the national struggle with Eğirdir Navy. The shortest way for the materials and ammunition to be brought to the First Army on the Western Front by the Government of the Grand National Assembly is shown as “Antalya-Burdur-Baladız-Eğirdir-Şuhut Line. During the peak periods of the war, an engine capable of towing 80 tons for transportation was put into service in Eğirdir Lake, along with another 70 tons traction engine.⁹¹

⁹¹ Bozkurt, R. (2014, November 28). Milli Mücadele’de Eğirdir Menzili ve Eğirdir Bahriye Müfrezesi. *Akın Gazetesi*.



Figure 4.7: Towing boat on the lake. (Bozkurt, 2007)

Eğirdir Train Station, which contributed much to the defense of the country during the war periods, continued to contribute to the city and region where it was founded with the establishment of the Republic. As of 1931, the name of Eğirdir Naval Detachment was changed to the Navy Office. This is a picture of the station neighborhood in the 1940s. The depots, nursing homes and personnel dormitories belonging to Eğirdir Range Point in the Gulf are constructed.⁹²

⁹² Bozkurt, R. (2007). *Ulusal Kurtuluş Savaşı'nda Eğirdir: 1919-1922*. Eğirdir: Eğirdir Belediyesi, p. 57-186

In 1951, Engineer Yakup KALGAY wrote to the Railways Magazine about that in 1945 the Eđirdir station sent 3814 tons, and received 1818 tons of weight. The railway turned the city into a focal point in terms of transportation and provided the city's economic prosperity.

Eđirdir Train Station has made a significant contribution to the environment and economy of Eđirdir. With the ease of transportation thanks to the railway, Eđirdir has become a popular place for people. On the other hand, since Eđirdir has been located on trade routes since the past and the products grown in the settlements around Lake Eđirdir are mainly transported by rail, it has also placed it in the central position in freight transportation. The Eđirdir Station was left open only for military transport before it was completely shut down. In 1997, 8751 passengers and 125 tons of freight were transported from the Eđirdir Train Station, 2796 roadways and 145 tons of cargo were transported one year before it was closed.

According to the news published in Eđirdir Akin Newspaper in 1985, Afyon, Izmir and Pamukkale Motorized train were disconnected from Eđirdir and the newspaper interpreted this situation as the Anatolian province was cut off.

Eđirdir Railway Station, which was closed to freight and passenger traffic in December 2001, was registered as the "1st Group Conservation Cultural Heritage" with the decision of the Antalya Cultural and Natural Heritage Conservation Board dated 19.09.2004 and numbered 68. In 2009, it was included in the scope of privatization and transferred to the department of privatization administration.



Figure 4.8: Protesting the selling of the station (Eğirdir Akın Newspaper Archive)

4.3. Physical Features of Complex

Eğirdir Train Station Complex is located 470 km away from Izmir and about 30 km east of Bozanönü Railway Station at the intersection of Izmir-Aydın railway and Isparta. Bozanönü and Eğirdir Station are located between Kuleönü Station and 23 km away.



Figure 4.9: Kuleönü - Eğirdir Train Station (Sinan, 2019)

To the west of Eğirdir, approximately 2.5 km from the center, Eğirdir Train Station is located on the Eğirdir - Barla road and within 200 meters of the Isparta - Konya crossing. Although the southwest of the railway station is a mountain, there is a lake in the northeast. The railroad to Eğirdir reaches the lakeside from this sloping terrain. Today, the curves made on the railway line in order to proceed on the slope are defined as a first-degree natural conserved area. In the first years of the construction of the lake and the beachside, the mountainside of the railroad, which is now wooded as it is today continues to serve as the border between the city and wooded areas.



Figure 4.10: Eğirdir Train Station Complex (Sinan, 2019)

Located on a very difficult geography on the railway today, there are bridges of various sizes. The most important of these is the 9-footed steel bridge that once formed the skyline of the city.

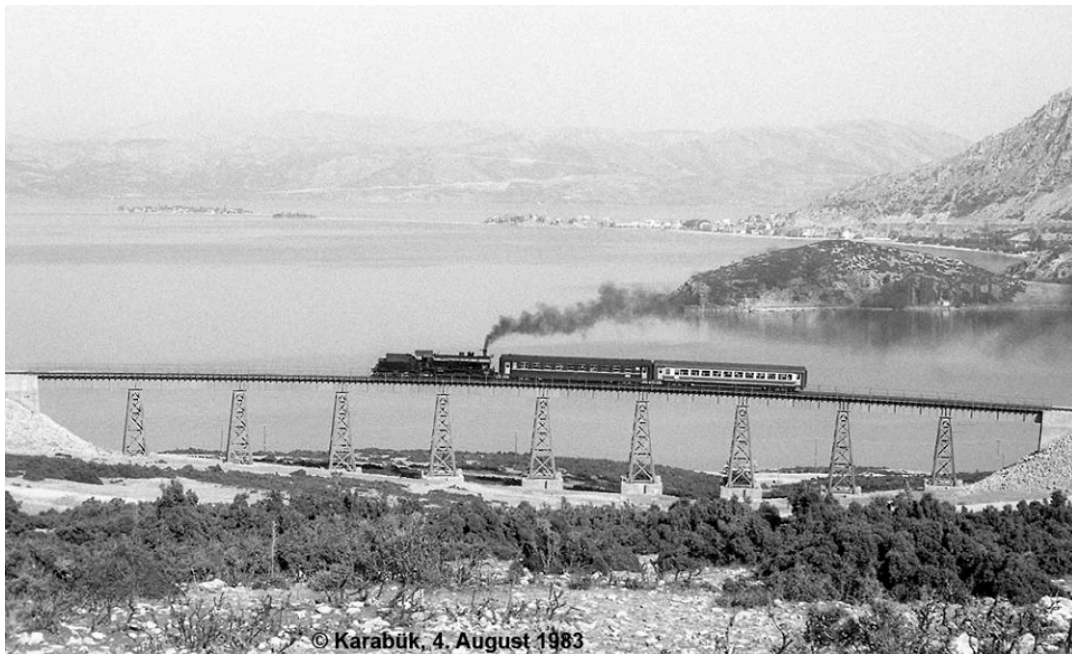


Figure 4.11: Steel Bridge (Eğirdir Akin Newspaper Archive)

Due to the climate and soil richness of Eđirdir, the region is rich in plant and animal diversity. This wealth is also observed in the land of Eđirdir Train Station. The area is an important part of the rich ecosystem with the trees planted while the station is set up, as well as the existing vegetation and living creatures.



Figure 4.12: Natural Beauty of Station (Sinan, 2019)

4.3.1. Station and Surroundings

The station and its environs have undergone serious changes since the day it was founded and the relationship it has established with the city has changed continuously. It would be appropriate to examine these relations through aerial photographs and zoning plans. Aerial photographs of 1943, 1953, 1960, 1963, 1975, 1981, 1992, 2014 and 2019 were taken from the General Directorate of Mapping for thesis study. The oldest visual source available is the 1943 aerial photograph.

In 1943, there are no signs of construction or construction except for one or two buildings in the train station and its environs. In those years, the road leading to Eğirdir Barla, which is currently present, ends with the station. The railways, on the other hand, appear to extend to the lake. There are three buildings in the area with the exception of the station buildings and the TMO building. Ömer Şengöl, the mayor of Eğirdir Municipality in 2014-2019, states that there is an inn at the entrance of the railway station. Building A is thought to be the inn mentioned by Ömer Şengöl. No information about the building of B and C could be reached. (see Figure 4.17 and Figure 4.18)



Figure 4.13: Eğirdir in 1940s. (Eğirdir Akın Newspaper Archive)

In 1953, it was observed that the buildings increased especially in the military and forestry directorate field. In 1943, the buildings at the point where the railway meets the lake are not visible (C). Besides, one of the two buildings on the way to the station is not visible, either (B). (see Figure 4.20 and Figure 4.21)

In 1963, the military construction continued to increase. It is also observed that some recreational arrangements were made at Altınkum beach in these years. The building on the way to the station is not visible (B). Another important change throughout these years is the construction of a small building group in the south of the railway station buildings. Ömer Şengöl states that this building group belongs to The Pancar District Directorate. It was observed that Eğirdir Barla road was open capillary in 1960 aerial photo. (see Figure 4.23, Figure 4.24, Figure 4.26 and Figure 4.27)



Figure 4.14: Station Area in 1960s. (Eğirdir Akın Newspaper Archive)

It seems that the new Eğirdir - Isparta road was built in 1975. It is also seen that the boundaries of the military field are better defined and enlarged by the road. In addition, the complex of the Forestry Directorate is expanding. (see Figure 4.29 and Figure 4.30)



Figure 4.15: Station Area in 1970s. (Eğirdir Akin Newspaper Archive)

In 1981 it appears that the Eğirdir-Barla road was widened and laid out. Along with this road, the relation between the station and the lake was damaged. On the coastal parts of the road, military lodgings were built. The construction of the military casino is also shown in this photo. The traces of railroads to the lakeside are close to disappearing on the lakeside of Eğirdir-Barla road. (see Figure 4.32 and Figure 4.33)

A completely different settlement occurred in 1992. The housing texture has emerged prominently since the 1981 zoning plan was implemented. Railway Street, which is designated as the ring road in the zoning plan, was opened and the station land was completely removed from the neighborhood. Reconstructed units of Altinkum beach have been completed. Construction has also begun in the southern part of the station. Military casinos and roads are also almost complete. (see Figure 4.35 and Figure 4.36)

When it comes to the year of 2014, we can see that the surrounding roads and buildings and the Eğirdir Train Station are far from the original context. On the lakeside of the Eğırdır-Barla road, with the increase of military housing and environmental regulations, railways became illegible and the boundary defining elements such as the wire wall drawn to the plots disappeared completely. It is observed that the buildings belonging to the soil products office and the beet regional directorate were destroyed. Only the garage building of the Beet District Office was replaced by molded buildings. The District Police Department was built instead of the land crops. (see Figure 4.38Figure 4.39)



Figure 4.16: Current Situation of the district. (Sinan, 2019)

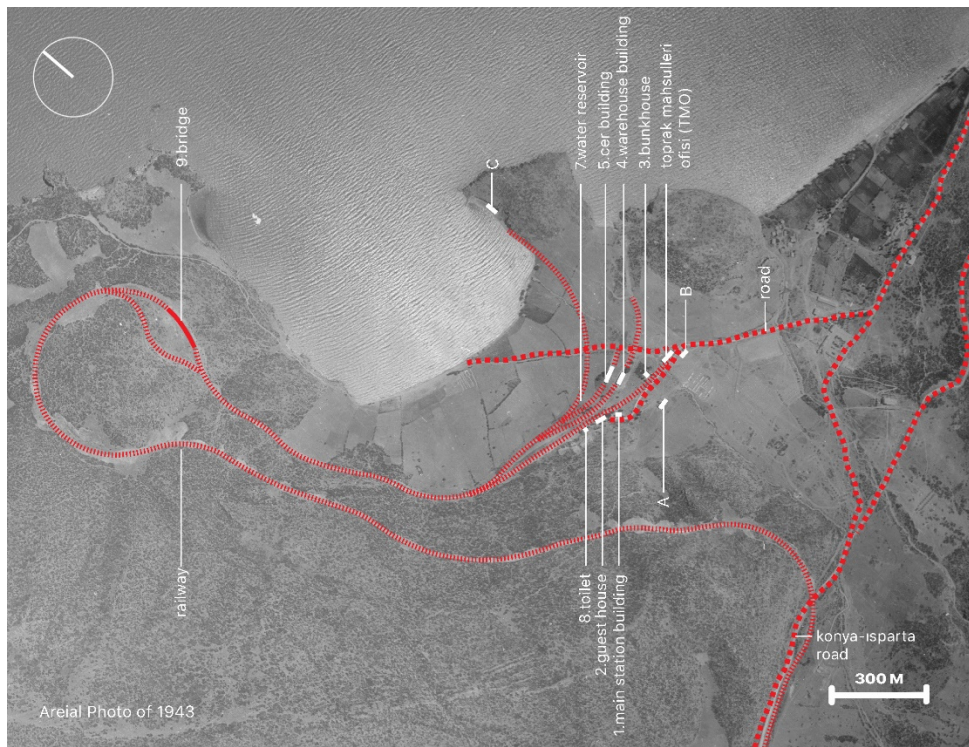


Figure 4.17: First Analysis of 1943 (Sinan, 2019)

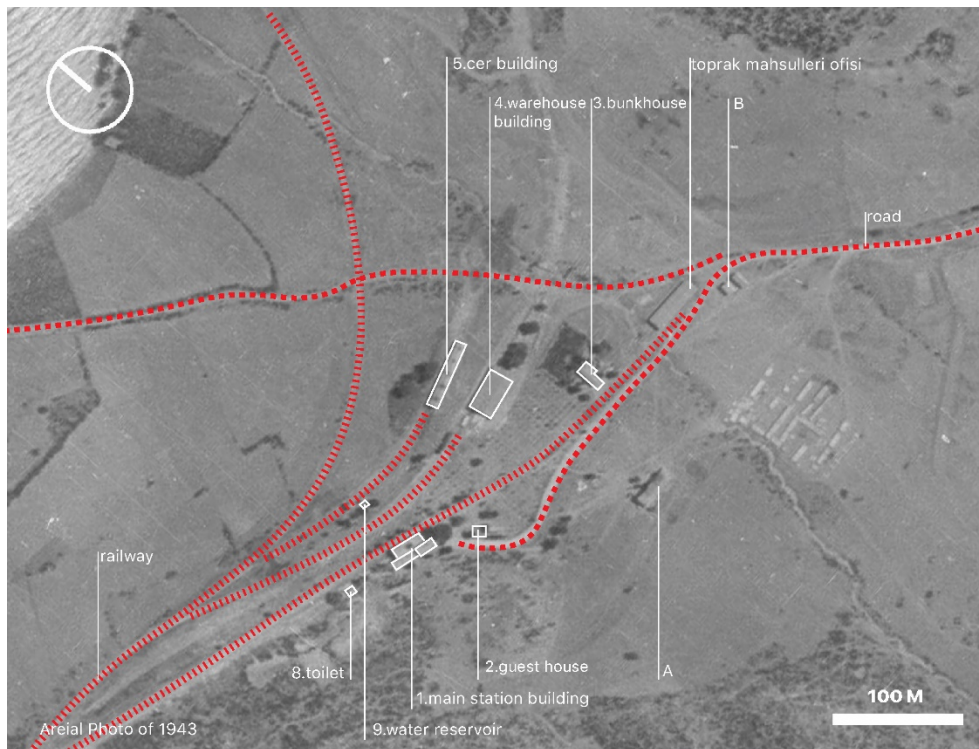


Figure 4.18: Second Analysis of 1943 (Sinan, 2019)

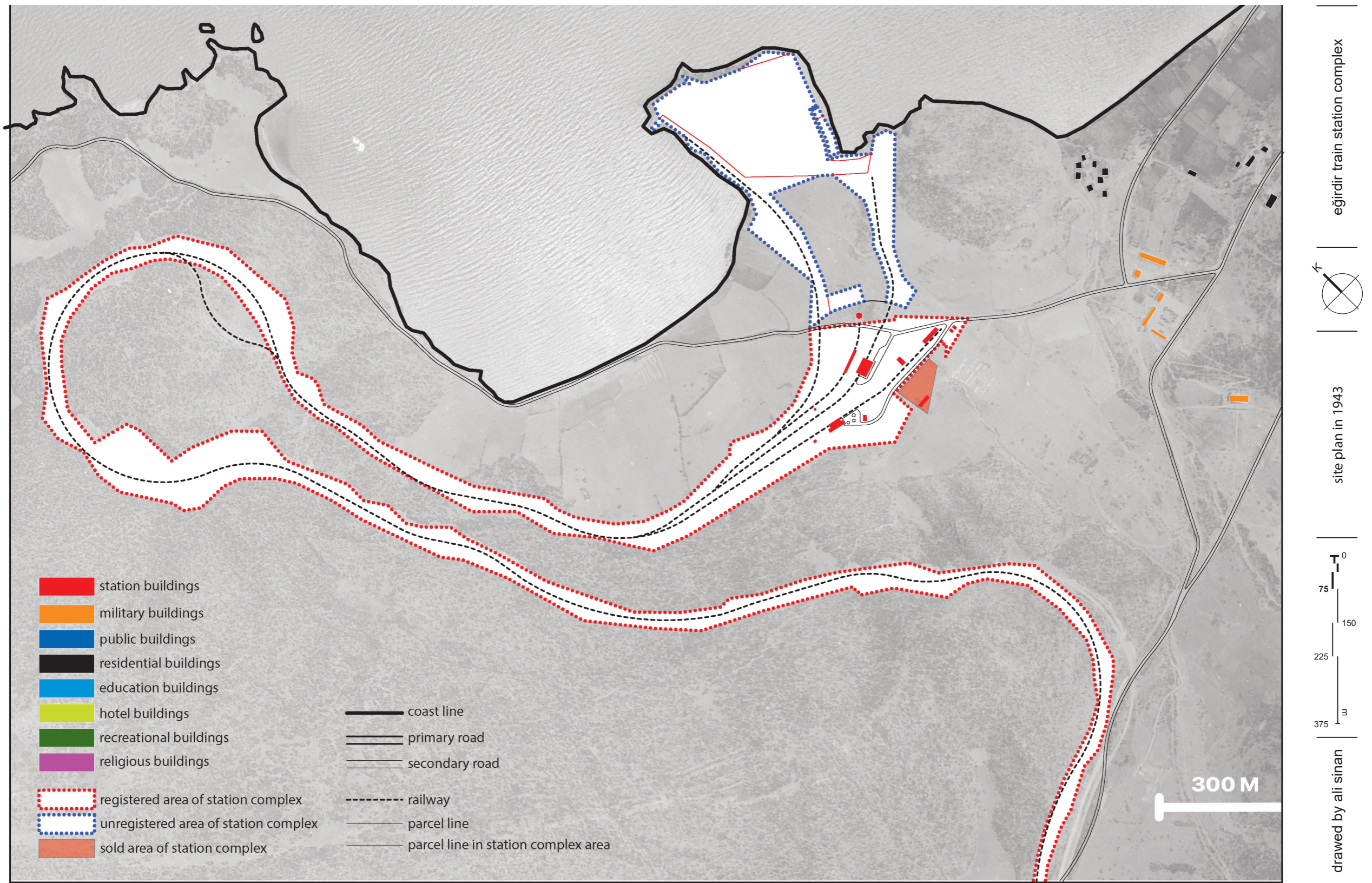


Figure 4.19: Site plan in 1943 (Sinan, 2019)

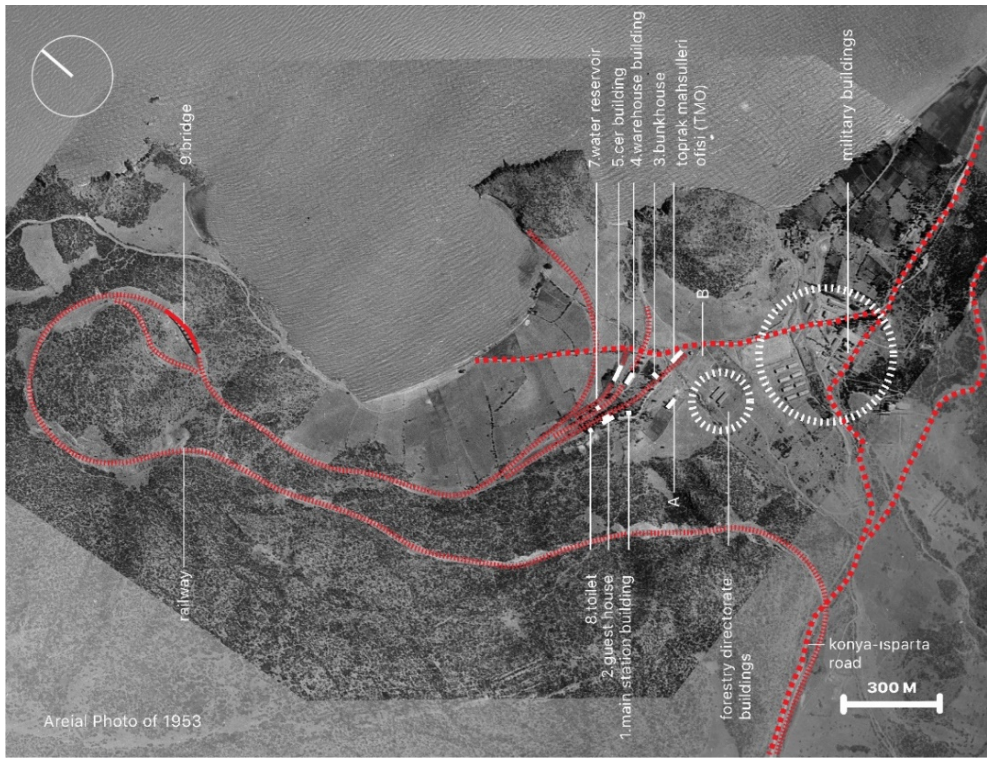


Figure 4.20: First Analysis of 1953 (Sinan, 2019)



Figure 4.21: Second Analysis of 1953 (Sinan, 2019)

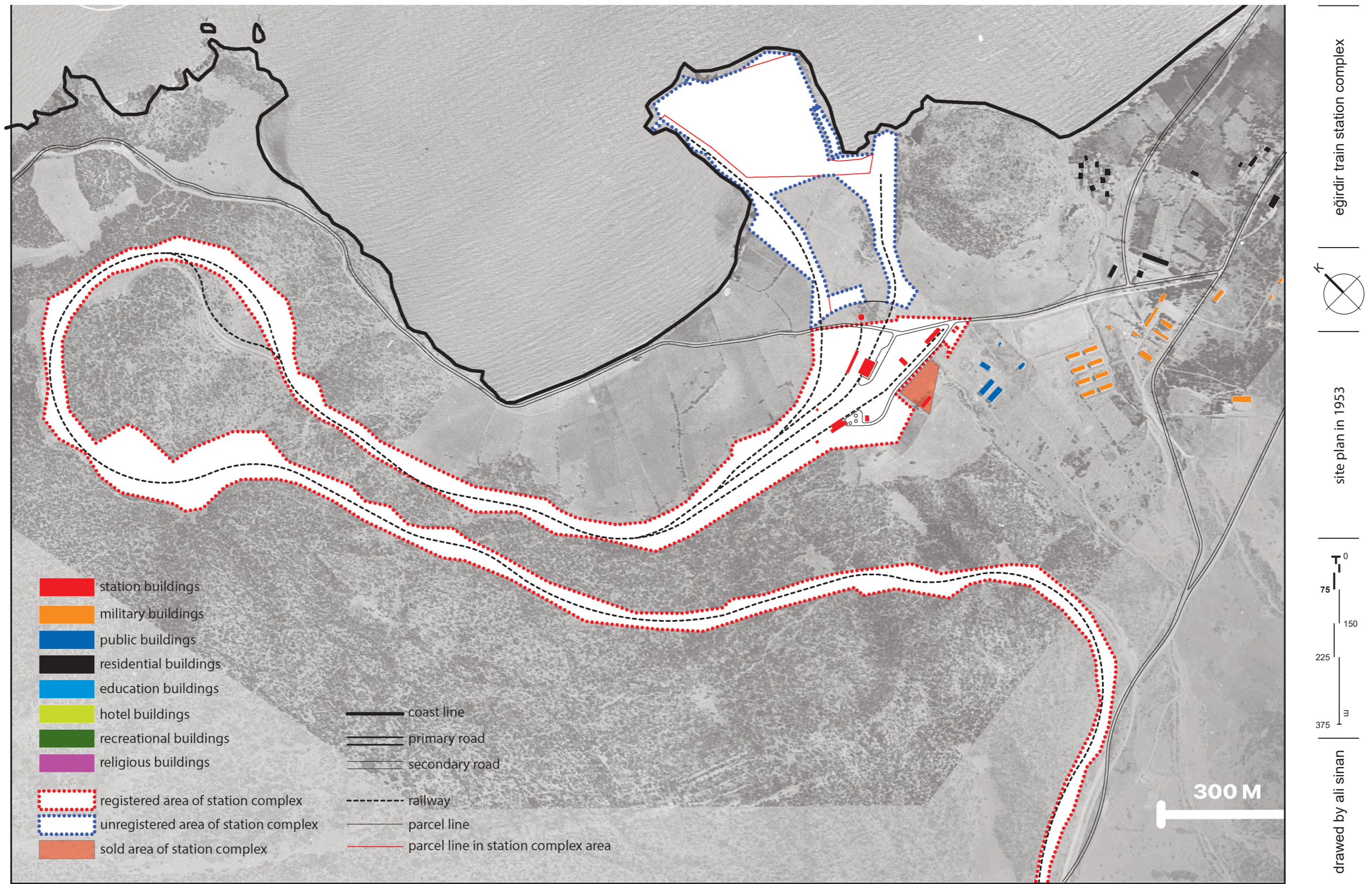


Figure 4.22: Site plan in 1953 (Sinan, 2019)

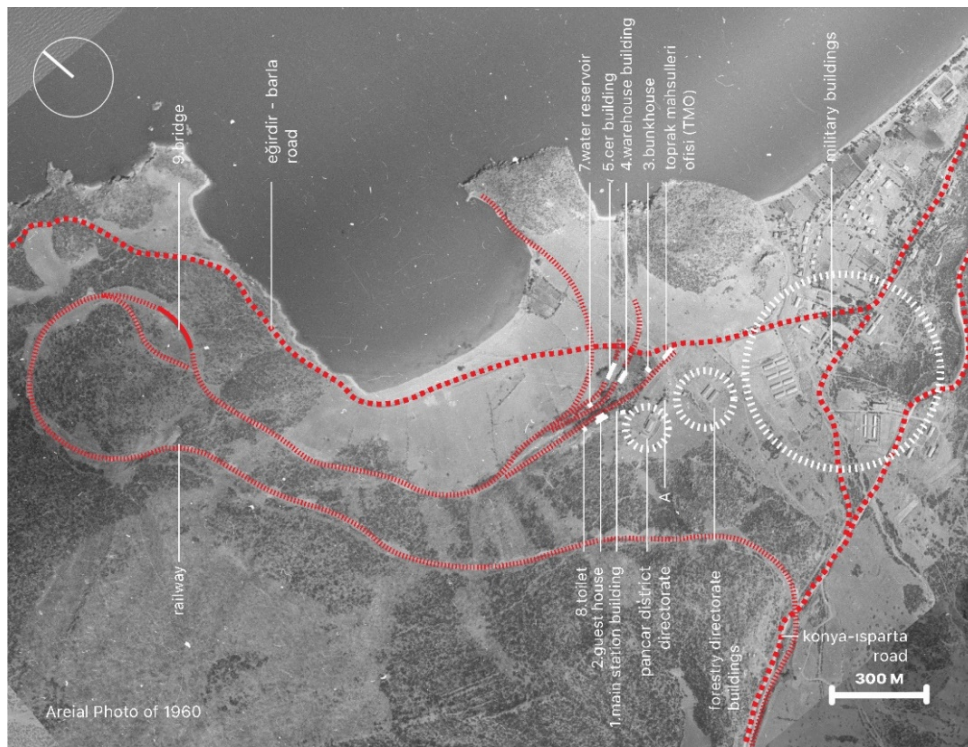


Figure 4.23: First Analysis of 1960 (Sinan, 2019)

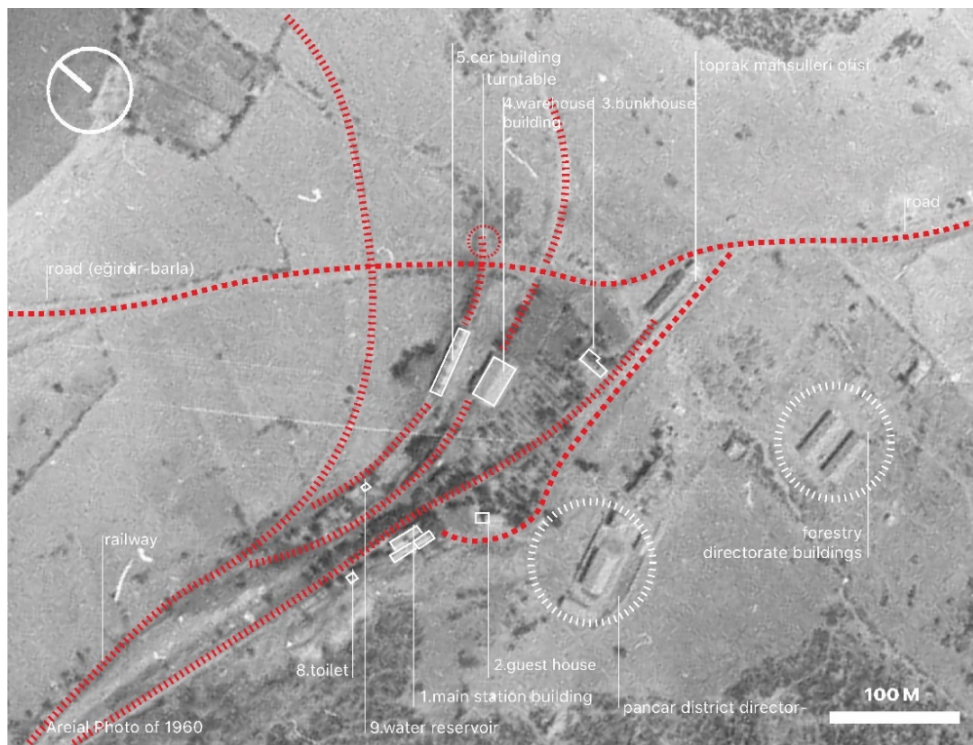


Figure 4.24: Second Analysis of 1960 (Sinan, 2019)

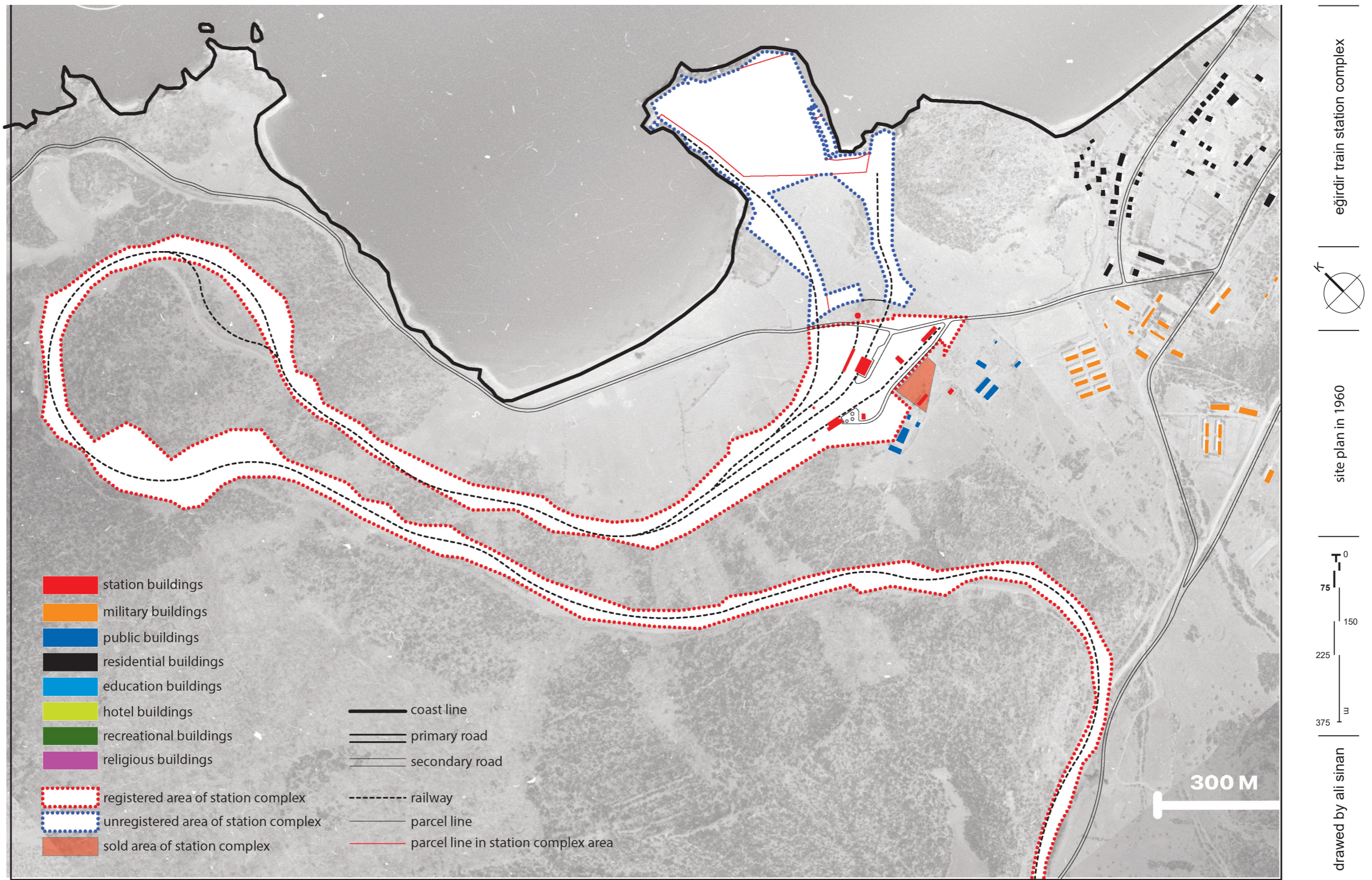


Figure 4.25: Site plan in 1960 (Sinan, 2019)

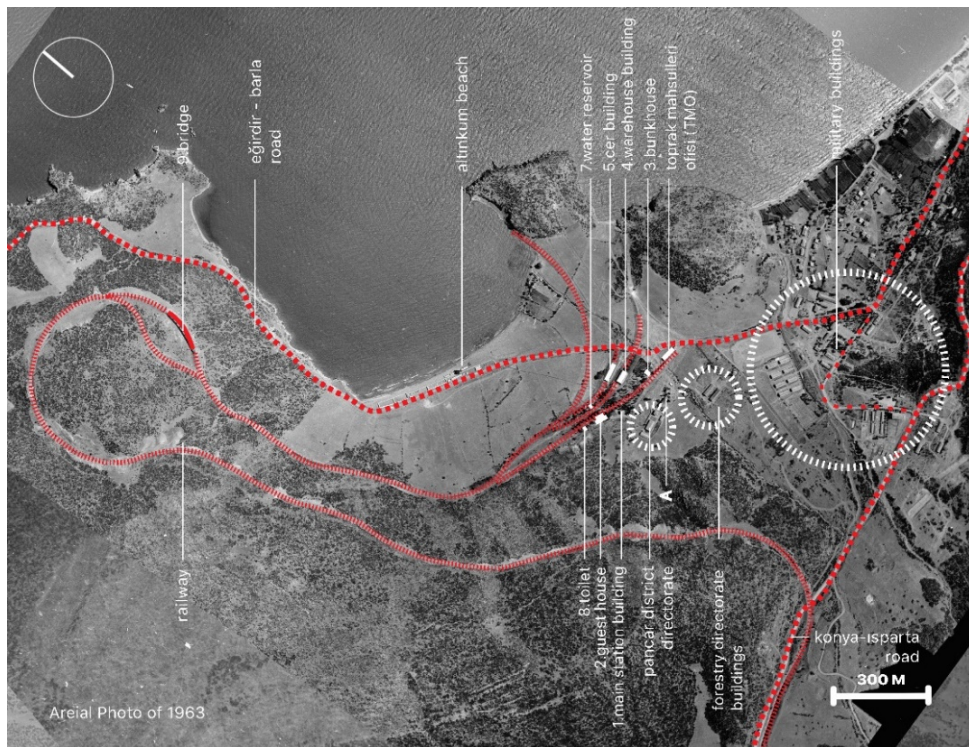


Figure 4.26: First Analysis of 1963 (Sinan, 2019)



Figure 4.27: Second Analysis of 1963 (Sinan, 2019)

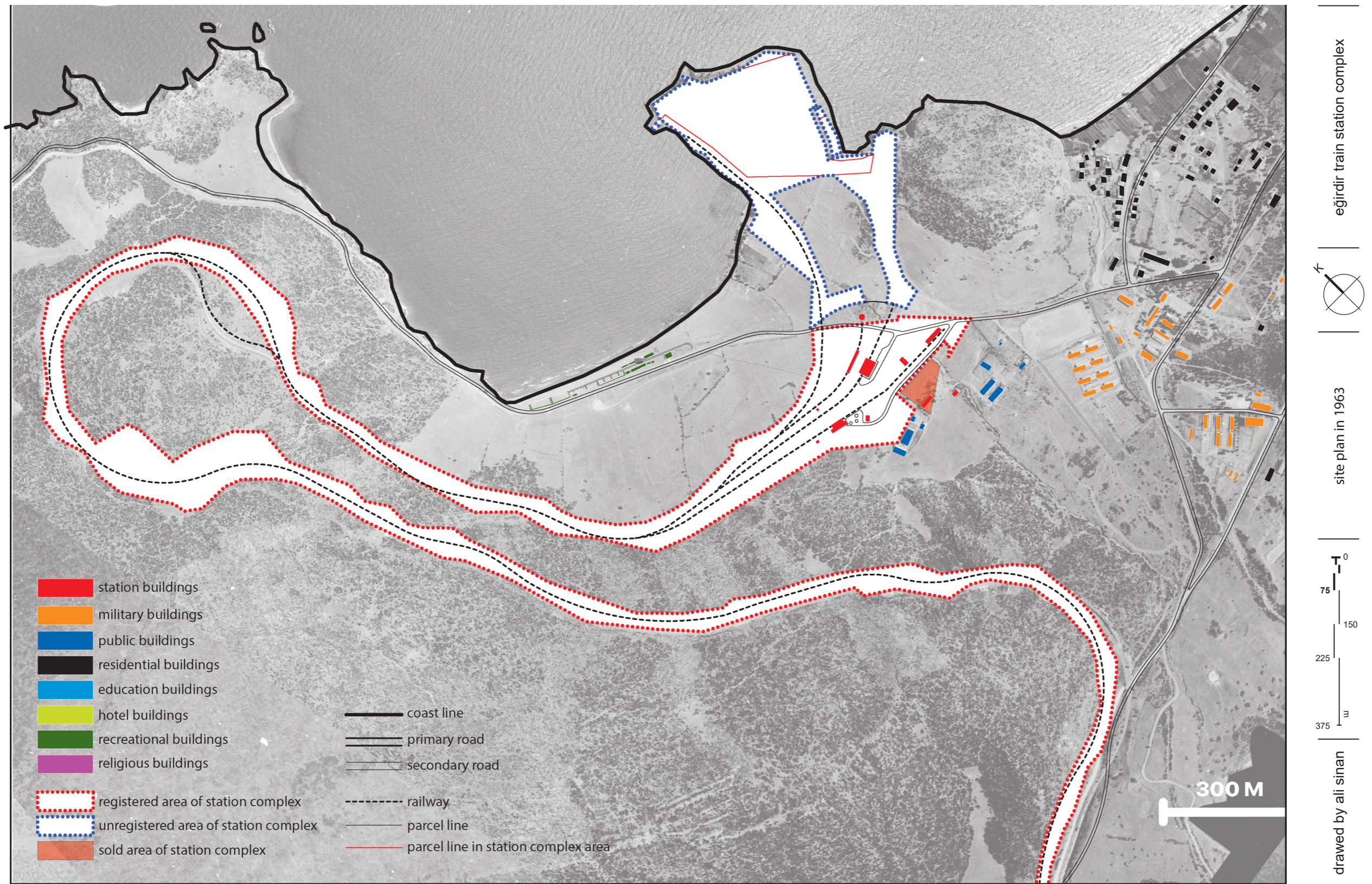


Figure 4.28: Site plan in 1963 (Sinan, 2019)

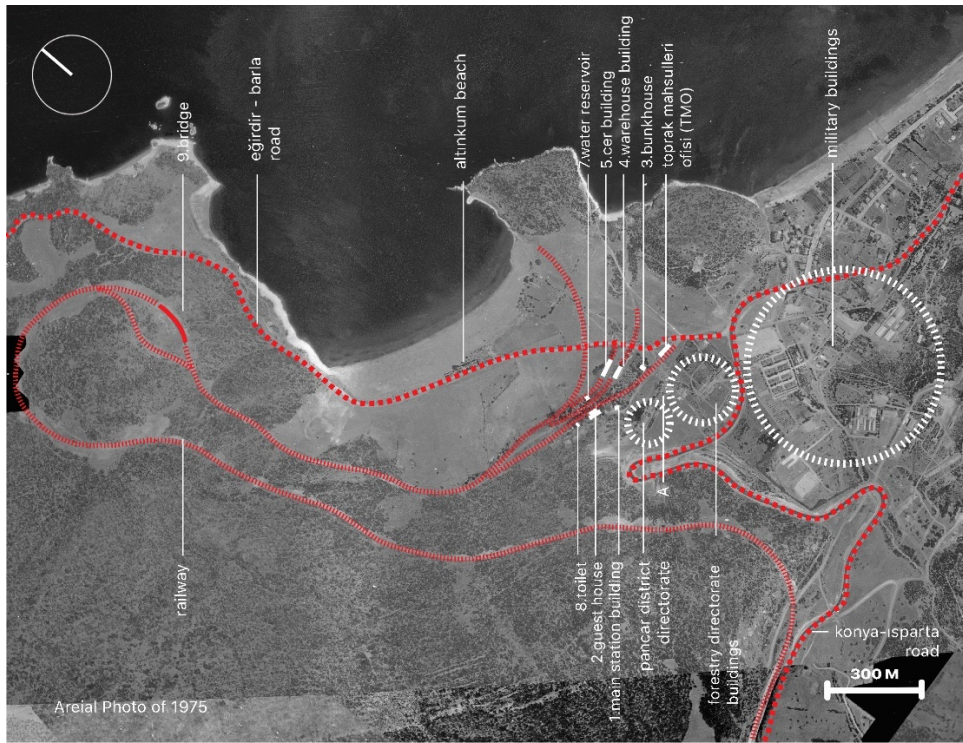


Figure 4.29: First Analysis of 1975 (Sinan, 2019)

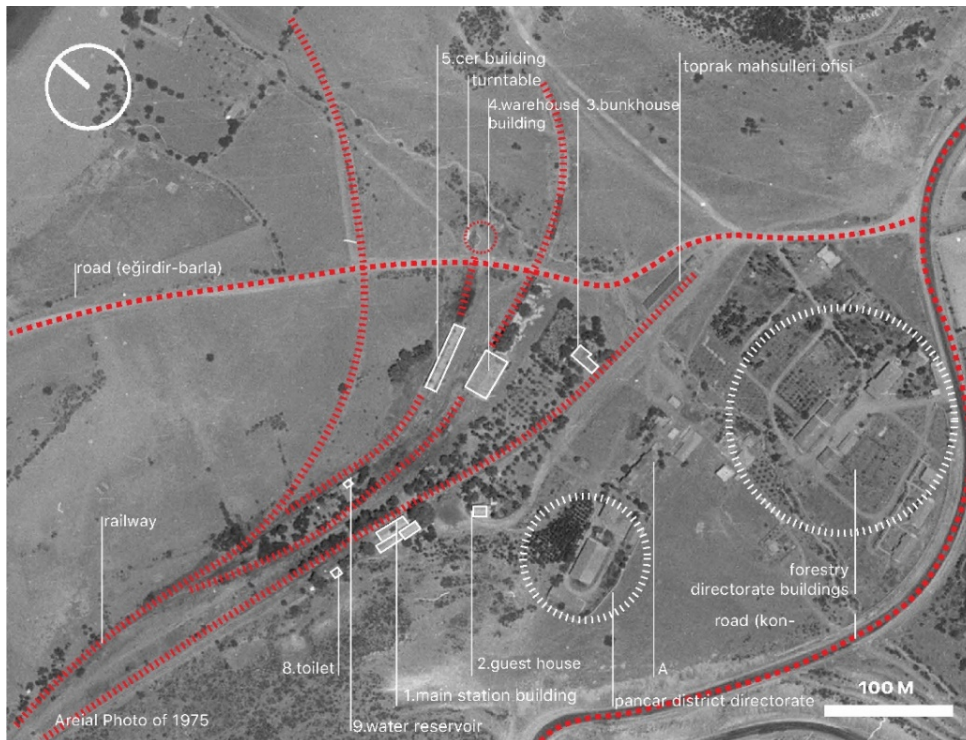


Figure 4.30: Second Analysis of 1975 (Sinan, 2019)

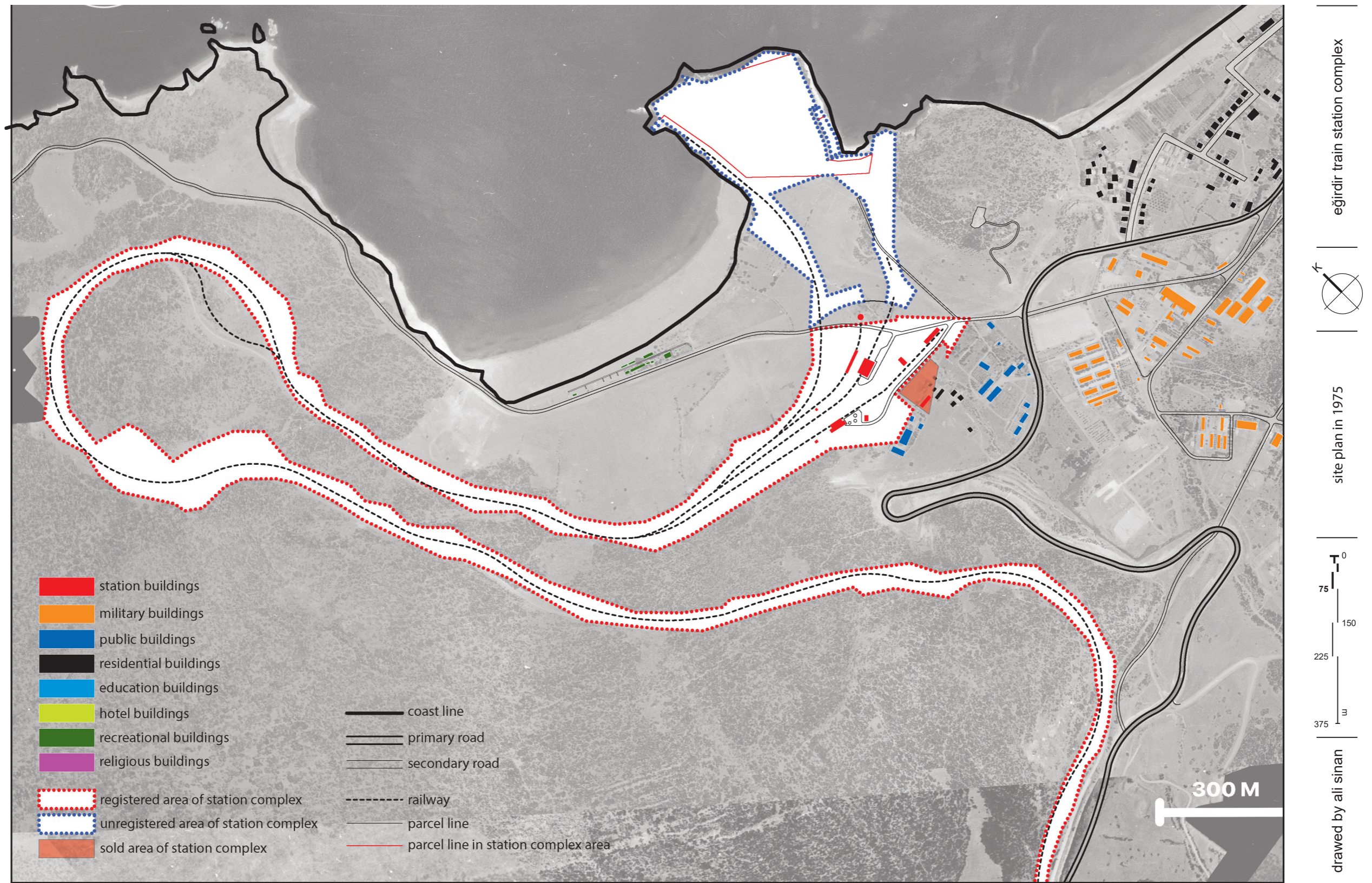


Figure 4.31: Site plan in 1975 (Sinan, 2019)

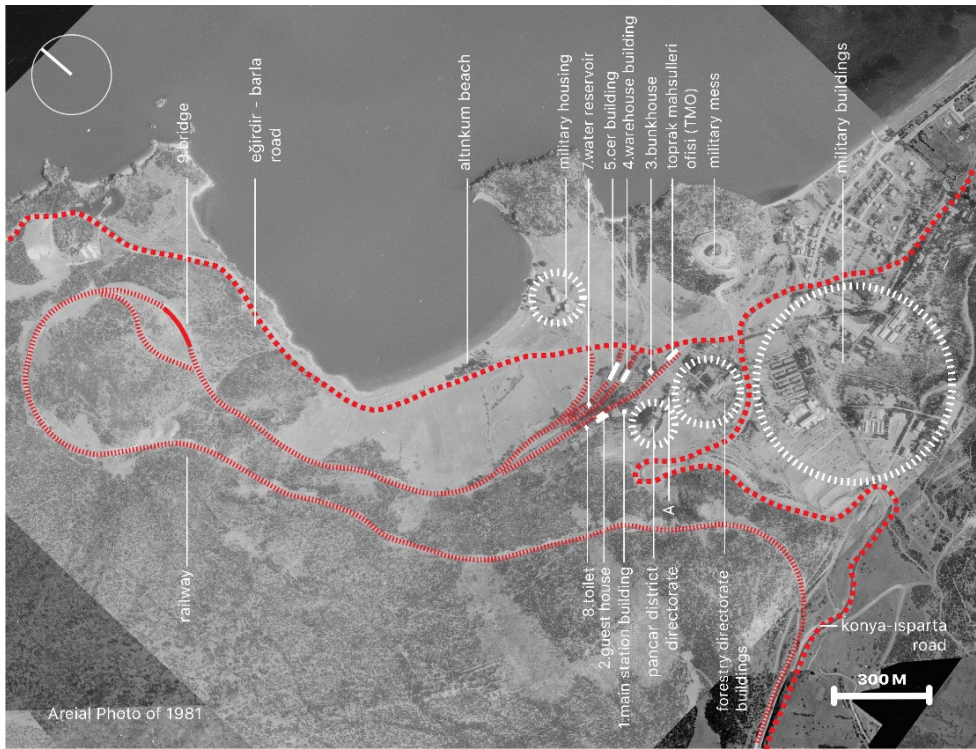


Figure 4.32: First Analysis of 1981 (Sinan, 2019)

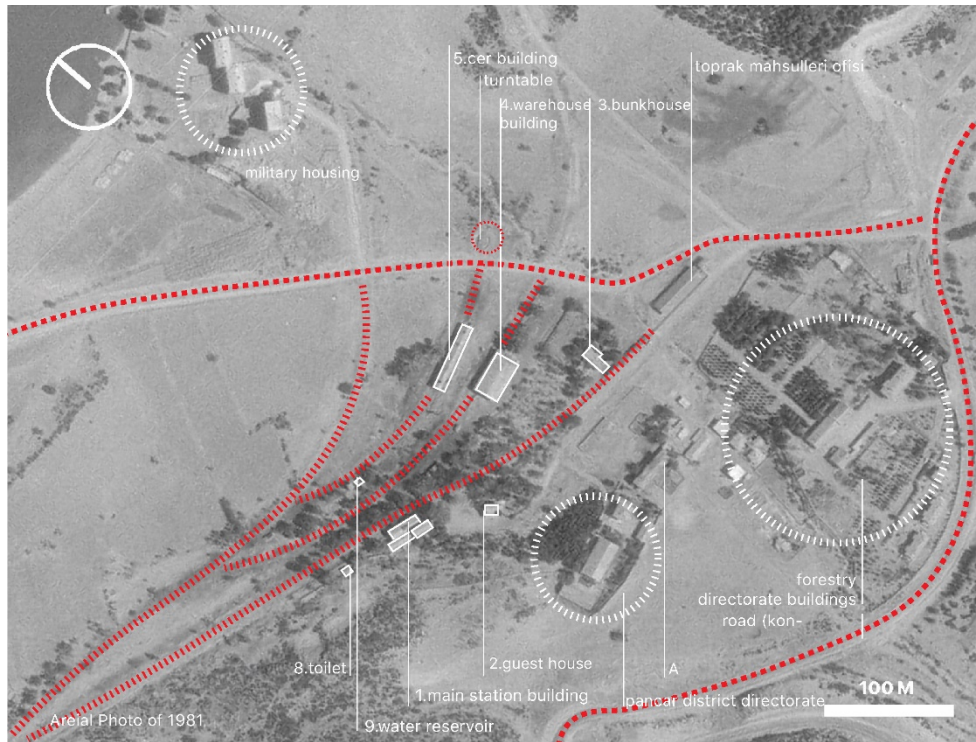


Figure 4.33: Second Analysis of 1981 (Sinan, 2019)

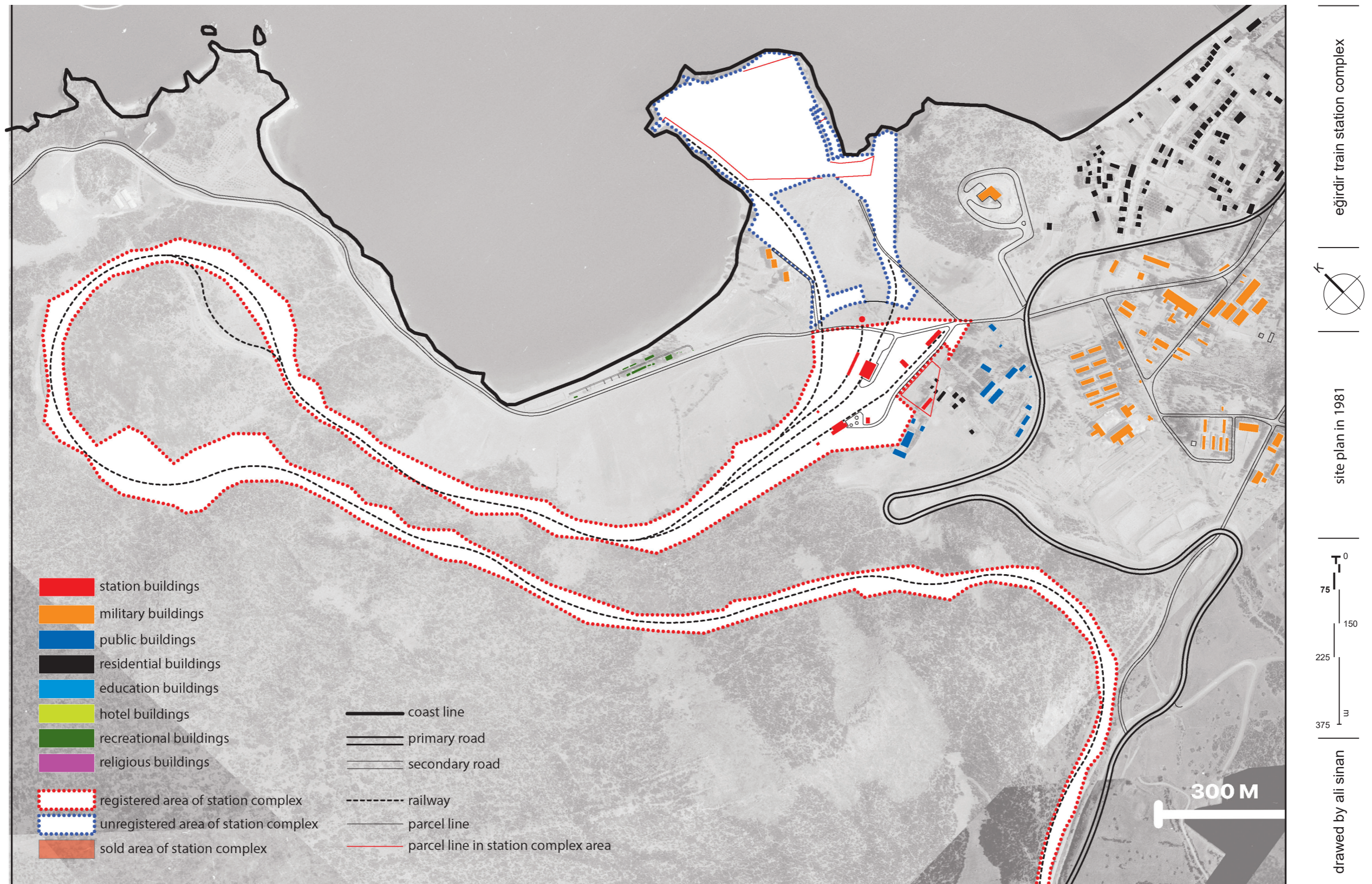


Figure 4.34: Site plan in 1981 (Sinan, 2019)

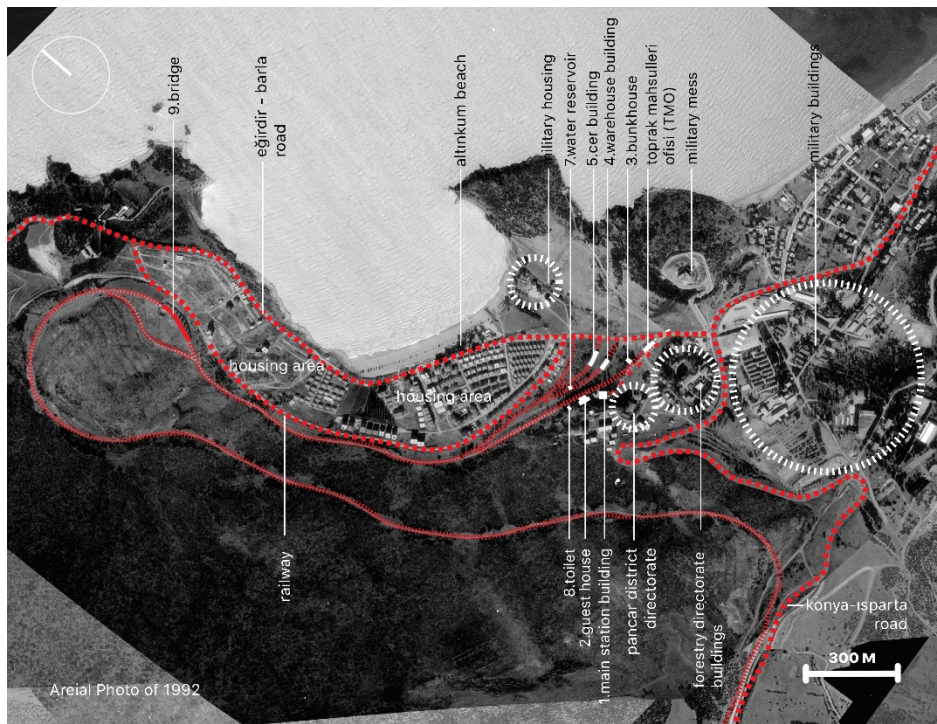


Figure 4.35: First Analysis of 1992 (Sinan, 2019)

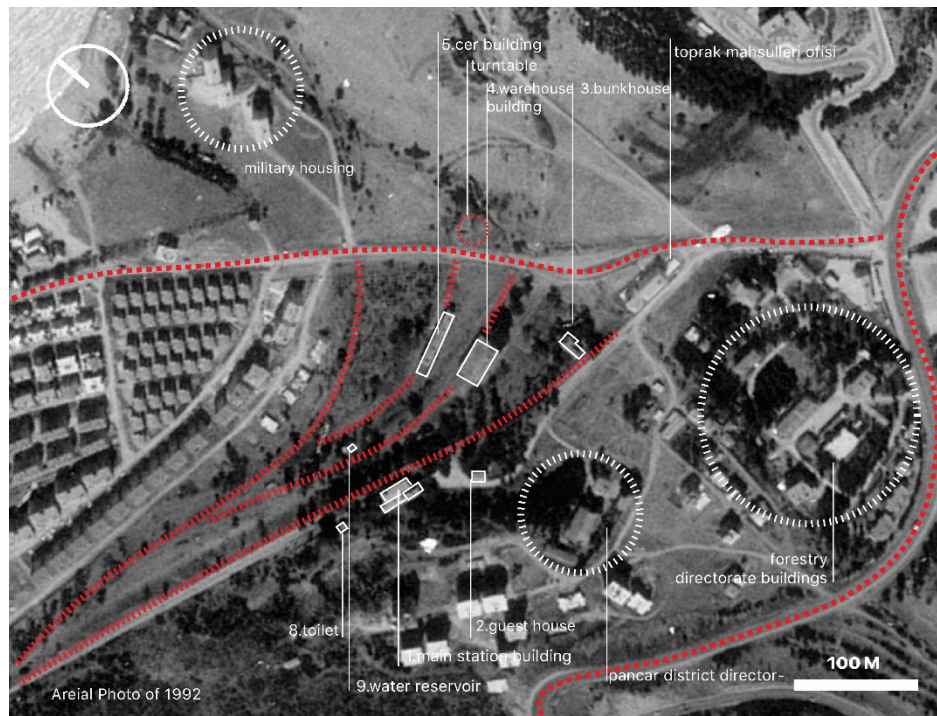


Figure 4.36: Second Analysis of 1992 (Sinan, 2019)

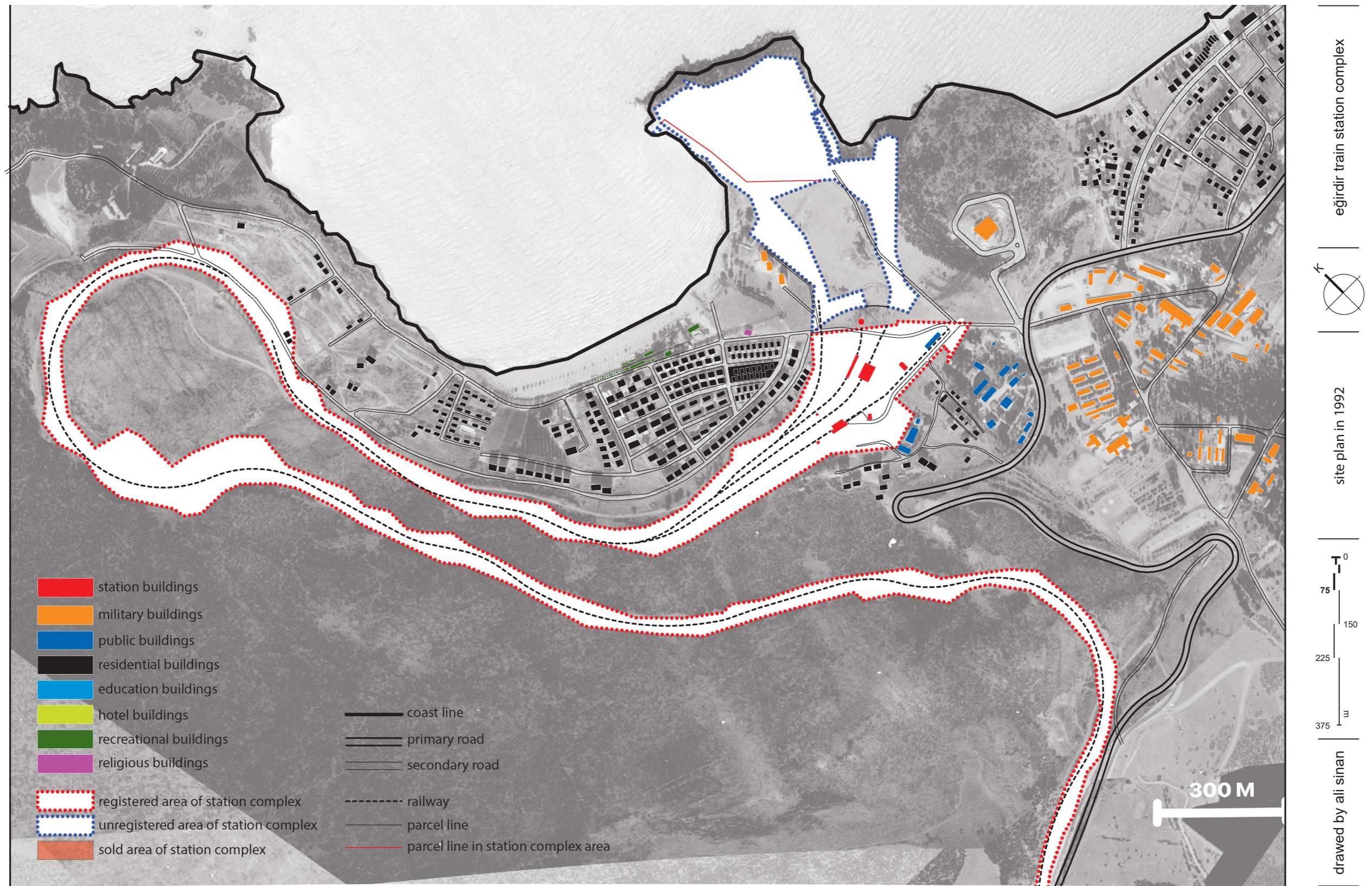


Figure 4.37: Site plan in 1992 (Sinan, 2019)



Figure 4.38: First Analysis of 2014 (Sinan, 2019)



Figure 4.39: Second Analysis of 2014 (Sinan, 2019)

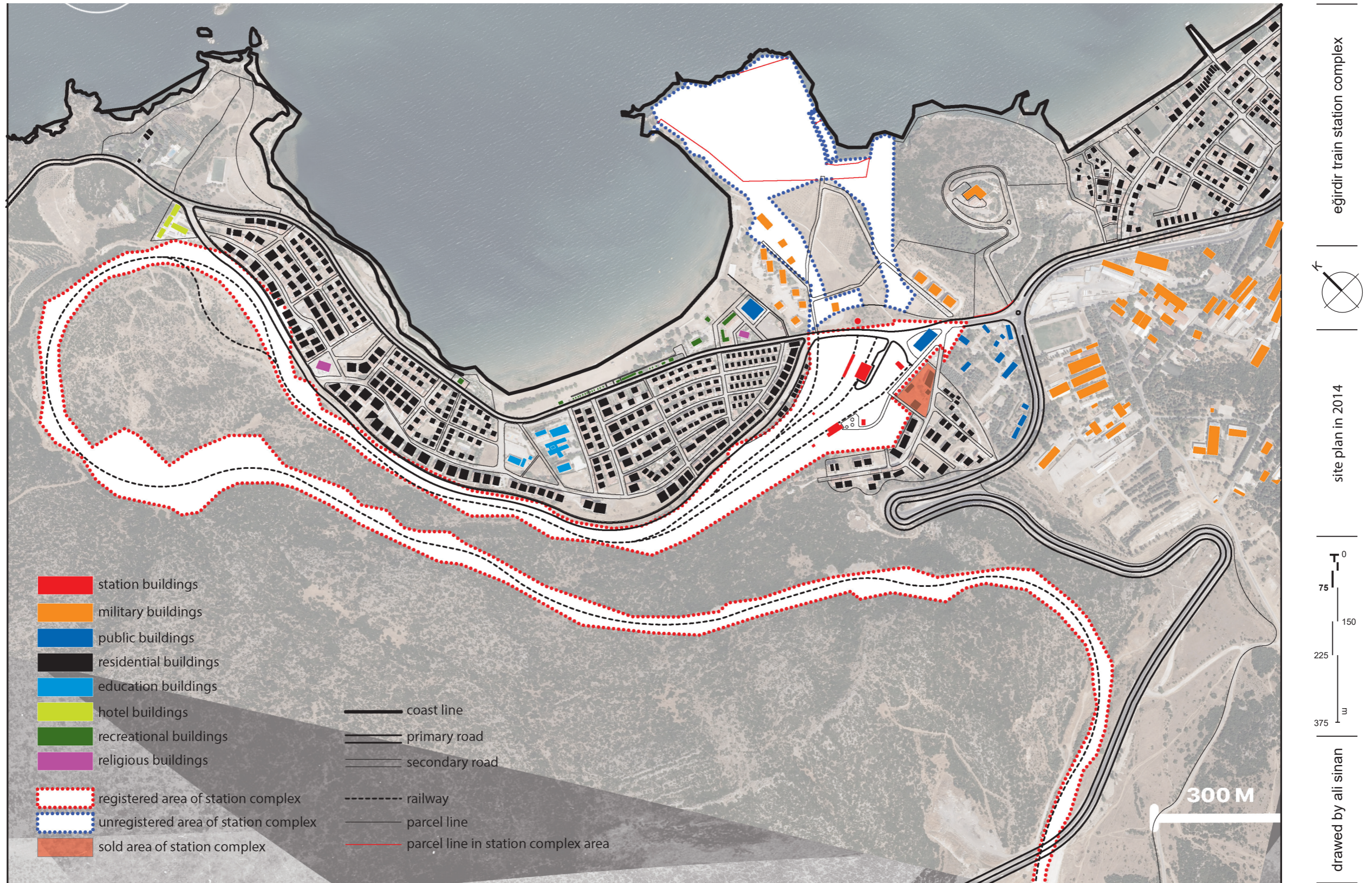


Figure 4.28: Site plan in 2014 (Sinan, 2019)



Figure 4.41: Station Complex and Surrounding in 1941 and 2019

4.3.2. Site Analysis

The railroad and highway determine the boundaries of Eđirdir Train Station Complex, which has a size of 360,000 m². The areas where the railway does not exist are in a vague state. The complex entry road starts from Eđirdir-Barla road. At the beginning of this road, the former Police Station was demolished and the building of the District Police Department is located. At the beginning of this road, a building belonging to the station appears in the aerial photographs. Today, in this area, instead of the building, there is the building of the District Police Department. The main station building, and the courtyard defined by the guesthouse are reached by a curved road with planted trees on both sides. If we consider the area in three main stages, it is located at the highest level.

Another entrance to the station area is from the front of the District Security Directorate Building. With this entrance, the warehouse building is reached. There is also a turntable positioned at this entrance to rotate trains leaving the draw frame by 180 degrees. There is cer building at the lowest level of the field. Today there is no entry here by vehicle.

There are trees planted in the area as we can read from 1941 aerial photographs. These trees are located both at the east of the main station building and north of the draw frame. Among the trees found there by the Cer building are railways leading to the lake. Today, due to lack of care, the landscape of the area cannot be fully read but is ecologically rich.

It is possible to reach the 9-footed steel bridge in the northern part when you go through the railway street (ring road) or follow the railway. On the other hand, there is an alternative railway on the west side of the bridge. The railway draws an arc on the hill where it is located and proceeds 180 degrees to the opposite direction. This area, defined by the route of the railway, constitutes the boundary of the railway land, but today it is a first-degree natural conserved area.



Figure 4.42: Site Analysis of Station (Sinan, 2019)



Figure 4.43: First Analysis of 2019 (Sinan, 2019)



Figure 4.44: Second Analysis of 2019 (Sinan, 2019)



Figure 4.45: Site plan in 2019 (Sinan, 2019)

4.3.3. Architectural Features

The railway complex has a total of seven buildings built to serve various programs, one old wagon, one turntable that allows the locomotives to be rotated 180 degrees, and finally a steel bridge that stands on 9 large feet. The buildings and turntables co-exist, while the steel bridge is positioned outside the building groups. The buildings in the area retain their originality as a whole and all the fiction of the complex can be read easily today.



Figure 4.46: Site Plan of Complex in 2019 (Sinan, 2019)



Figure 4.47: Photo of Station Complex (Sinan, 2019)

4.3.3.1. The Main Station Building

The main station building is the most dominant building in the station complex. The main station building is encountered when entering the area from the original entrance of Eğirdir Train Station. The station building has a linear plan and all spaces are designed to meet minimum needs. The structural system of the building is masonry. The walls are stone and the floors are wooden. Interior walls are plastered. The station building consists of three main parts, excluding the add-ons.

The first part is the passenger platform and steel shed. The shed is made of steel to prevent passengers from being affected by rain and sunlight while waiting and there is no physical deterioration. The platform is concrete and approximately 1 meter above the ground. There are deterioration and cracks on the concrete floor.

The second part has two floors. Its entrance is from the southwest (rear) front. There is a staircase on the ground floor just opposite the entrance. On the south east side of the entrance is the kitchen and the storerooms, with a later added toilet. There are doors and passages between these places. The entrance of the passenger lounge is by the platform. Upstairs, there are three rooms.

The third part is the continuation of the second part, which is single storey. There are three main units in this section. Located in the southeast, the unit is accessed from the platform and includes an entrance hall, kitchen and room. The room has a subsequently made bathroom, WC and storage. The central unit consists of a single room and its entrance is on the platform side. The unit to the northwest is accessed from the platform. Upon arrival there is a large shared room and two small rooms that open into this room. In the common room, the mass is opened which houses the service units such as toilet, storeroom, kitchen and sink.

The building of the Main Station had not undergone any major change except for 3 add-on mass and maintains its original features. It can be used after necessary interventions are carried out.



Figure 4.48: Photo of *The Main Station* (Sinan, 2019)

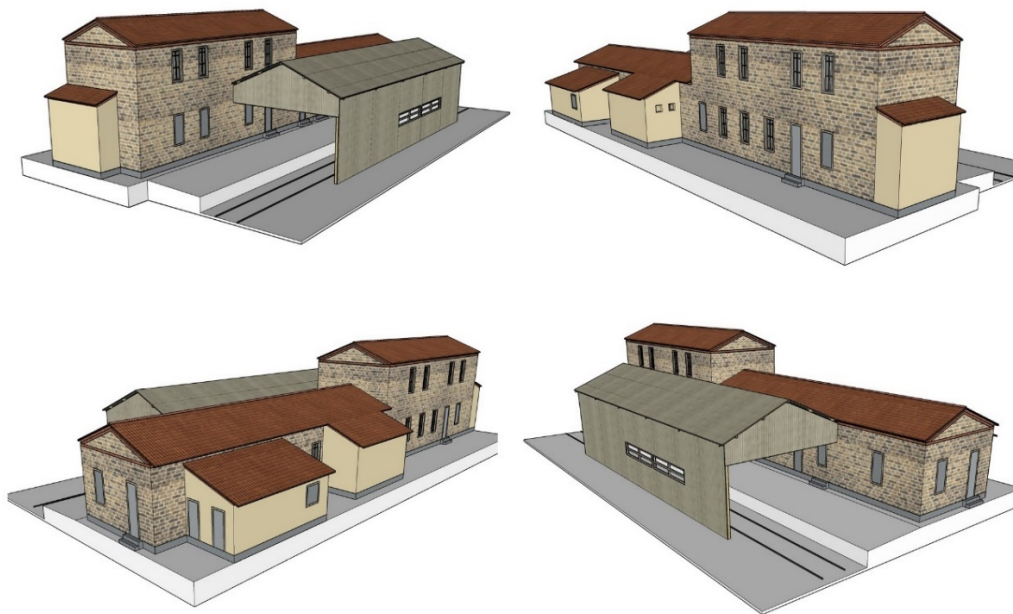


Figure 4.49: 3D views of *The Main Station* (Sinan, 2019)

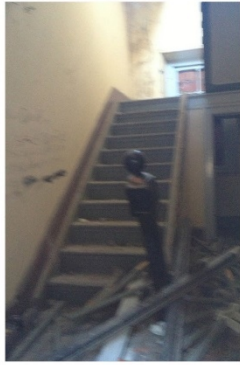


Figure 4.50: Photos of Main Station Building (Sinan, 2019)

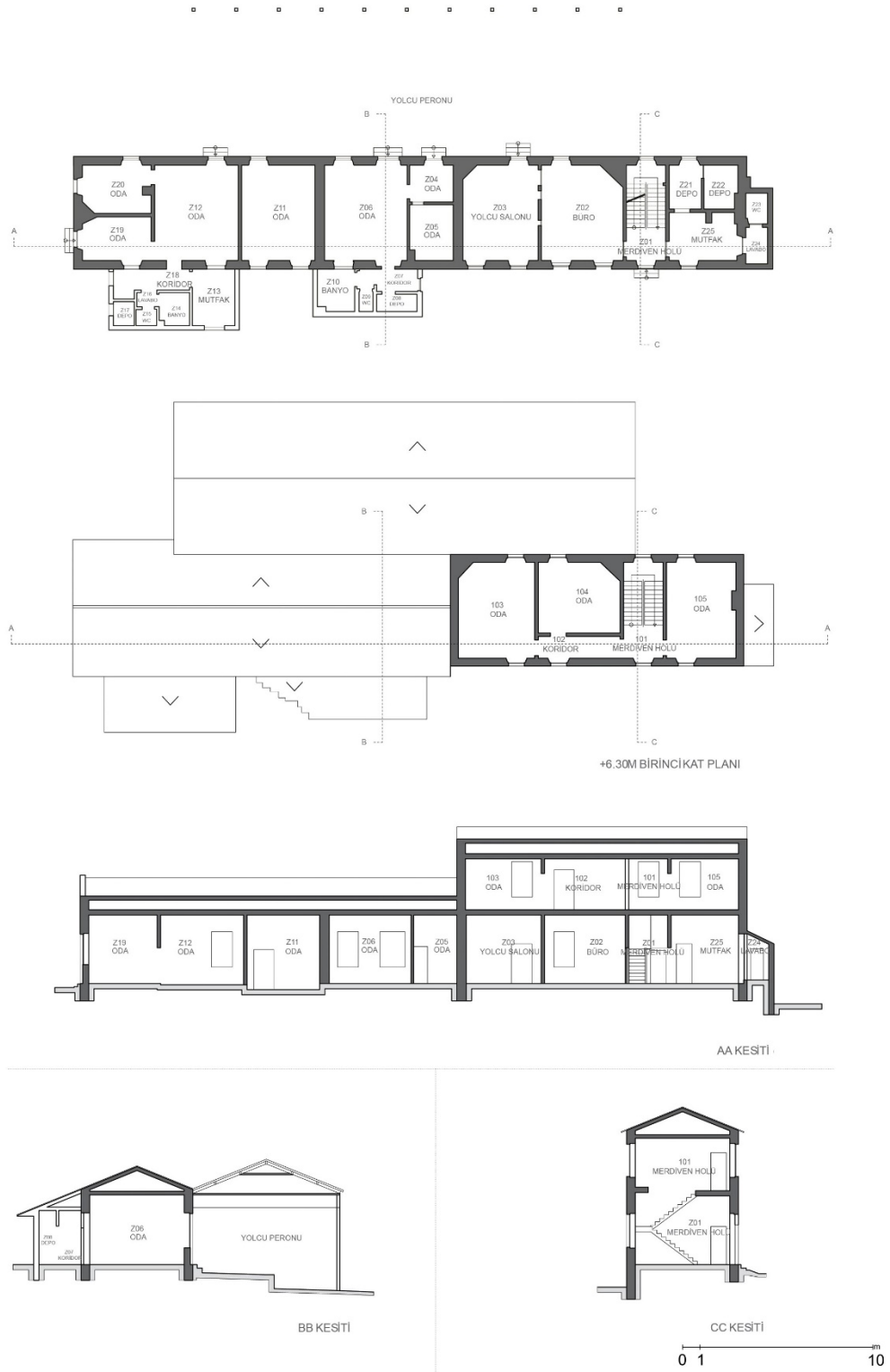


Figure 4.51: Drawings of the Main Station (Sinan, 2019)

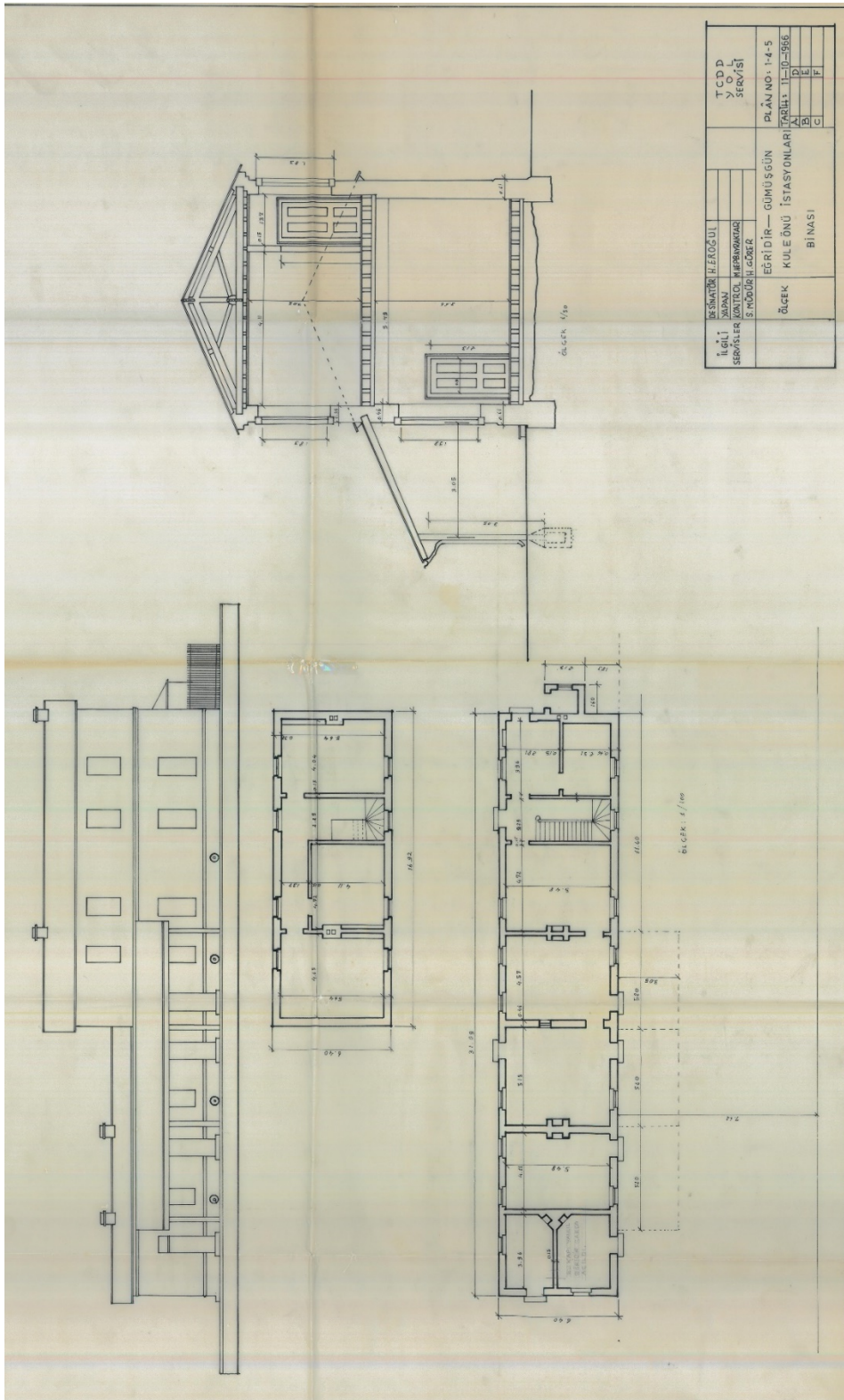


Figure 4.52: Drawings of Main Station Building in 1966 (TCDD Archive)

4.3.3.2. Guest House

The Guest House Building is a two-storey building adjacent to the road at the original entrance of the Station complex, which is closest to the Main Station Building. This building is the most intervened in the station complex. The main structural system of the building is masonry. The walls are stone and the floors are wooden. Interior walls are plastered. The building consists of two adjacent units, one of which is single and the other is two-story.

The first unit is single-storey and its entrance is on the southwest front. There are total of two rooms to the right and left of the entrance hall and a bathroom. The entrance to the second part is by the Northeast front. After passing the veranda that is constructed later, the building is entered. Opposite the entrance is a staircase, a room in the northwest, and a toilet, bathroom, and kitchen in the Southeast. On the second floor, there are three rooms, one of which has a toilet.

The original drawings of the guesthouse could not be found but 1966 drawings were available. In comparison, it is generally authentic except for the reinforced concrete terrace. Small interventions were made in the internal plans. It can be used after the necessary interventions are made.



Figure 4.53: Photo of Guest House (Sinan, 2019)

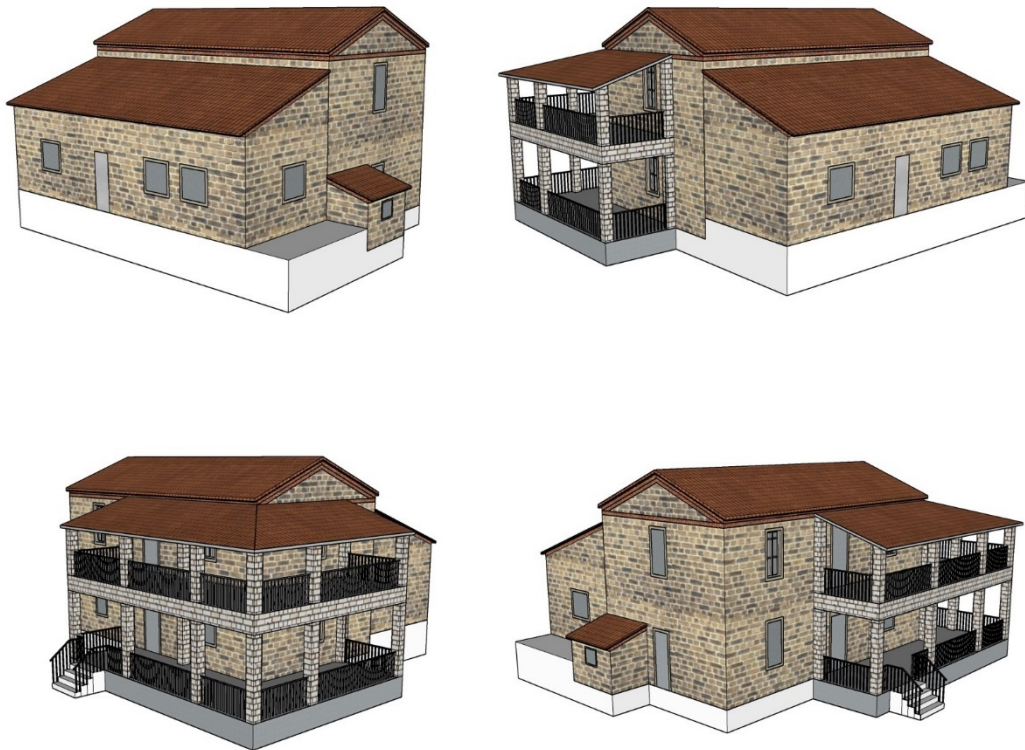


Figure 4.54: 3D views of Guest House (Sinan, 2019)



Figure 4.55: Photos of Guest House (Sinan, 2019)

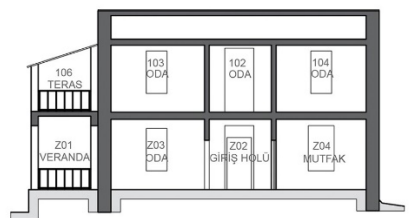
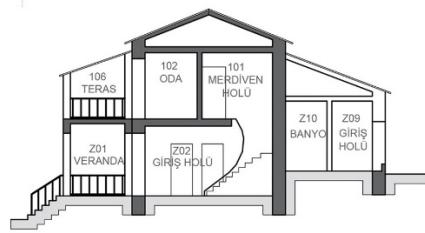
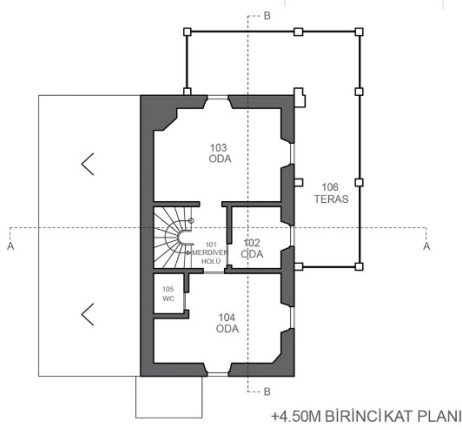
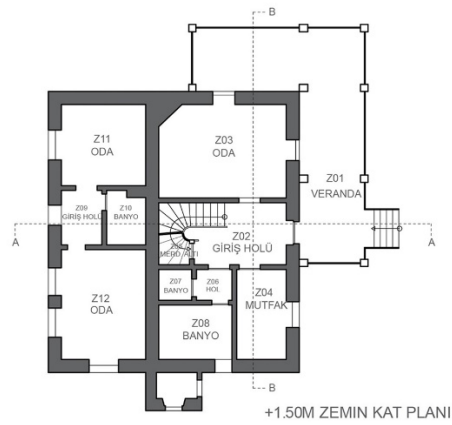


Figure 4.56: Drawings of Guest House (Sinan, 2019)

4.3.3.3. Bunkhouse

It is located on the verge of the station complex to the west of the District Police Headquarters building. The difference between this building and other buildings is that it has an enclosed courtyard. These units have separate entrances and separate courtyards. The main structural system of the building is masonry. The walls are stone and the floors are wooden. The interior walls are plastered.

The entrance to the first unit is on the South Front. This unit consists of a single room and has an exit to the courtyard. In the courtyard of this unit, there is one toilet and a bathroom which is not in the original lawn.

The second building is entered from the west side. Three doors open to the entrance hall. The doors in the south and north directions open to the rooms, while the door opposite the entrance opens to the kitchen. There is an exit from the kitchen to the courtyard, as well as another room located in the south direction. The courtyard of this unit has a toilet in the original drawings, yet there is not a space in the current situation.

Original drawings of the guesthouse building have been obtained. There are no lost spaces except the restrooms located in the courtyard. The plans are intact. It can be used after the necessary interventions are made.



Figure 4.58: Photo of Bunkhouse (Sinan, 2019)

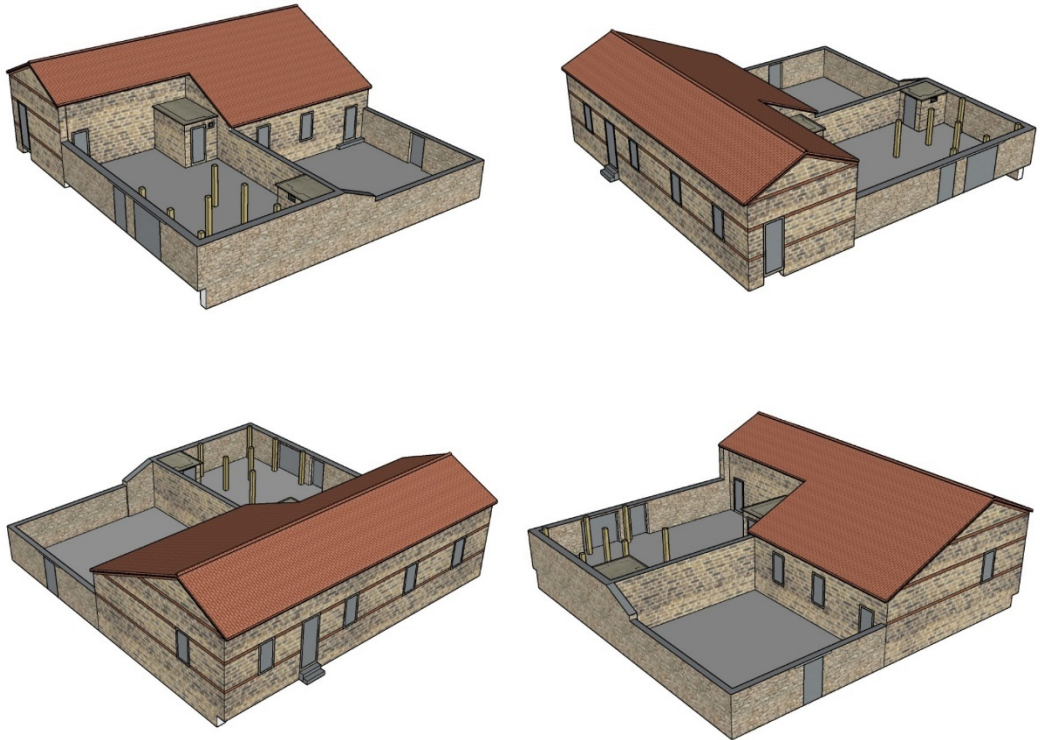


Figure 4.59: 3D views of Bunkhouse (Sinan, 2019)



Figure 4.60: Photos of Bunkhouse (Sinan, 2019)

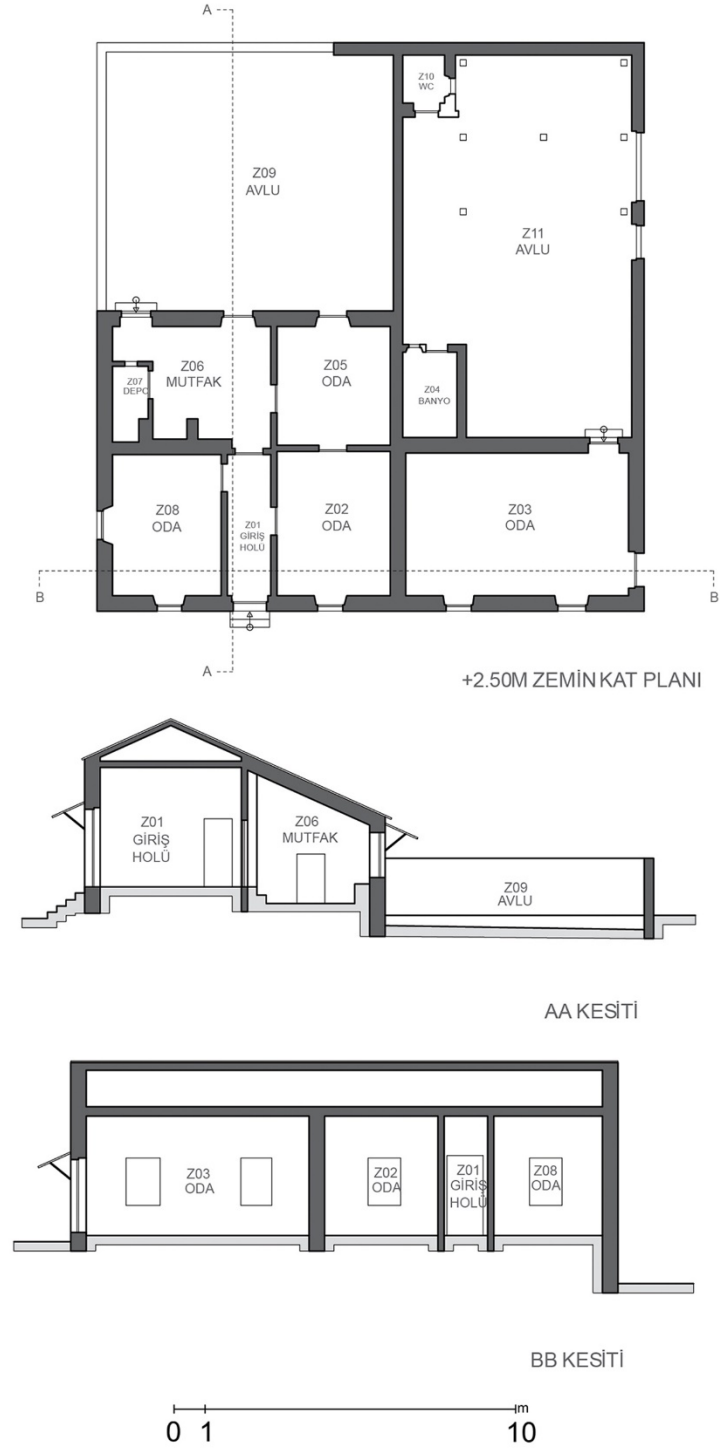


Figure 4.61: Plan of Bunkhouse (Sinan, 2019)

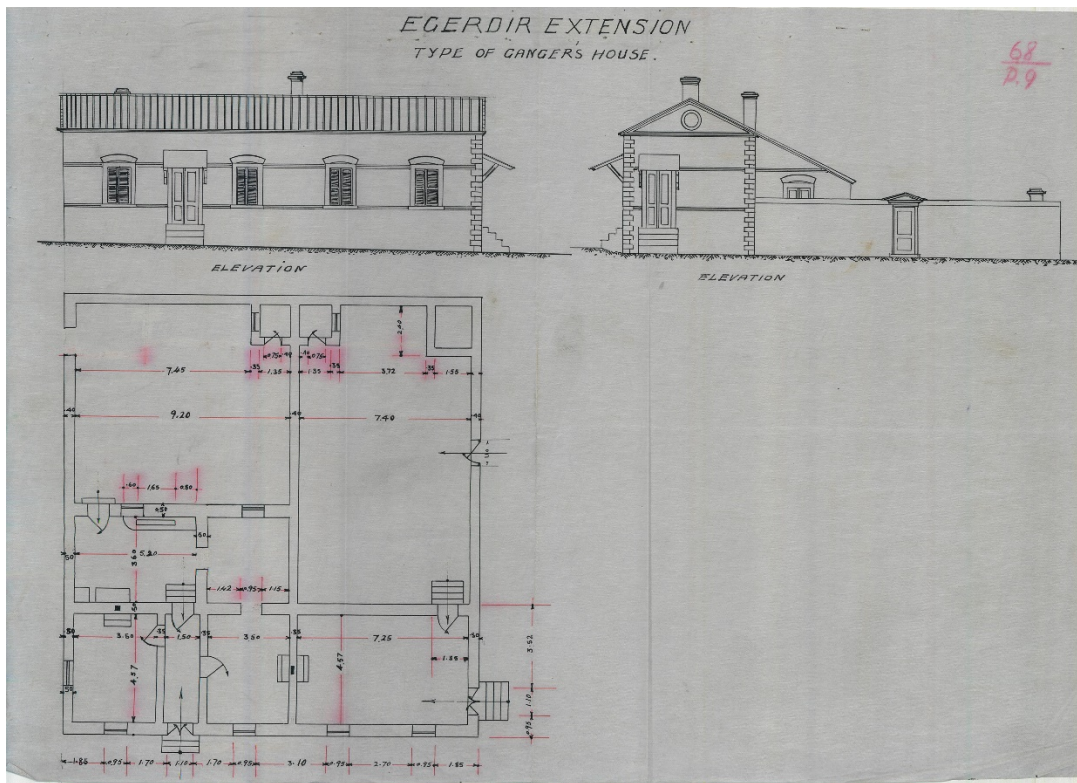


Figure 4.62: Original Drawings of Bunkhouse in 1966 (TCDD Archive)

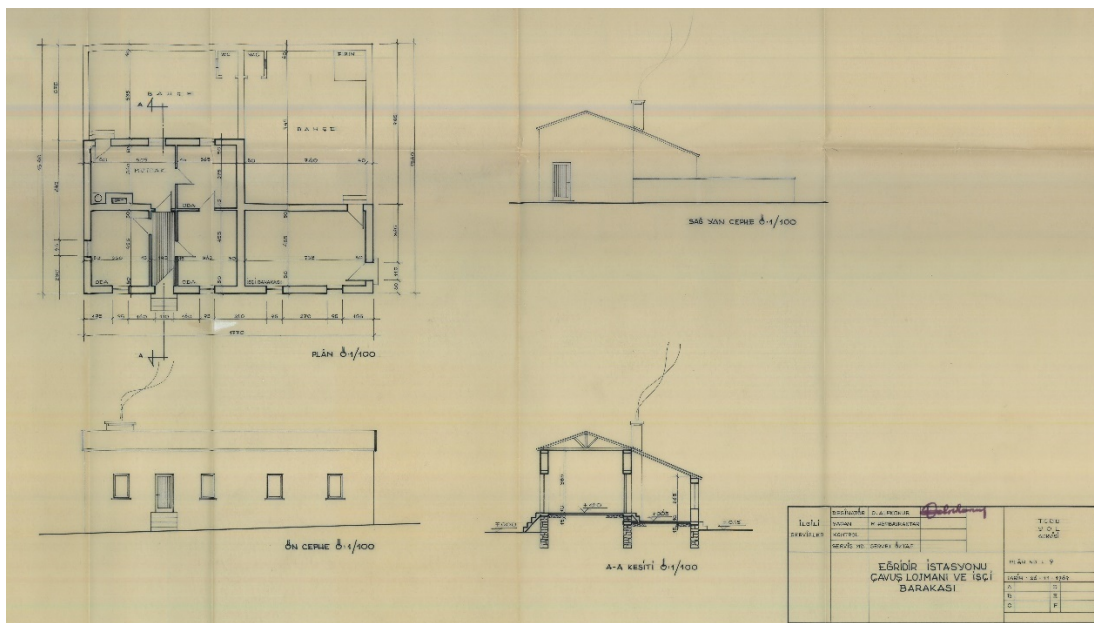


Figure 4.63: Drawings of Bunkhouse in 1966 (TCDD Archive)

4.3.3.4. Warehouse Building

It is the largest building of the station complex. The structural roof system is steel because of its wide span. The walls are masonry. The railway passes through it. The railway running through the northern part makes a difference in elevation. On the west side of the warehouse, there are a total of two large entrances on the eastern front, a big one for the entrance to the train and the small one for the upper floor. There is also an entrance from the south side to the two-storey section. Although the building is a single mass and a single volume, a small part of it on the east side has two floors.

The ground floor entrance of the two-storey section is on the southeast facade. The entrance opens to an entrance hall and opens to a bathroom, a toilet and two-room doors. The room opposite the entrance leads to another room in the northwest direction. There is also a single room entered through the hangar on the northwest side. The flooring of the two-storey section is made of steel but is covered with wood.

The second floor is accessed by an external staircase. There is only one room. Original drawings of the Warehouse Building have been obtained, but the drawing is a reflection (mirror) of the existing building. The warehouse building is largely authentic compared to the original drawings. It can be used after necessary interventions.



Figure 4.64: Photo of Warehouse Building (Sinan, 2019)

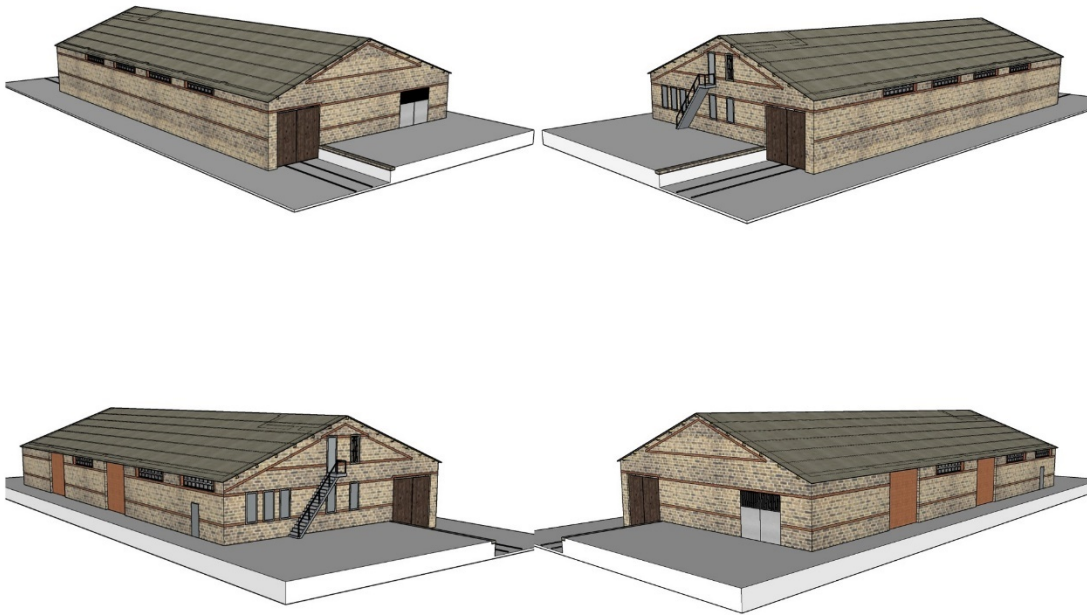
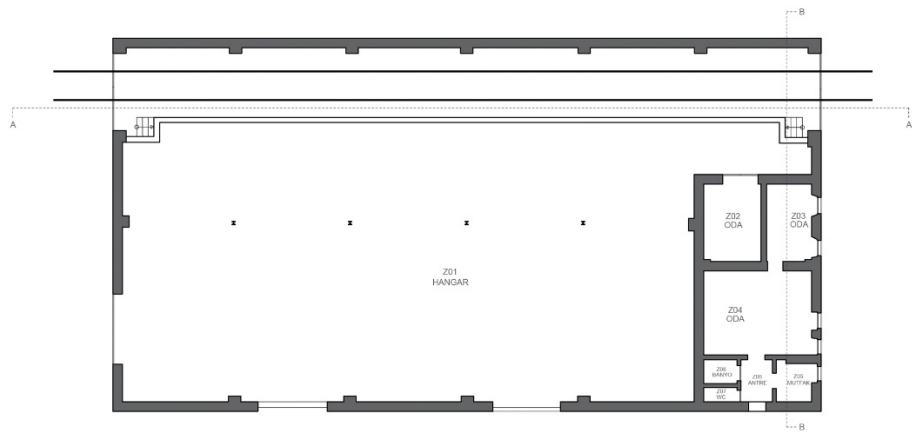


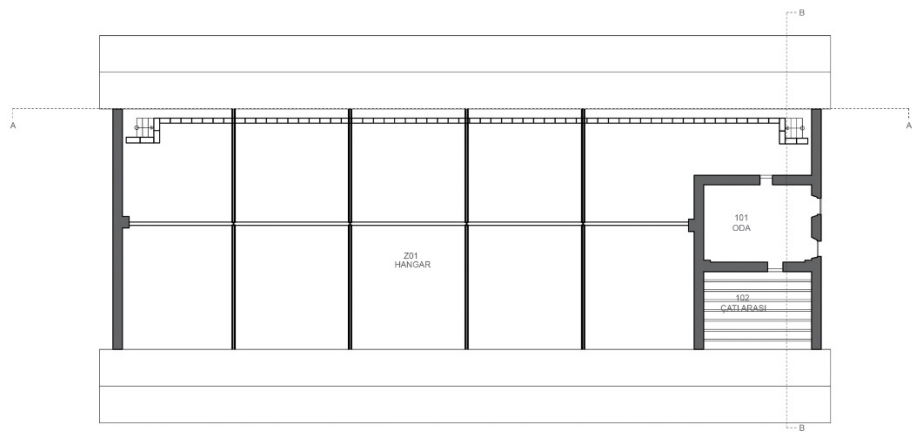
Figure 4.65: 3D views of Warehouse Building (Sinan, 2019)



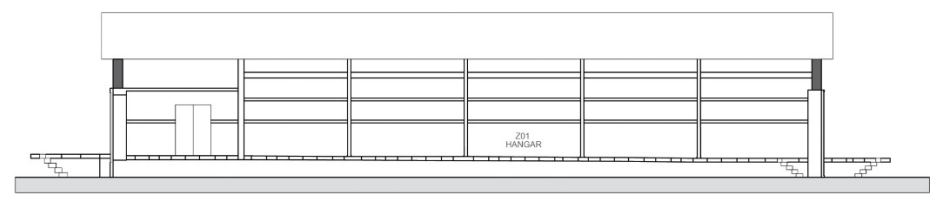
Figure 4.66: Photos of Warehouse Building (Sinan, 2019)



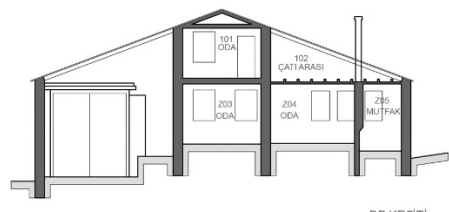
+2.50M ZEMİN KAT PLANI



+5.50M ASMA KAT PLANI



AA KESİTİ



BB KESİTİ



Figure 4.67: Drawings of Warehouse Building (Sinan, 2019)

4.3.3.5. Cer Building

The Cer building consists of a single room. The walls are masonry, but the roof system is steel. The walls are unpainted and unplastered. In this workshop, which is long and thin, the railway passes through and the level of the railway floor for train maintenance is below. There are chimney openings on the roof. The original drawings of the cer building have been obtained and it has not changed compared to today. It retains its mass and spatial originality. On the east side of the cer building, there is a switch used to turn the trains 180 degrees. It can be used after necessary interventions.



Figure 4.70: Photo of Cer Building (Sinan, 2019)

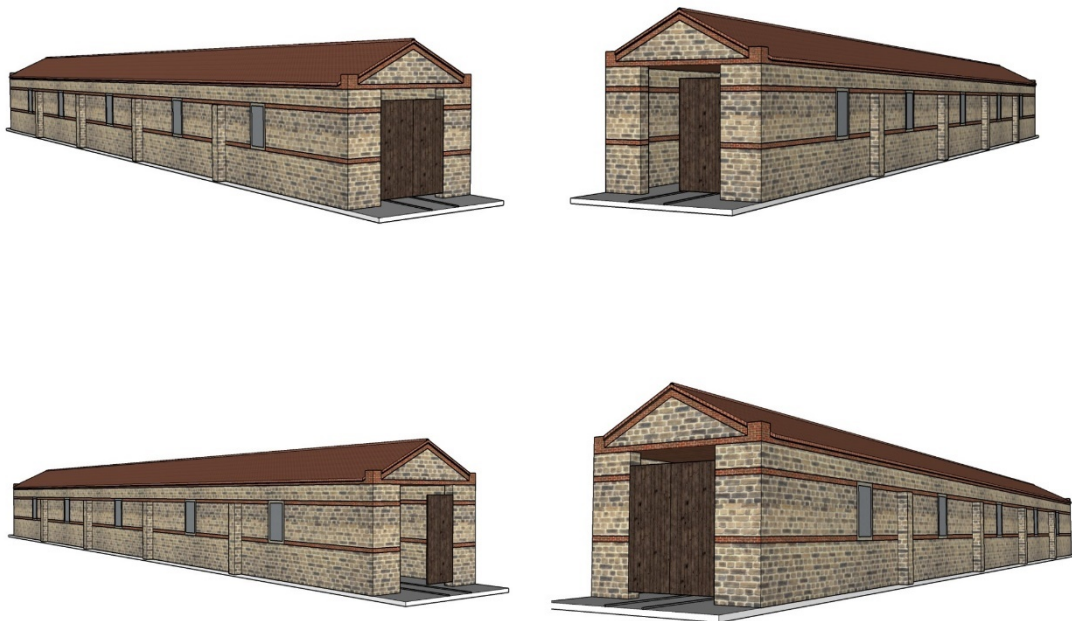


Figure 4.71: 3D views of Cer Building (Sinan, 2019)

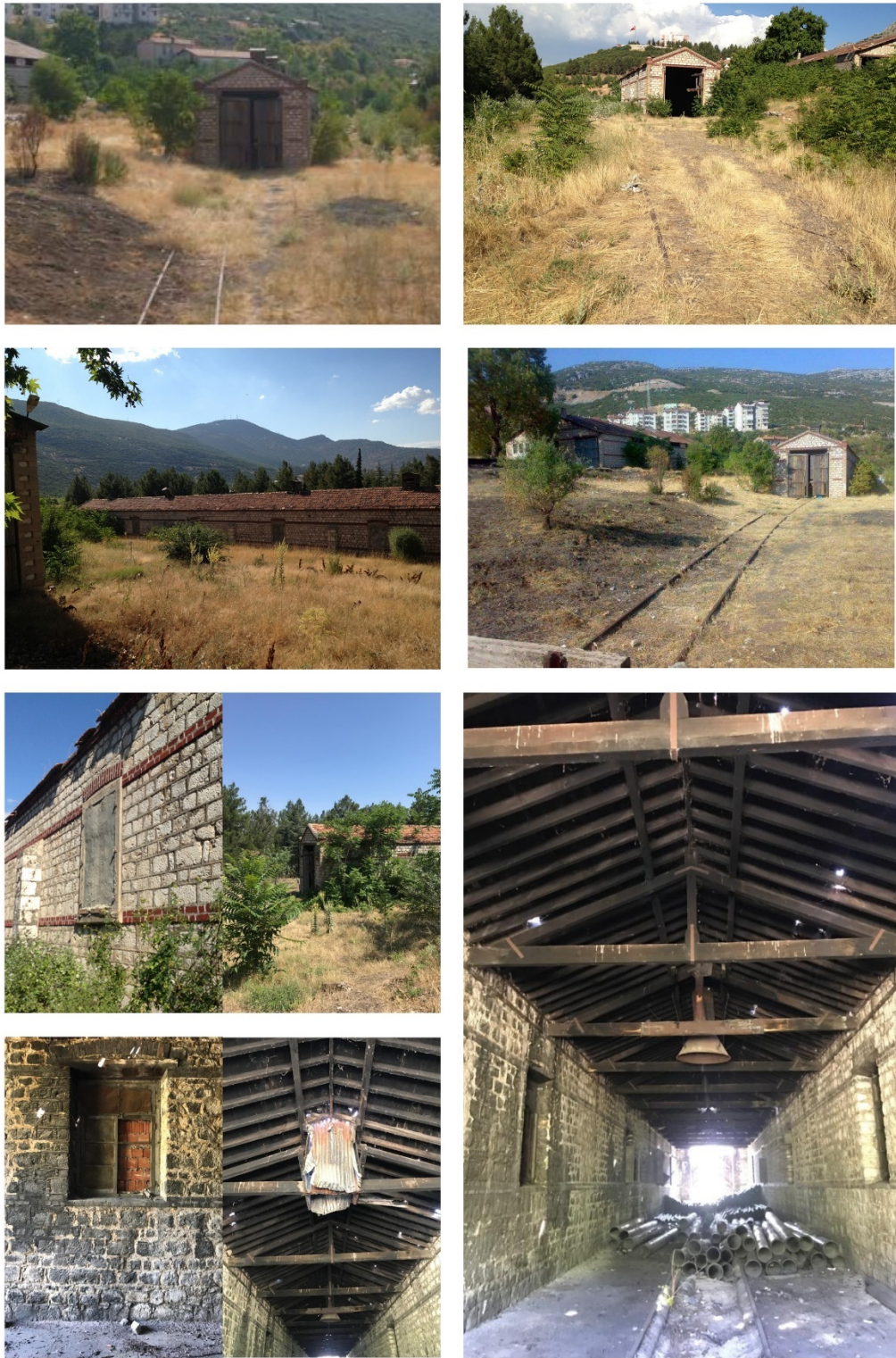


Figure 4.72: Photos of Cer Building (Sinan, 2019)

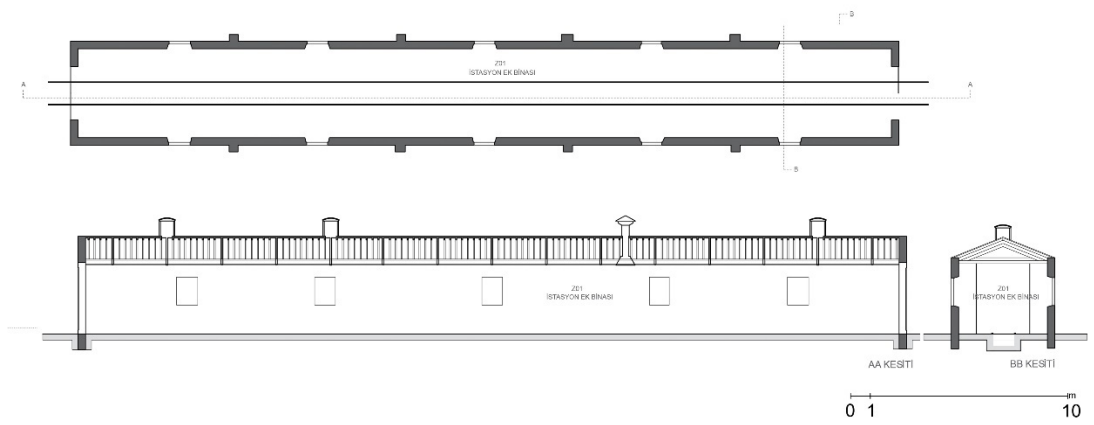


Figure 4.73: Drawings of Cer Building (Sinan, 2019)

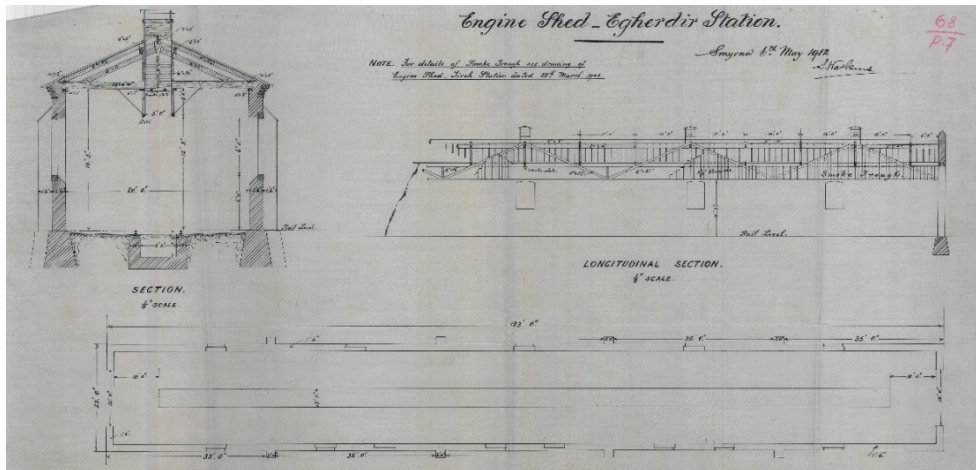


Figure 4.74: Original Drawings of Cer Building (TCDD Archive)

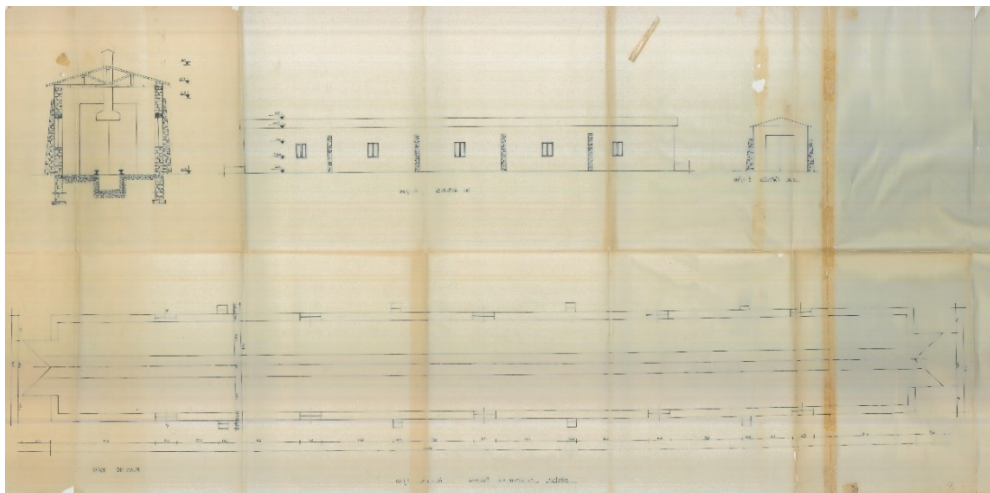


Figure 4.75: Drawings of Cer Building in 1966 (TCDD Archive)

4.3.3.6. Office (Old Wagon)

A wagon was used instead of a building for Office. The wagon is placed on a concrete floor. No information on the wagon was available. It is quite old because it is made of wood, but its spatial characteristics are still observable. The two ends of the wagon were used as the entrance. It is divided into three places.

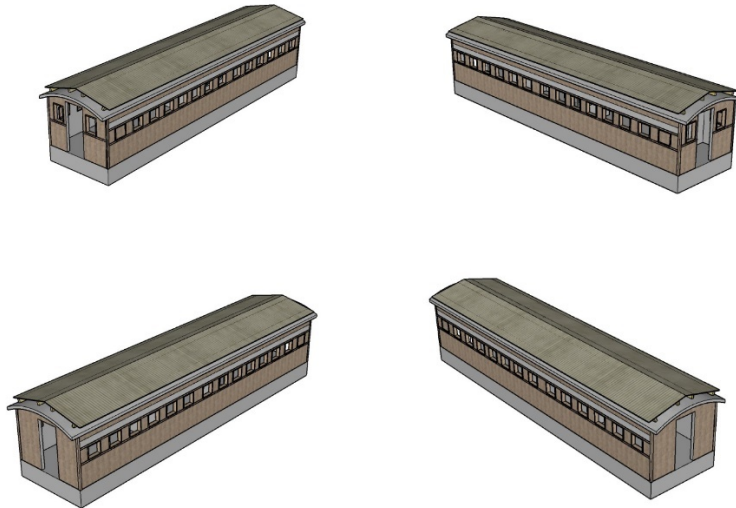


Figure 4.76: 3D views of Office (Sinan, 2019)

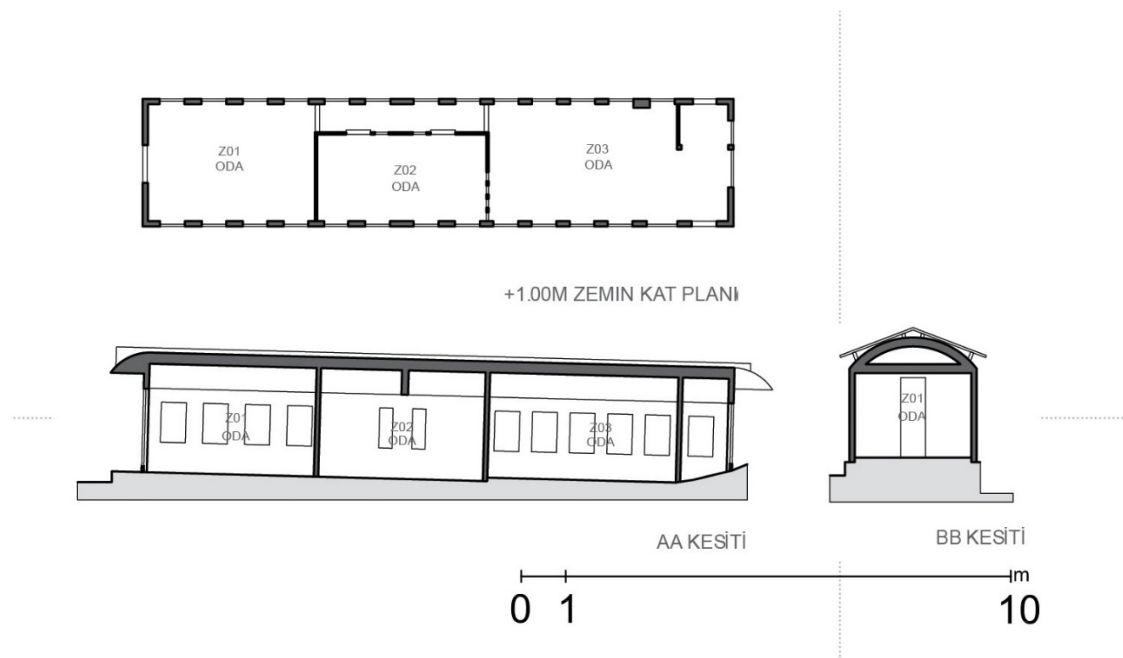


Figure 4.77: Drawings of Office (Old Wagon) (Sinan, 2019)



Figure 4.78: Photos of Office (Old Wagon) (Sinan, 2019)

4.3.3.7. Water Reservoir

The water reservoir lies on the west of the cer building. This reservoir consists of two floors. The ground floor of this depot, which consists of two floors, is of masonry stone and the upper floor is of timber cladding on the steel plate. The ground floor entrance of the building is on the western facade. The upper floor is reached by an iron staircase from the south facade. Although the wood part is physically damaged, the stone and steel parts are in good condition.



Figure 4.79: Photo of Water Reservoir (Sinan, 2019)

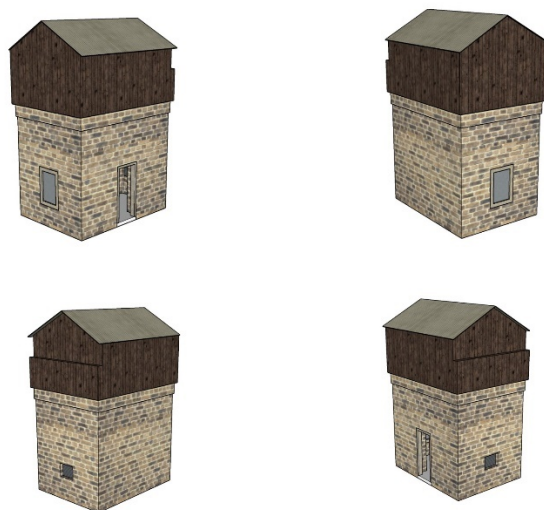


Figure 4.80: 3D views of Water Reservoir (Sinan, 2019)

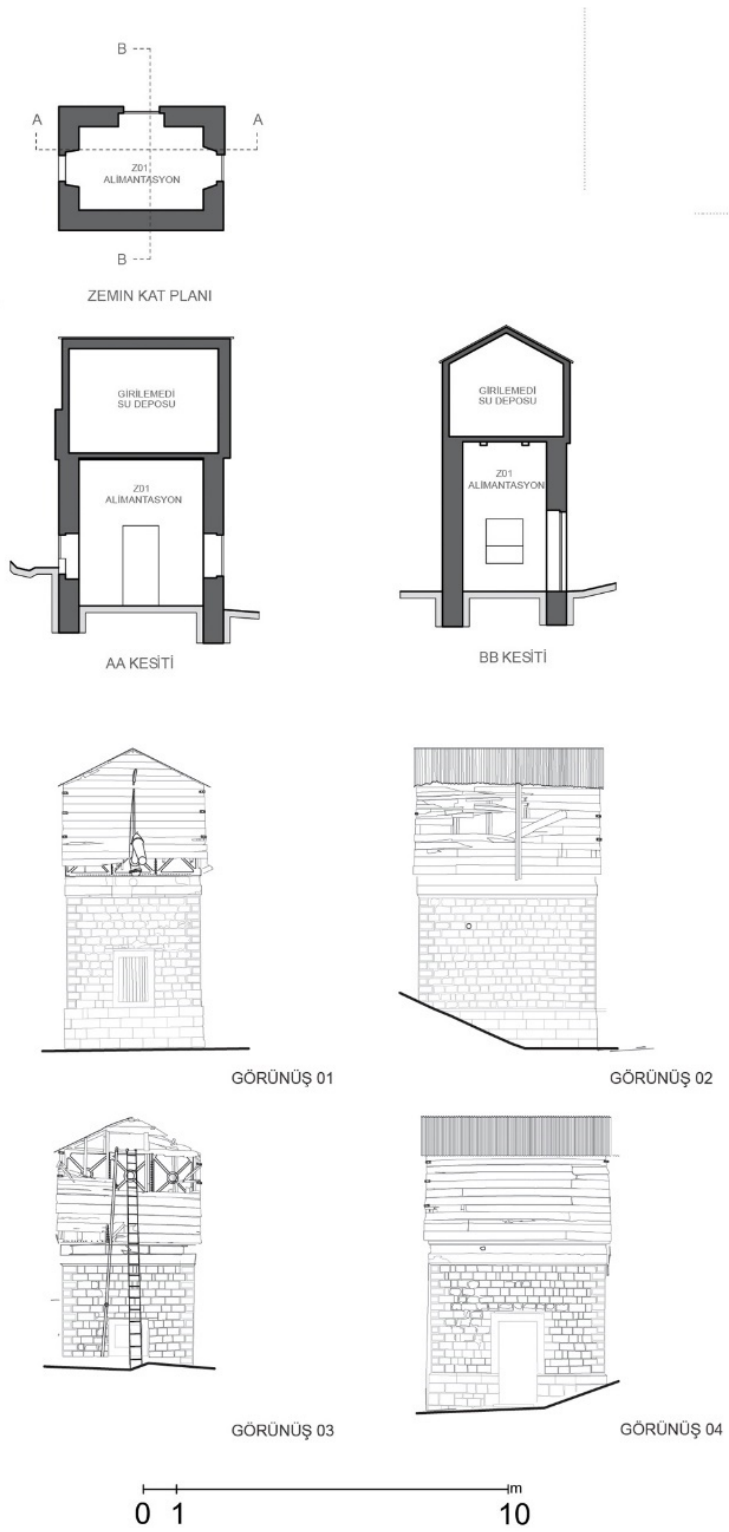


Figure 4.81: Drawings of Water Reservoir (Sinan, 2019)



Figure 4.82: Photos of Water Reservoir (Sinan, 2019)

4.3.3.8. Toilet

It is located on the west side of the main station building. The structural system is masonry. It has two different entrances. Its original drawing has been obtained. When compared with the original drawing, changes are observed in the plan. There is almost no intervention on the front. It is in good physical condition. It can be used after necessary interventions



Figure 4.83: Photo of Toilet (Sinan, 2019)

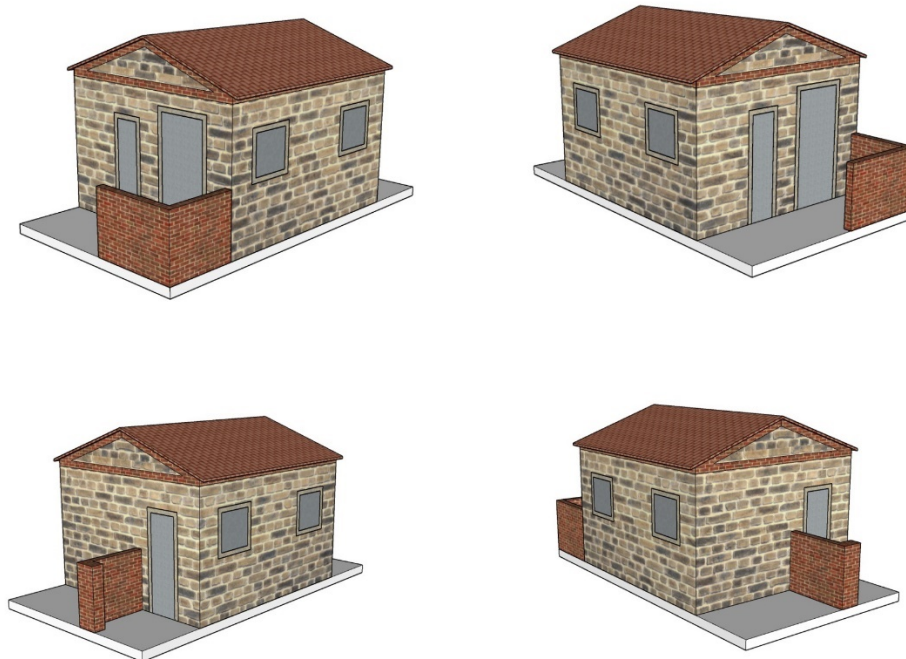


Figure 4.84: 3D views of Toilet (Sinan, 2019)

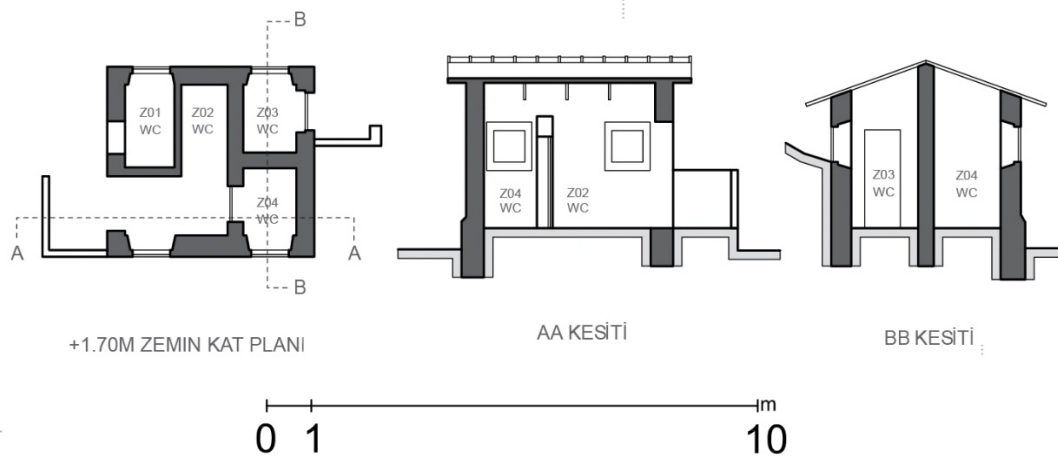


Figure 4.85: Drawings of Toilet (Sinan, 2019)

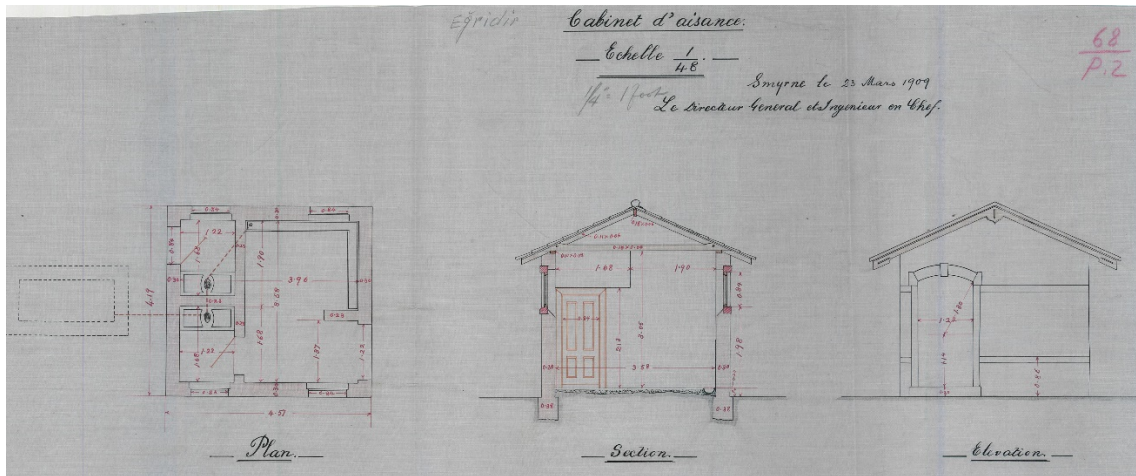


Figure 4.86: Original Drawings of Toilet (TCDD Archive)



Figure 4.87: Photos of Toilet (Sinan, 2019)

4.3.3.9. Steel Bridge

The steel bridge carried by the nine steel legs on the concrete foundation is remarkable both in terms of its construction system and its location. The bridge, which provides a lot of information about the construction technology of the period, is structurally good. Also, it is clear that the bridge is maintained, and it has become one of the symbols of Eğirdir. Various celebrations, especially the arrival of Atatürk to Eğirdir, are held around the bridge, and it is occasionally used by mountaineering clubs for various trainings.



Figure 4.88: A Historical Photo of Bridge (Eğirdir Akin Newspaper Archive)

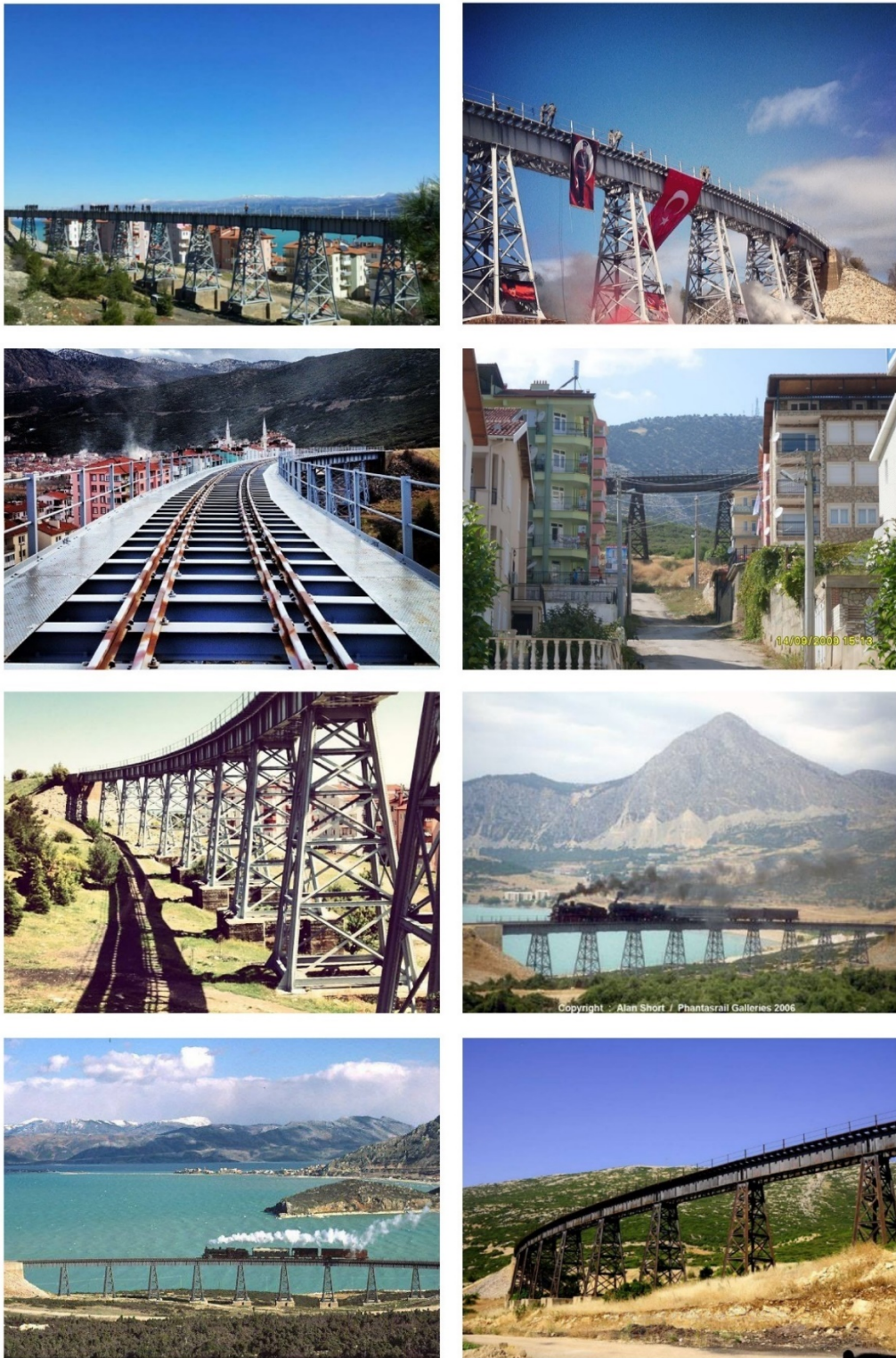


Figure 4.89: Photos of Steel Bridge (Sinan, 2019)

4.3.3.10. Turntable

It is one of the most original railway elements in the area and is made to rotate the locomotive 180 degrees with manpower. In 2016, the turntable, which was located in front of the draw frame, was intended to be dismantled and stolen. They were caught at the last moment while their parts were taken out and reassembled. In September 2017, the historic steam train locomotive brought from Uşak was placed on the turntable. Nowadays, it is observed that the logotype has been painted and maintained.



Figure 4.90: Photos of turntable (Sinan, 2014)

4.3.3.11. Open Areas

It can be understood that the open areas have designed. First of all, afforestation was made in the southern and northern parts (5, 6, 7) of the area. This is evident in the 1943 aerial photograph.

The original entrance of the station remains in the southern boundary. Routing has been increased by afforestation on both sides of the entrance road. There is a large courtyard (1) when you enter the station complex. The floor of the courtyard is degraded asphalt. There is a total of 3 monumental trees, one of which has been cut down.

Located opposite the station building, the guesthouse has its garden (2), separated by fences. Despite being neglected, it has a magnificent landscape. The ground is all soil except the station building and the original complex entrance road.

There is also a large space (3) in front of the warehouse building. This space is left for maneuvers of large vehicles.

There is a turntable used to turn the train 180 degrees in the area (4) between the Eğirdir-Barla road and the iron bars. The part between Eğirdir-Barla road and the lake is currently being used by the military and most of the tracks are unreadable. Although the area is neglected, it is quite rich in its nature



Figure 4.91: Analysis of Open Areas (Sinan, 2019)



Figure 4.92: Photos of Open Areas (Sinan, 2019)

4.4. Social and Cultural Features

The stations are undoubtedly important public spaces for the cities to which they belong and the people living in them. Eđirdir Station, which represents the last period of the Ottoman Empire, the struggle for independence, played a major role in the growth and development of the city in the Republican Era, and reminds and represents these periods and carries the traces of history and that process.

Cihangirođlu (2019) describes memory spaces as places where memory is fermented rather than what we remember. Memory spaces are primarily ruins; they arise from the idea that there is no memory. The ones we encounter in space are the ones that trigger things in our memory, which make them appear again and bond by associating with our past experiences. These can sometimes be a sound, a smell, an event, as well as the history of that place itself. Anything we see or feel can stimulate our memory. At this point, we can say that the concepts such as sense of place and memory are actually intertwined concepts. We can express that if the sense of place is the thing that is born of an identity or character that space has and that man can attach to space, then the person who carries the memory and reveals his history, past and experiences is also human, and that the bridge in the relationship with place is human.⁹³

“When space makes us feel familiar, it becomes a place.”⁹⁴

Place is a space loaded with meaning. Grouping the various ideas of the place and its inhabitants in areas such as geography, urban planning, urban design, architecture, sociology and environmental psychology; the meaning and importance of that place, its users and locals about the place forms the spirit of the place.⁹⁵

⁹³ Cihangirođlu, M. S. (2019). *Bellek Mekanlarının Deđer Algısı ve Deđişimi Üzerine Bir İnceleme: Ankara Tren Garı* (PhD Thesis). Graduate School of Fine Arts of Hacettepe University. p. 38

⁹⁴ Tuan, Y.-F. (2014). *Space and place: the perspective of experience*. Minneapolis, Mn: University of Minnesota Press.

⁹⁵ Rifaiođlu, M. N., & Güçhan, N. S. (2013). Property Rights as a Source for Identifying and Conserving the Spirit of Place. *MEGARON / Yıldız Technical University, Faculty of Architecture E-Journal*, 8(3), 137–148. <https://doi.org/10.5505/megaron.2013.61687>

With all this information, it is important to look at the social and individual experiences of the station in order to understand the spirit of the station. Firstly; Eđirdir Train Station Complex has a very important position in the Turkish Nation's struggle for independence., Eđirdir is mainly the subject of railway station in written documents where it is the subject of recent history. This is important in the memory of the people living in Eđirdir.

Researcher and retired teacher Recep Bozkurt mentions about the station in his article in Akın Newspaper in 2009 as follows:

“I'm really sorry as someone who knows why and how this railway came to Eđirdir 97 years ago (1 November 1912) without even visiting the provincial centers such as Burdur and Isparta (the railway could only come to Isparta 34 years later), under what conditions and how the station buildings were built; how Eđirdir Railway plays an active role in the formation of NATION; and how hundreds of tons of ammunition carried by the locals of Eđirdir, the young and the old, both men and women, from this station to the Western Front; and how they all contributed to the expulsion of the enemy from the country.”⁹⁶

In the action taken by the local people against the sale of Eđirdir Station in 2014, Bozkurt stated the importance of Eđirdir Station in the letter read by me, again highlighting the process of the struggle for independence.

During the struggle for independence, the vital importance of the Eđirdir Lake and Eđirdir Station must have attracted the attention of the founder of the Republic of Turkey, Mustafa Kemal Atatürk, since he came to Eđirdir by train on March 6, 1930. The train that came to Eđirdir at 02: 30 stopped on the Iron Bridge and Atatürk spent the night there. When the weather was bright, Atatürk admired the view he saw from the Train's window and said the phrase that the local people would boast about in the

⁹⁶ Bozkurt, R. (2012, November 8). Eđirdir Evi'nde GemiŖe Yolculuk. *Akın Gazetesi*.

future. “*What a beautiful sight!*” Today, Atatürk's arrival in Eđirdir is celebrated publicly in front of the Iron Bridge with great enthusiasm.⁹⁷



Figure 4.93: Celebration of Atatürk's arrival in Eđirdir in 2014 (Eđirdir Akın Newspaper Archive)

Apart from such important events and moments, Eđirdir station has taken its place in the memories by hosting many Turkish films. The film *Mine*, which takes an important place in the career of Trkan Őoray, one of the most important actors of Turkish cinema, was shot at Eđirdir station in the 1980s. While such famous actors came to the city, creating a great excitement for the city, the fact that Eđirdir and Station were used as a stage in cinema played an important role in the recognition of the city.

⁹⁷ Eđirdir Belediyesi – Atatrk’n Eđirdir’e Geliřinin Tarihçesi. (2009). Retrieved November 10, 2019, from Eđirdir.bel.tr website: <http://www.egirdir.bel.tr/icerik.aspx?iid=14>



Figure 4.94: Posters of Film (Eğirdir Akın Newspaper Archive)

Eğirdir Train Station Complex is not included in the daily life practice of the citizens until the mid-1980s. The most important reason for this is that it remains outside of the city center and there are no programs to feed the daily life around it. Before the 1981 reconstruction plan, the station was designed with recreational programs such as a youth camp. With the plan of 81, the area around the station was opened for construction and remained among the residential building. Nevertheless, it has been involved in daily life practices with recreational programs. However, the sound of the train, the presence of the train and the journeys, greetings and farewells have taken place in the memory of the citizens. Şapçı (2008), who wrote his memoirs with Eğirdir, describes the place of Eğirdir Station in his life as follows.

“... Steel Bridge: There was always the beginning or the end of a longing in the hearts of people when they left Eđirdir and on their way back to Eđirdir. On the way back from Istanbul, when the “dark locomotive” starts approaching to the hill of Miskinler (beli), after passing the “Karakuyu” or Isparta, we would close our eyes to see that wonderful blue and unique sight suddenly, and when the train crossed the hill, we would watch the beauty appears in front of our eyes opened, each time, as if we were seeing for the first time.. We always looked for Sivri mountain. Who will see it first? Finally, after the Great Findos bend, Sivri* appears in all its glory and give us the good news that you came to Egirdir. Then the Muskin and what I'm saying...”⁹⁸*



Figure 4.95: Sivri Mountain and Railway (Sinan, 2019)

⁹⁸ Şapçı, O. (2008). *Güdüklü Muşilli* (Vol. 1). İstanbul: Akdeniz Yayıncılık A.Ş. p.138-145



Figure 4.96: First view of lake from railway on Miskinler Hill. (Sinan, 2019)

Researcher and retired teacher Bozkurt (2015) Eğirdir in his writings named Trips in Eğirdir, mentions of the station area as follows:

“I started one of the morning walks at Eğirdir Station on May 30, 2004. It was a bright spring morning...My goal was to follow the train up and down to sloth hill. You have no idea how sad I was when I arrived at the historical Eğirdir Station! There were no trains nor train passengers in this historical place, which carried people away for years and brought homesick people back to their loved ones! It was all deserted and quiet!

The doors of the historic stone building were locked and the shutters were closed! The great sycamore trees, the acacias, the aspens would have dried up! Living safely between the branches of these majestic trees in winter and summer, they sang to the passengers in their own language, “Welcome! Good-bye! ”the rest of the hundreds of birds, three or five Magpies and crows, were about to leave! A tear in my eyes, a blind knot in my throat, I've been pinned to this sight!”⁹⁹

⁹⁹ Bozkurt, R. (2015). *Eğirdir Gezileri Eğirdir Yazıları*. Eğirdir: Eğirdir Belediyesi. p.155-160

In the 1970s, Recep Bozkurt expressed his memory of Eđirdir and Railroad as follows;

“... twice a year, we would use the Eđirdir-Mekece (Adapazari) Line to go to my hometown Iznik, take the Pamukkale-Istanbul Train departing from Eđirdir at 18.00, we land in Pamukova or Ali Fuat Pasha in the morning... Our children sleep soundly with the sounds like “taka taka”, “tiki tiki”, coming from the steam locomotives; We could not understand how our journey passed so quick... In the 80s, when saying farawell to my young son, Zeki, going to military high school in Istanbul, we would still prefer this way, and when we hear the furious screams coming from the train, we would start shaking hands, with the tears on our eyes, with the longing newly appeared on the spot.. On his forehead it reads, “Eđirdir-Altitude: 953-4”, and with a stubbornness against all the abandonment, it still glows!”



Figure 4.97: A sign on the main station building. (Sinan, 2019)

“Although almost 100 years have passed - except for the wooden parts - this beautiful stone building is still intact!

The scale branded "London-Liverpool", waits almost eagerly to be used in front of the entrance door, as if not pulling thousands of tons of load over the years. I'm jumping and stepping on the rotten sleepers, still standing under the dark shadows of the tall trees that are cracked.. The railway leads 300m more, and it continues climbing to Misikinler Hill with a very sweet inclination ... The newly built houses of İstasyon Neighbourhood were slowly appearing under my feet. When I arrived the famous “Steel Bridge”, I can't express how excited that I felt!”



Figure 4.98: The relationship between the bridge and housing (Sinan, 2019)

“Standing on top of this steel structure up to 30-40 m high, is it possible to respect and not be excited in the face of human intellect and power?”

It was not in vain that the Great Atatürk stood right here during his visit to Eğirdir and was struck by the magnificent view of Eğirdir, which he saw from the window of the wagon he was travelling in!

In front of me; from the turquoise blue to the sky green, from silver to pansy, Lake Eğirdir in a thousand colors of revelation. Islands, peninsulas lined up like pearls.

Mauve slopes and Eğirdir houses shining brightly in the morning sun ...

It's like a dream!

While I was crossing the gigantic “Iron Bridge” and jumping over the sleepers, the sounds of nightingales, tits, goldfinches, turtledoves blend into the smells of thyme, I become “nature drunk!...”



Figure 4.99: Lake View from Steel Bridge. (Sinan, 2019)

“When I turn the Nafiz Yürekli Bend and enter the Valley of Stone, I suddenly find odors in the spindle that burn my nose. The angustifolia trees on the left slope make me dizzy my with their newly opened flowers, even though they have been laid down from this year's snowy winter ...”



Figure 4.100: Nafiz Yürekli Bend (Sinan, 2019)

“Not to mention the thousands of bees who come to buy honey from these flowers and the colorful butterflies on them.

This stone valley has a height of up to 50 m and a length of up to 100 m is also a human made structure! I get startled!

Nature has its draw from this human hand!

As soon as the stone corridor ends, I see such a sight, I can't tell!”



Figure 4.101: Stone Corridor (Sinan, 2019)

“Station District and Altinkum Beach just under my feet. On the right, Yazla Quarter, Mountain Commando School Training Center, Eğirdir Poyraz Quarter, Kale Quarter, Canada and Yeşilada ...”¹⁰⁰



Figure 4.102: A view from railway after stone corridor. (Sinan, 2019)

¹⁰⁰ Bozkurt, R. (2015). *Eğirdir Gezileri Eğirdir Yazıları*. Eğirdir: Eğirdir Belediyesi.

Osman Şapçı and Recep Bozkurt explain very well what the station complex mean for the urban people and how they feel. In this respect, the whole space itself and the physical conditions of the buildings within it, as well as experiences, memories and responses of each of the senses are important for understanding the spirit of the place.

4.5. Conservation Decisions and Registration Process of Complex

Eğirdir Train Station Complex were unfortunately conserved at a very late date. On 29.08.2002, it was decided by Antalya Culture and Natural Heritage Conservation Board to register 239 islands as 1 parcel conservation area and its buildings as immovable cultural property that should be conserved under law No. 2863-3386. The reason for this decision was that the area was a physical document of railway investments made by foreigners in The Last Years of the Ottoman Empire and as a reflection of the understanding of infrastructure in the first periods of the Republic of Turkey.

The Central Operation Directorate (Afyon) stated in its letter dated September 19, 2002 that the Department of Immovable Goods carried out the evaluation studies for tourism for Eğirdir Station Complex, and that there could be various operating activities (renting, selling, etc.) for Eğirdir Station Complex, and the registration decision taken in this process could be demanded the cancellation with the thought that they could prevent them.

On 24 September 2002, the Directorate of Immovable Property reported to the Afyon Immovable Property Directorate that the Conservation Board had made a negative return to the objection. On 07 October 2002, the Directorate of Immovable Property, in a petition written to The Conservation Board, stated that 412.445, 42 m² of surface area Eğirdir Station Complex area which has 412.445, 42 m² of surface area is divided into zoning island in the zoning plan approved by the municipality of Eğirdir, the buildings in the area as idle and considering the evaluation of these buildings for tourism and demanded a reduction in the area of conservation.

On 25.10.2002 the Conservation Board re-determined the conservation area. As a whole, 239 islands and 1 parcel are designated as conserved areas. (415 islands remain out of scope.) At the end of these processes, the land registry records containing the following information were forwarded to the Directorate of Afyon immovable property on 05.02.2003 by Eđirdir Land Registry Office.

239 city block 1 parcel: 412445,42m²

21.06.1988: It's under zoning regulations.

27.07.1998: It remains within the military security zone.

01.09.1998: Annotation: According to the Article 7 of Law No. 2942

Note: In accordance with the decision No. 295 of Antalya Culture and Natural Heritage Conservation Board dated 25.10.2002, permission must be obtained for the changes to be made on these parcels.

239 city block 2 parcel: 69619,00m²

27.07.1998: It remains within the military security zone.

239 city block 9 parcel: 283,82m²

27.07.1998: It remains within the military security zone.

277 city block 1 parcel: 170,00m²

27.07.1998: It remains within the military security zone.

415 city block 2 parcel: 387,15m²

27.07.1998: It remains within the military security zone.

415 city block 3 parcel: 352,39m²

27.07.1998: It remains within the military security zone.

415 city block 6 parcel: 260,29m²

27.07.1998: It remains within the military security zone.

415 city block 15 Parcel: 298,25m²

27.07.1998: It remains within the military security zone.

415 city block 7 parcel: 307,30m²

27.07.1998: It remains within the military security zone.

The Ministry of National Defense requested that the areas under the control of the Military since the 1980s be transferred to them, and against this situation, the Ministry of Culture and Tourism stated to the General Directorate of the State Railway Enterprise on 25 March 2009 that the 56750 m² portion of 239 Island 1 parcel remained outside the boundaries of the conserved area and the registered area, and 239 Island 2 parcel, along with 277 Island 1 and 2 parcel are not in the scope of the Law no. 2863, therefore it didn't concern them.

Part of Eđirdir Station Complex, which was transferred to the privatization administration in 2009, was put up for sale in 2014. This situation, which the local dynamics have reacted to exceedingly, has made quite a sound. In this process, in the article written by the Ministry of Culture to the State Railways Enterprise General Directorate it is stated that the new parcel, containing registered railway buildings and conserved area, is 239 city block 11 parcels accordingly with the cadastral regulation, and also that the 239 city block 9 parcels, 415 city block 2,3,6,7 and 15 parcels are remained outside the conserved area. In 2014, all of the parcels belonging to TCDD, including 415 city block, were sold.

Cadastral Status

When the records of the Directorate of Land Registry and Cadastral were examined, 239 city block 1 parcel was not reached and it was understood that this parcel was divided into two parts as 10 and 11. There was no change in the 239 city block 2 parcel in the correspondence. 239 city block 3 parcel, where the district police department is currently located, was divided into 4 parts, 12, 13, 14, 15 on 19.02.2013. The ownership 239 city block 4 parcel has not been found, yet it is used by the military. 239 city block 5, 6, 7, 8, 9 parcels are not known to whom they belong, but they are not large enough to affect the integrity of the area. 239 city block 10 parcel was divided into 5 pieces on 25.06.2019 and 16, 17, 18, 19, 20 parcels were formed. The positions of 19 and 20 parcels couldn't get detected. 239 city block 11 parcel is a protected parcel that also includes station buildings. 239 city block 16,17,18 parcel belongs to TCDD but it is used by military lodgings.

277 city block 1 and 2 were not altered.

415 city block 1, 2, 3, 13, 14, 15, 16 parcels were merged into 19 parcels of 415 city block on 02.03.2015. 415 city block 4, 5 no changes were found in the parcels. 415 city block 6 and 7 parcels were combined on 16.04.2014 to form 415 city block 18 parcels. Information on 415 city blocks 8 and 9 could not be reached. Also, there were no changes to parcels 10, 11, 12 and 17 of the 415 city block.

Furthermore, when we look at the cadastral map today, we see that all 239 islands 11 parcels (359827,67m²) belong to TCDD. Other than that, the areas requested to be transferred by the Ministry of National Defense for training field purposes are used as military lodgings. As a result, the area of Eđirdir Station Complex, which has an area of 484.123, 62m², is today 359.827, 67 m².

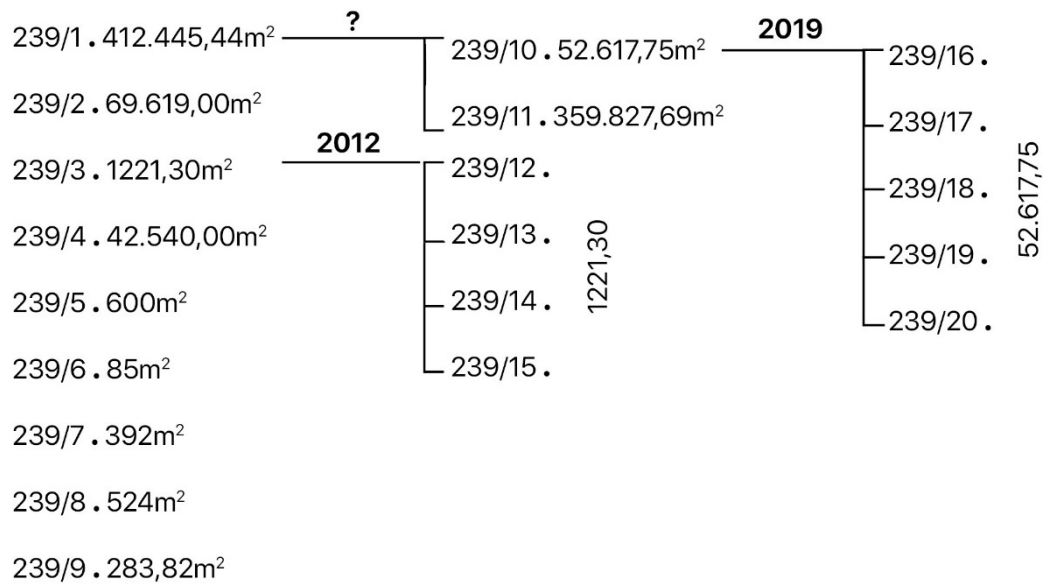


Figure 4.103: Parcels Status In 230 City Block (Sinan, 2019)

All correspondences and decisions are available in APPENDIX A, B and C.

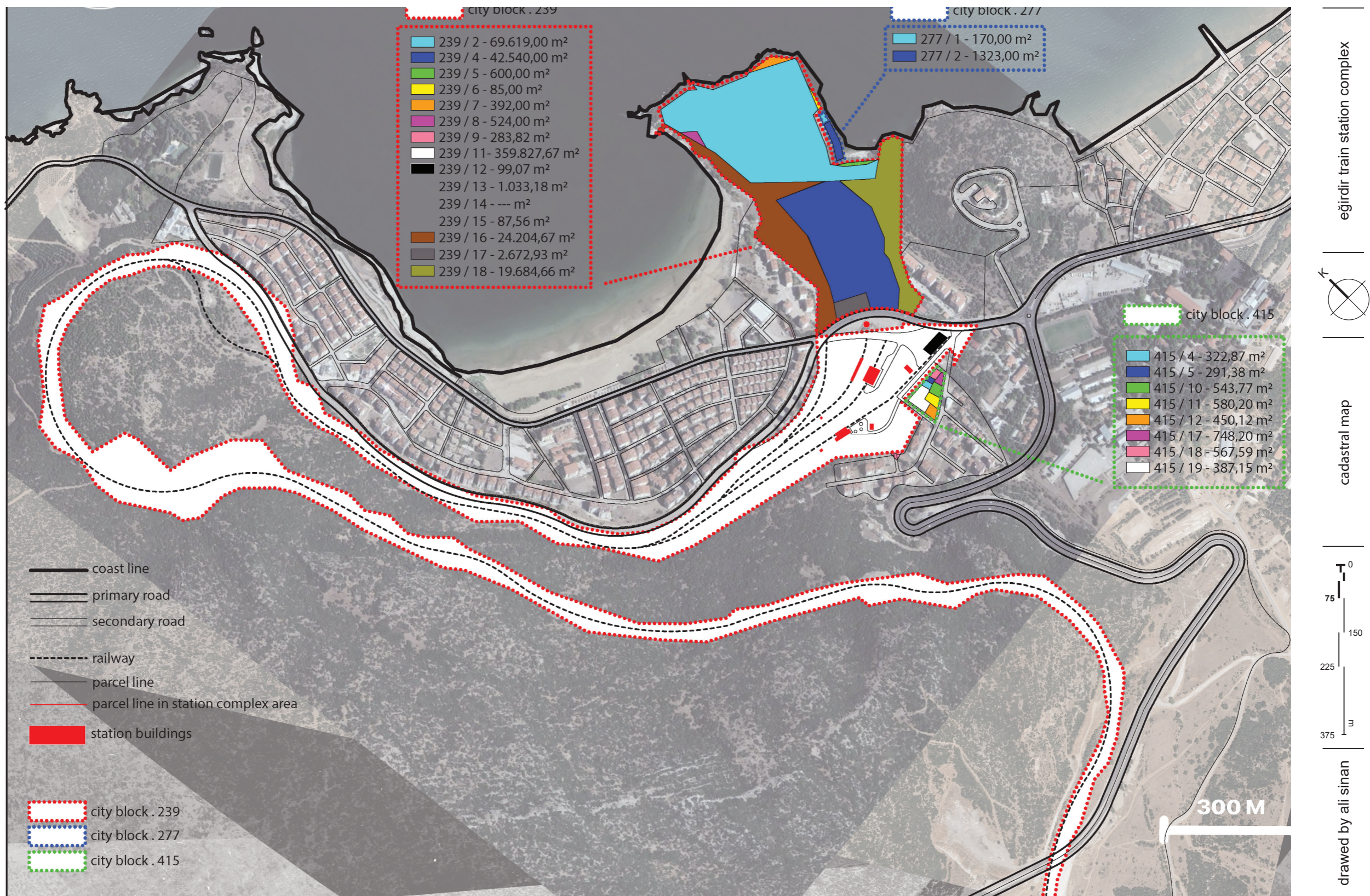


Figure 4.104: Cadastral map (Sinan, 2019)

Zoning Status

It is known that the master plan of Egirdir has been made three times. However, only the 1966 and 1981 master plans could be reached. When the 1966 master plan is examined, it is seen that 239 islands were not disintegrated and no program was added on them. On the other hand, it is observed that the Station and Altinkum neighborhood are all considered as camping and social facilities. When the copy of the 1981 master plan for 1988 is examined, it is seen that the road passing through the middle of 239 Islands was widened, that the parcels 2, 4 and 16 of 239 islands belonging to TCDD were allocated to the sports center and that the remaining area was divided into municipal and military areas.

Today, when we look at the zoning plan, we see that all the area except the station area is parcelled as residential area. In addition, 239 islands 11 parcels are allocated as a single part to the station, 239 islands 2 parcels are shown as TCDD training and recreation facilities, and the rest area is shown as a commando school site.



Figure 4.105: 1966 Zoning Plan - Partial (Eğirdir Municipality Archive)

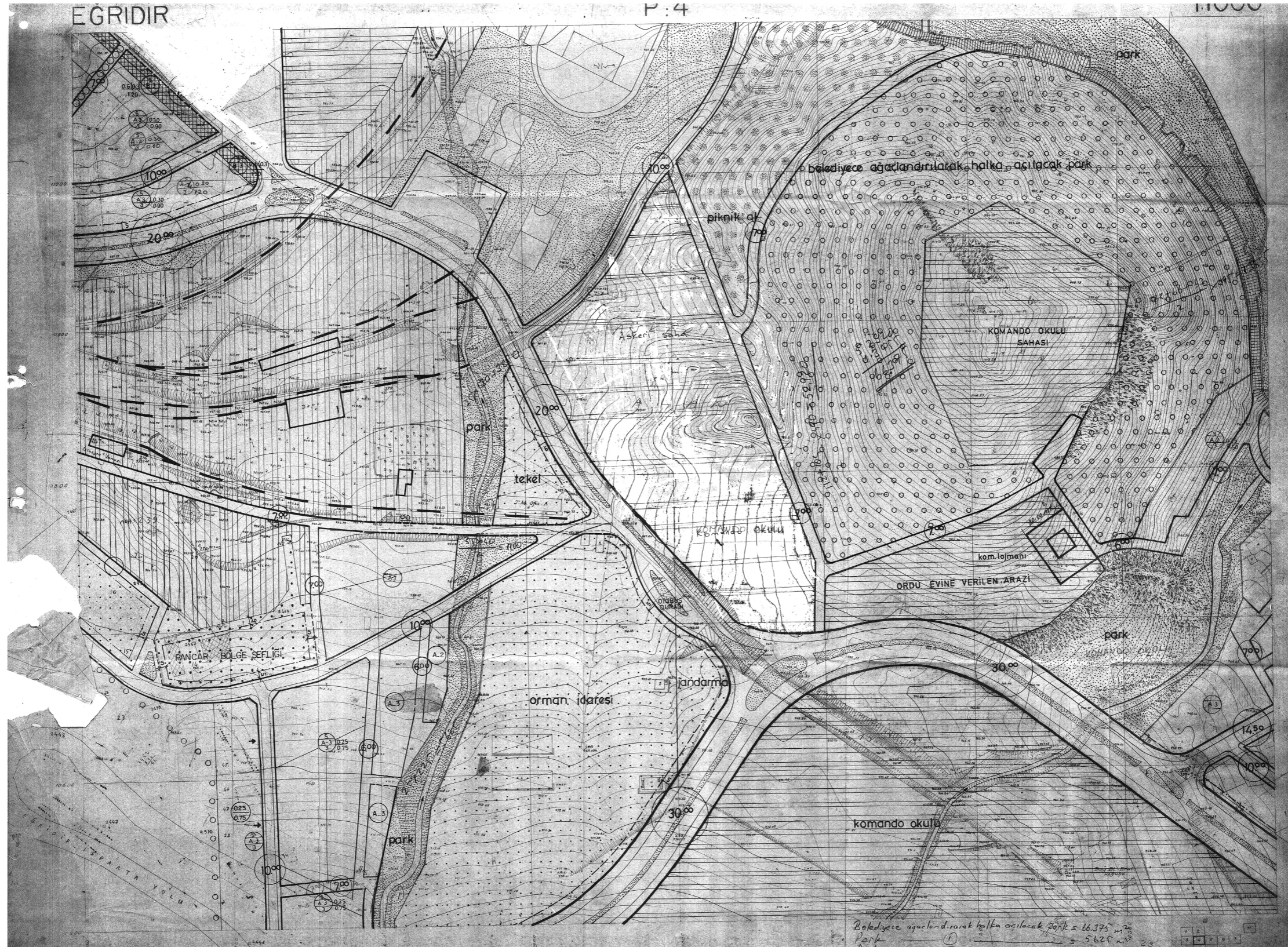
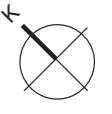


Figure 4.106: 1966 Zoning Plan (Adding) - Partial (Eğirdir Municipality Archive)

eğirdir train station complex



1966 Zoning Plan (Adding) - Partial



taken from eğirdir municipality



Figure 4.107: 1981 Zoning Plan - Partial (Eğirdir Municipality Archive)

CHAPTER 5

EVALUATION AND STRATEGIES OF EĞİRDİR TRAIN STATION COMPLEX

Eğirdir Railway Station Complex has great importance in terms of being the last station of the line, its location in the region, its relation to the city, and its buildings within its borders. Eğirdir Railway Station Complex, which has been standing for nearly 100 years, has great value both technically and socially.

As mentioned in the thesis content, the station is currently out of use but no security has been provided. Consequently, the buildings are getting old and destroyed. Even if all openings are covered with concrete in order not to enter the buildings, they have a serious risk of extinction.

This section focuses on the evaluation of the data given in the third and fourth sections using the concepts and general framework given in the first and second sections. As mentioned in the second part of the review of the conservation of railway heritage, the criteria given in the article published by ICOMOS are as follows;

- “1. A creative work indicative of genius*
- 2. The influence of, and on, innovative technology*
- 3. Outstanding or typical example*
- 4. Illustrative of economic or social developments”*

Railway heritage is a complex issue where too many parameters are combined. As Coulls (1999) points out, this complexity is divided into two main groups, namely “social and technical. In the social part, it is discussed under the titles of history, geography, economy, tourism, and archeology. In the technical part, the urban and architectural features of Eğirdir Train Station and its surroundings are discussed.

The Venice Charter (1964) mentioned that

“The concept of a historic monument embraces not only the single architectural work but also the urban or rural setting in which is found the evidence of a particular civilization, a significant development or a historic event. This applies not only to great works of art but also to more modest works of the past which have acquired cultural significance with the passing of time.”

In this sense, it would not be correct to evaluate Eğirdir train station separately from the context in which it is located. İzmir - Aydın railway is one of the first steps of Ottoman modernization. It is the symbol of the technological development of the late Ottoman period, and one of the examples of the early years of the Republic of Turkey's efforts to develop and become independent.¹⁰¹ Eğirdir Station is the last station in this line. Therefore, it is important to take a holistic approach to the evaluation part.

All data collected up to this section are based on different scales. Therefore, Eğirdir Train Station Complex was evaluated socially and technically on the scale of the line, city and building.

¹⁰¹ Köşgeroğlu, E. F. (2005). *An Approach for Conservation of Railway Heritage; Assessing and Experiencing the İzmir- Aydın Railway Line* (Msc Thesis). The Graduate School of Natural and Applied Sciences of Middle East Technical University.

5.1. Evaluation of Complex on Urban Scale

The role of evaluation in the decision-making process of cultural heritage is quite important. In particular, it is necessary to look at a wide range of disciplines, such as railway heritage, and to approach them in a holistic manner.

When evaluating the Eđirdir Railway Station Complex, it would not be correct to make an independent assessment of the station apart from the city in which it is located. Therefore, it would be correct to start the evaluating the complex from the upper scale to the lower scales.

History

“The industrial building or facility that has witnessed an important technological development in the life of a city or country has historical value due to its historical past and the period to which it belongs.”¹⁰²

Each of the railways and railway complexes is undoubtedly a technological development and has a great impact in many ways. However, Eđirdir Station Complex has its own unique values. The İzmir-Aydın railway line, where it is primarily the last station, is the first railway line in Anatolia. Therefore, many railway settlements on the İzmir-Aydın railway line are important. The difference of Eđirdir Station from other settlements is that it is primarily the last. Although there are hints that we can predict that the line will continue after Eđirdir, it will remain the last station in the future in line with the current and followed policies.

Eđirdir Railway Station Complex, which was used during the independence struggle, has a very different importance compared to other settlements due to its location and strategic importance in the war. The use of the people of Anatolia to collect and transport the soldiers and weapons from Eđirdir Lake and its environs to the western front, which is the most critical front of the struggle for independence, and to become

¹⁰² Madran, E. & Kılınç, A. (2008). Endüstri mirasının korunması ve değerlendirilmesi atölye sonuçları. *Korumada yeni tanımlar yeni kavramlar “endüstri mirası”, Zonguldak 6- 7 Nisan 2007*. TMMOB Mimarlar Odası Ankara Şubesi Yayın Birimi. p. 147-148

one of the biggest headquarters of the western front, has brought Eđirdir Station to an important point in history. In the protests against the sale of Eđirdir Railway Station Complex in 2014, the local people demanded that the Eđirdir Railway Station Complex should not be sold, rather conserved by prioritizing its contribution during the struggle for independence.

Undoubtedly, each station building has witnessed important events in the history of the city. The most important event after the liberation war at the Eđirdir Railway Station was the arrival of Atatürk to Eđirdir. This event that the local people mentioning of with enthusiasm is celebrated at the steel bridge there every year, and it is very important in terms of urban memory.

Investments made by foreign states for the Ottoman Empire, which could not be industrialized within the conditions of the 19th century, were in a sense the first steps of industrialization. As one of the important steps of these investments, İzmir-Aydın Railway is the first railway built within the borders of Anatolia. Eđirdir Railway Station is the last station of this historic road. However, the historical value of Eđirdir Railway Station Complex is not only an example of the industrialization steps of foreign states in Anatolia, but also has an important historical value in terms of its place and purpose in the national struggle period.

Geography

“If the industrial building or facility is unique in international, national, regional and urban sense and valuable in architectural and symbolic sense, it has environmental value.”¹⁰³

Especially as explained in Chapter 3, Eđirdir is a very rich campus in terms of its natural features. It has a rich natural heritage, especially Lake Eđirdir. It is thought

¹⁰³ Madran, E. & Kılınç, A. (2008). Endüstri mirasının korunması ve değerlendirilmesi atölye sonuçları. *Korumada yeni tanımlar yeni kavramlar “endüstri mirası”, Zonguldak 6- 7 Nisan 2007*. TMMOB Mimarlar Odası Ankara Şubesi Yayın Birimi. p. 147-148

that this. In fact, it would not be wrong to describe Eğirdir as the beautiful ending scene of a beautiful film. The İzmir-Aydın railway line runs through the most fertile lands of western Anatolia. When the railroad passes through the tremendously rich geography, it reaches Eğirdir and a huge lake appears after a hill. The most memorable moment of arrival in Eğirdir is the view of the lake. Similarly, to the Austrian zig zag railway, which is on the world heritage list, it descends a slope of about 40% from the hill to the lakeside. Both the lake view and the vegetation it passes through are magnificent. Today, this area is still used by the public for recreational purposes, and it is used primarily for beekeeping, and also as a grassland of animals.

The area on the lakeside, which is currently occupied by the military, is very characteristic. The lake side of the land, which is in the form of peninsula, still maintains its virginity. Looking at this area, which has survived to the present day without deterioration, the relationship between the lake and the railway is easily understood in the past. Today, Altinkum beach is located to the west of the peninsula and it is the biggest and touristic beach in the region. From a holistic perspective, Eğirdir Railway Station Complex cannot be considered as separate of İzmir-Aydın line. It is the continuation of this line, and even the end of the line. What is valuable is the continuity of the geographical wealth on the İzmir-Aydın railway line rather than the site itself. On the other hand, keeping the Eğirdir Station Complex intact and preserving its natural beauties will provide many alternatives for future program proposals.

Eğirdir Train Station is the last station of the line connecting İzmir to the lakes' region. This situation is very important both for country strategies and the region. Turkey's 4th largest lake and 2 large fresh water source is propolis, which take place on the shores of the lake, there is a central location in the region as in the past is a valuable case.

Economy

İzmir-Aydın railway has contributed greatly to the route it has passed since the beginning of its operation. The most important reason for this is that this line ends with İzmir Port. The products collected from the Port of İzmir and Western Anatolia were exported to Europe. As explained in Chapter 3, many products were also produced in Eğirdir and these products were loaded on trains to be sent to Europe. In this way, the railway has significantly enriched Eğirdir. On the other hand, the railroad reaching Eğirdir has enabled Eğirdir to become a center in the region. Factories and cold storage depots were established in Eğirdir and important public units were also established in Eğirdir.

Today, at this point, Eğirdir has become one of Turkey's most important centers in the production of fruits such as apple, cherry, shellfish exports, marble. Every day a lot of trucks distribute goods to all over Turkey and even to abroad. From this point of view, production is still carried out in Eğirdir and the products produced are delivered both in Turkey and abroad.

The other economic importance of the area is the land value. The land that can build buildings in Eğirdir is quite insufficient. The large land suitable for construction of Eğirdir Station attracts the attention of many people and institutions today. Accordingly, land prices are high.

Tourism

İzmir-Aydın railway line passes through the very campus has developed in terms of Tourism. Eğirdir is one of these cities. Eğirdir is a tourist center both in terms of its natural features and its location.

There can be found different occupations in each season in Eğirdir, advertising itself with the “paradise of alternative tourism” slogan, and Eğirdir is only about 150 km away from Antalya, one of the biggest tourism center of Turkey.

When you look at the route of the İzmir-Aydın railway line, the relationship of the line with the archaeological sites, as well as its natural beauty, is also visible. All this brings to mind the Eastern Express, which today has difficulty finding tickets due to intense interest. The city of Eğirdir, where the İzmir-Aydın railway ends, and the city of Eğirdir have a lot of potential in terms of tourism.

Archaeology

The importance of archaeological sites in determining the route of the railways in the 19th century and the relationship between the İzmir-Aydın railway and archaeological sites are examined in Chapter 3. It is unclear whether archaeological sites have an impact on the location of Eğirdir Railway Station Complex. In the archaeological sense, Eğirdir Station Complex occupies an important and valuable position within the framework of the İzmir-Aydın line.

As a result, railway heritage is a multidisciplinary issue that needs to be explored not through a singular discipline but a combination of many different experts. Since this thesis focuses on the urban and structural contents of Eğirdir Train Station Complex, it is not possible to go into much detail on other subjects. To make a holistic evaluation, it is important and necessary to act with a multidisciplinary team.

Social

“Industrial heritage has social value because it causes significant changes in every field in the lives of ordinary people.”¹⁰⁴

The railways are undoubtedly affecting the social structure of the society. Especially with the industrial revolution, there have been great changes in the daily lives of people and the social life of cities. Eğirdir Railway Station Complex is one of the most important tools that socially change the geography in which it is located.

¹⁰⁴ Madran, E. & Kılınç, A. (2008). Endüstri mirasının korunması ve değerlendirilmesi atölye sonuçları. *Korumada yeni tanımlar yeni kavramlar “endüstri mirası”, Zonguldak 6- 7 Nisan 2007*. TMMOB Mimarlar Odası Ankara Şubesi Yayın Birimi. p. 147-148

Firstly, the city, which has strengthened its connection with metropolitan cities such as Izmir and Istanbul, has entered the modernization process. The process of modernization has been changed both by the use of structural materials and techniques, which the people have not seen before, in peoples' own buildings as they notice these materials in the station buildings and by their daily life habits.

After a while, with the use of Eđirdir Station Complex as the scenes of the films, the people who are known in the society came to Eđirdir and interacted with the public.

On the other hand, many state institutions have started to be established with the convenience of transportation. For example, thanks to the arrival of various institutions such as the most bed capacity bone diseases hospital of that period in the middle east, mountain commando school training center, and fruit research institute, Eđirdir's the city population increased, habits changed and it is economically enriched. In this sense, Eđirdir Station Complex has a social value.

Originality

“If the position of the industrial building or facility, architectural elements, building material, construction system and interior equipment has reached the present day without deterioration, that means that it has the value of originality.”¹⁰⁵

Perhaps the most unique value of Eđirdir Station is its location. The route, which slopes from the slope of a mountain down to the lake level and curves with a magnificent view and ends right next to the lake, is partially fragmented, but it still maintains its originality. In addition, although the parcels are sold piece by piece, the station complex maintains its integrity with its buildings.

¹⁰⁵ Madran, E. & Kılınç, A. (2008). Endüstri mirasının korunması ve değerlendirilmesi atölye sonuçları. *Korumada yeni tanımlar yeni kavramlar “endüstri mirası”, Zonguldak 6- 7 Nisan 2007*. TMMOB Mimarlar Odası Ankara Şubesi Yayın Birimi. p. 147-148

VALUES						PROBLEMS	POTENTIALS	STRATEGIES	
REGIONAL SCALE	HISTORY	GEOGRAPHY	ECONOMY	TOURISM	ARCHAEOLOGY	SOCIAL	<ul style="list-style-type: none"> TRANSPORTATION POLICY FRAGMENTED MANAGEMENT OF LINES NO HOLISTIC APPROACHES PROJECT PROCUREMENT METHODS 	<ul style="list-style-type: none"> UNIQUE ROUTE INTERNATIONAL BRANDING 	<ul style="list-style-type: none"> REVIEW OF TRANSPORT POLICIES INTEGRATED MANAGEMENT OF RAILWAY LINES MULTIPLE MANAGEMENT OF RAILWAY HERITAGE MULTIDISCIPLINARY PROJECT TEAMS REVITALIZATION OF EĞİRDİR TRAIN STATION COMPLEX AS PART OF İZMİR-AYDIN LINE REINTEGRATION OF RAIL AND LAKE TRANSPORT
CITY SCALE	HISTORY	GEOGRAPHY	ECONOMY	-	-	SOCIAL	<ul style="list-style-type: none"> TOPOGRAPHY OF CITY ZONING PLAN HOUSING DENSITY MILITARY 	<ul style="list-style-type: none"> TOURISIM ALTERNATIVE TOURISM CENTER TRANSPORTATION CENTER 	<ul style="list-style-type: none"> REINTEGRATION OF THE COMPLEX INTO THE CITY AND STRENGTHENING ITS POSITION IN URBAN MEMORY PREPARATION OF CONSERVATION ZONING PLAN COVERING ALL CULTURAL AND NATURAL HERITAGE URBAN DEVELOPMENT STRATEGY GETTING IN CONTACT WITH THE ARMY TO ENSURE THE INTEGRITY OF THE AREA
COMPLEX SCALE	HISTORY	GEOGRAPHY	ECONOMY	-	-	-	<ul style="list-style-type: none"> CONSERVATION STATUS ZONING PLAN DISINTEGRATION OF THE AREA OUT OF USE 	<ul style="list-style-type: none"> UNIQUE STATION COMPLEX ALTERNATIVE ORGANIZATION CENTER RECREATION CENTER SIMULTANEOUS USAGE BY MANY PEOPLE 	<ul style="list-style-type: none"> DETAILED DOCUMENTATION STUDIES OF THE COMPLEX WITH ALL OF ITS ELEMENTS REGISTRATION OF THE ENTIRE AREA AS A PROTECTED AREA INTEGRATION OF THE COMPLEX TO DAILY LIFE MAINTAINING THE ORIGINAL USE OF THE COMPLEX IS PREFERABLE ENSURING THE INTEGRITY OF THE COMPLEX IN CASE OF RE-USE THE RELATIONSHIP OF THE AREA WITH THE LAKE SHOULD BE STRICTLY PRESERVED.

Figure 5.1: Evaluation of Complex on Urban Scale (Sinan, 2019)

5.2. Evaluation of Complex on Building Scale

In order to better evaluate of the Eğirdir Station Complex, a separate study was conducted on the building scale.

Sixteen types of values were selected for value assessment on the building scale. These are; Age value, historical value, authenticity value, technical/artistic value, document value, socio-cultural value, political value, aesthetic value, symbolic value, identity value, commemorative value, educational value, group value, use/functional value, market value and continuity in use-value.

Age value can be described as the long-time existence of the cultural asset which is more valuable. All the buildings of the Railway Station were built between 1910 and 1912. The trees in the vicinity also appear to have existed since 1943, considering that they could be observed in 1943 air photos. Therefore, Eğirdir Railway Station buildings and open spaces have an age value. There is no value in this sense of the iron shed in the area.

Historical value is the relation of a cultural asset with historical events and/or with a specific period in history. Eğirdir Railway Station buildings and open spaces have been the main actors of many important events, especially the First World War and the War of Independence. Its use on the Western Front also has significance in the history of the country. There is no value in this sense of the iron shed in the area.

Commemorative value is related to personal or public memories of cultural assets.

For Eğirdir people, station buildings and open spaces are an important part of the collective memory of the city, so the memory value can be determined. Users' memories of buildings and open spaces are acceptable for the entire space.

Use/functional value refers to the ongoing or potential use of the cultural property for any purpose. Eğirdir Railway Station buildings and open areas have not been used since 2002. However, considering the physical and spatial quality of buildings and open spaces, they have the potential to be used for many purposes in the future.

Aesthetic value is the recognition of a special quality in the style, beauty, and art of cultural assets. Each of Eğirdir Railway Station buildings is qualified in terms of design, form, material and construction technique.

Also, the railway station buildings and bridge have become the symbol of the city most of the time. It can be said that the station and its environs, which are included in many photographs taken to describe Eğırdır, have aesthetic value in this sense. There is no value in this sense of the iron shed in the area.

Authenticity Value is the originality of cultural assets related to form, design, material, and usage, function, tradition, location, technique.

In this sense, Eğırdır Train Station buildings each conserve their original spatial relations, construction systems, and materials. The buildings have seen little change periodically and rarely functionally changed.

Although the bridge inside the buildings is not defined as a closed space, it still maintains its originality. When we look at the open spaces, the courtyard that remains between the station building and the guest house, and especially the road to the station, maintains its originality. There is no value in this sense of the iron shed in the area.

Technical/Artistic value is related to the application of cultural assets based on experience, the importance of the technical or structural concept, the use of materials and labour. In this sense, each of Eğırdır Railway Station buildings is important buildings of the period. Compared to the period of its construction, steel structures are considered very innovative. The guest house and ganger buildings are made of stone and have wooden roofs. As with other buildings, stonework and details are very valuable for these buildings. There is no value of the iron shed in the area in this sense.

Sociocultural value is the social evaluation of cultural assets. Social areas are political, identity, memory, religious, spiritual. Eğırdır Train Station is currently not used. However, it has great importance in the lives of people living in the region in the past.

Today, there are several activities such as Atatürk's arrival in Eğirdir, which is celebrated in front of the steel bridge at Eğirdir Railway Station. There is no value of the iron shed in the area in this sense.

Identity Value is related to the impact of cultural assets on society and the city. In this respect, one of the most important reasons why Eğirdir is such a developed city after the Republic is the Eğirdir Railway Station. Moreover, especially in the early years of the republic, transportation is very valuable because communication is technologically weak in the process of modernization and democratization of society. In this sense, Eğirdir Railway Station has been a major factor in the public's adaptation to this process easier and faster.

Group value is related to the combination of various production and service units such as industrial complexes or different types of buildings such as social facilities. Many buildings serve different purposes in the period when they are used in Eğirdir Railway Station. These different buildings come together to form a whole, namely Eğirdir Train Station. Although buildings are not used today, their spatial quality is different because their construction aims are different. Therefore, the group value can be specified for buildings and open spaces.

Market Value relates to the monetary value of cultural assets that can be traded and priced. Today, one of the biggest reasons for the danger of the destruction of the Eğirdir Railway Station is because of its great economic value. In this sense, it is possible to indicate the market value of Eğirdir Railway Station and its surroundings.

As a result, it is understood that the Eğirdir Train Station and its environs have many values. All buildings and open spaces, except for the shed, which is assumed to have been placed recently in the area, must be considered value for the items examined for the landscape elements that define the open spaces.

	Age value	Historical value	Authenticity value	Technical/artistic value	Document value	Sociocultural value	Political value	Aesthetic value	Symbolic value	Identity value	Commemorative value	Educational value	Group value	Use/functional value	Market value	Continuity in use value
Main Station Building	•	•	•	•	•	•		•			•	•	•	•	•	•
Guest House	•	•	•	•	•	•		•			•	•	•	•	•	•
Bunkhouse	•	•	•	•	•	•		•			•	•	•	•	•	•
Warehouse Building	•	•	•	•	•			•				•	•	•	•	•
Cer Building	•	•	•	•	•			•				•	•	•	•	•
Office (Old Wagon)	•	•	•	•	•			•			•	•	•	•	•	•
Water Reservoir	•	•	•	•	•			•				•	•	•	•	•
Toilet	•	•	•	•	•			•				•	•	•	•	•
Steel Bridge	•	•	•	•	•	•		•	•	•	•	•	•	•	•	•
Turntable	•	•	•	•	•			•				•	•	•	•	•

Figure 5.2: Values of Complex on Building Scale (Sinan, 2019)

5.3. Problems and Threats of Complex

As a result of the transportation policies implemented since the 1950s, rail passenger and freight transportation was left unfortunately ignored. Our railways and railway campuses located in Turkey are not given enough importance. As a result, some stations are shut down, while others are in poor condition due to incorrect interventions. İzmir-Eğirdir line was also negatively affected by this process. Some of the railway stations on the İzmir-Eğirdir railway were closed and some were left to decay by being abandoned to their destiny. However, the İzmir-Eğirdir line is the first railway line of the Anatolia, established in the Ottoman Empire period. Such an important line should be handled in a holistic way and a holistic strategy should be developed.

One of the main reasons of these fragmentary management and strategies is undoubtedly that the TCDD is divided into sub-regions and the historic railway lines are managed by different regional directorates. TCDD has divided the railway network into

seven separate regions, leaving the Alsancak-Kaklık between the İzmir-Eğirdir railway line to the management of 3rd region and the area between Kaklık-Eğirdir to the management of 7th region. These fragmentary management and strategies are a major challenge for our historic railway lines.¹⁰⁶



Figure 5.3: TCDD regional directorates
(<https://www.trenhaber.com/images/upload/tcdd-bolge-mudurlukleri-sinirlarini-gosteren-harita-2016-trenhabercom.jpg>, last visited on November 2019)

Apart from this situation, TCDD owns all railway properties and other equipment. It undertakes its own maintenance and repairs. The restoration works are carried out with the project services purchased through tender procedure. Although the institution works meticulously compared to other institutions, it is not correct to obtain methodically qualified results. Open tenders were taken into account for the restoration works of Eğirdir Railway Station Complex, and the contractor was selected among the offers

¹⁰⁶ Ekizoğlu, G. (2012). *Demiryolu Yerleşkelerinin Endüstriyel Miras Olarak Korunma Sorunları: İzmir- Aydın Hattı Üzerindeki Demiryolu Yerleşkeleri Örneği* (Msc Thesis). The Graduate School of Natural and Applies Sciences of Dokuz Eylül University. p.339-340

given without any further evaluation at the most favorable price. Therefore, there are major problems in the project-acquisition model and methodology for such qualified and unique areas.

Another important problem of the Eđirdir station complex is the master plan and other government institutions. When we look at the 1966 master plan, Eđirdir Station Complex was taken as a whole and the environment was reserved for recreational purposes in a way that would not disturb the original natural and historical texture. However, the military first occupied the part of the area between the road and the lake between 1970 and 1980 with the claim that training would be carried out there. This is the first fragmentation of the area. Even though the area mentioned in the master plan of 1981 was left to the use of the station at first, military did not leave the area, and today it occupies the station area occupied for educational purposes and uses it as a military lodging area. In addition, with the 1981 master plan, the İstasyon and Altinkum Neighbourhoods were opened for reconstruction. The railway bridge, which has become one of the symbols of Eđirdir, has been blocked and the original perception has changed due to the fact that the railway border has been multiplied. With the completion of the housing areas and the transfer of Eđirdir Railway Station Complex to the privatization administration in 2009, some areas were put up for sale and sales were completed in 2014. In this direction, the surrounding area becoming a housing stock and losing its original texture; the railway station and iron bridge becoming invisible between these buildings; and the sale of the area by fragmentation constitutes big problems.

In addition, the railway station and Barla road has been divided into two parts and lost its integrity.

When we look at the building scale, the areas and buildings are not used today. Due to the lack of usage and the lack of security, vandalism has begun. As a result, the station buildings of Eđirdir got through many huge hazards, especially the fire. In order to prevent these hazards, although all openings of the buildings were covered with concrete, deterioration could not be prevented. The last scissors (rotary bridge) was

tried to be cut and stolen, and thieves were caught at the last moment. The area is becoming increasingly depressing and is becoming dangerous for the people living in the surrounding area and for people who use it in their daily lives. This situation adversely affects the conservation process of Eđirdir Station Complex.

5.4. Potentials and Strengths of Complex

Structurally, station buildings are still standing and can be returned to daily use with small interventions. There is the potential to host many programs in the option of re-functionalization due to the fact that the physical condition of the buildings is not too bad and they have different spatial qualities with wide openings or small divisions.

Within the framework of this study, solutions were sought to revitalize through the mission of Eđirdir Train Station Complex on the İzmir-Aydın route, and suggestions were made to re-associate with the region and city where it is located and to strengthen its position in urban collective memory.

Before presenting the suggestions, it would be useful to look at the overall structure of the organizations in Turkey and to make an assessment on the railway heritage perspective. TCDD has an important role in the big organization in which Eđirdir Station Complex are located. TCDD continues its administration by dividing into regions. These regions are separated without considering line continuity. İzmir-Eđirdir line faces two regions. Alsancak-Kaklık is within the jurisdiction of the 3rd Region while Kaklık and Eđirdir are within the 7th Region. This way of line fragmentation prevents both inventory, maintenance-repair and restoration to work in a holistic way. In this sense, the areas of authority and responsibility of the sub-regions of TCDD should be revised or joint working teams should be established for such fragmented lines.

Although the Ministry of culture is influential in registration decisions, only the Ministry of Transport develops a strategy on railways. As mentioned in the beginning, the railway is a versatile system. Therefore, within the mechanism of decision-making and strategy production, other ministries as well as the transport ministry should take on duties and powers. This upper mechanism should be organized together with local

dynamics and they should work to conserve our railway heritage. At this point, the United Kingdom, where the three organizations (National Railway Museum, the Railway Heritage Trust and the Railway Heritage Committee) work in coordination, can be seen as an example.

Today, the eastern express has become a global brand and has become an important tourism tool. “Göller Ekspresi” is also highly likely to become a brand and serve. Starting from Izmir, which is one of the most popular cities of the world and ending with Eğirdir, this line has a lot of potential for tourism. A railway campus, which is located on the shores of a lake such as Eğirdir Train Station Complex, can rarely be found. In this direction, together with the station, Eğirdir which has become the center of alternative tourism is a great potential.

It is possible for the complex to have a very important place in the promotion of the country thanks to its important heritage with its context, its route and valuable structural elements such as bridges on the route, landscape and natural richness.

Eğirdir Station Complex also has many potentials for the citizens. The railway has become a good barrier between the dense residential fabric and nature. Thus, natural life remains intact. This area can be an important recreational area for the citizens, also it has features that could easily make room for social activities such as youth camps and festivals.

5.5. Conservation Strategies of Eğirdir Train Station Complex

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5.5.1. İzmir-Aydın Railway and Station Complex

First, it would be right to establish the future strategy of the İzmir-Aydın railway line in terms of preserving the Eđirdir Train Station Complex and to develop proposals for the Station in that direction. The expedition between İzmir and Eđirdir on the İzmir-Aydın Railway, which ends with Eđirdir, is called the lakes express. The route, which was known for this expedition, was reopened in October 2019 after being closed for a long time. However, the last station of this route was planned as Isparta, not Eđirdir. The opening of the lakes express to transport is a very important development for Turkey and the railways of Turkey, but the most important feature of the lakes express

is the route it passed while going from Izmir to Eğirdir. The route has a holistic archaeological-natural degree. One of the most important parts of this value is undoubtedly The Last Station, Eğirdir.



Figure 5.4: Route of “Göller Ekspresi” (Sinan, 2019)

Eğirdir holds a very important place not only in terms of railway but also in the sense of land and lake transportation. Due to the central location of the city and fertile soil / water products, it was also located on the route of trade routes. The railway station complex is located at the point where railway, highway and waterway transportation come together.

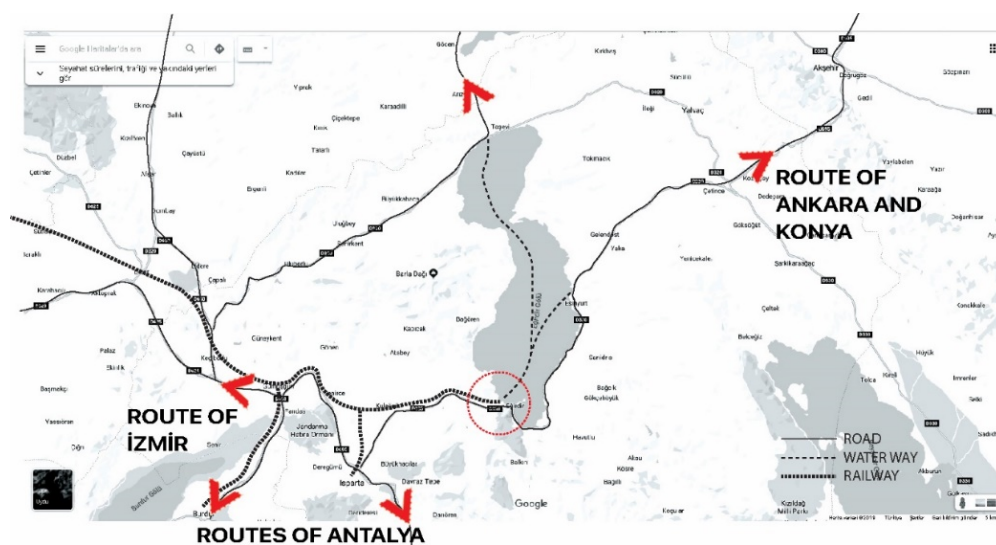


Figure 5.5: Transportation routes around the Eğirdir (Sinan, 2019)

In this respect, the first step of the conservation strategy of the Eğirdir Train Station Complex is to maintain the complex as an important part of the İzmir-Eğirdir line in the future and to reintegrate it with land and lake transportation. In order to achieve this integration, Eğirdir Lake passenger and freight transport must be reactivated. Agricultural and economic activities in and around Eğirdir will ensure the sustainability of rail transport. In addition, this situation will provide not only economic but also tourist value. Besides becoming an important tourism route with its natural and archaeological riches, the İzmir-Eğirdir road can make significant contributions to the country and region by branding it “Göller Ekspresi”. A very unique model can emerge when taken together with the transportation on the lake. Eğirdir Train Station, besides its branding as "the last station", will also be included in the memories as the port of the railway connecting with the lake.

5.5.2. City and Station Complex

When we look at the station on an urban scale, it is observed that the biggest problem stems from the 1981 reconstruction plan and the military areas. Altinkum and Station Quarter are new neighborhoods established after 1980. These neighborhoods have a master plan but lack urban design. Altinkum and the Station Quarter are used only as residential areas and urban accessories are not well constructed within the area. The presence of the station in the dense residential area is a great danger in terms of housing development of the area, while the inclusion of it in the city poses a great difficulty. In addition, the station remains quite large on the neighborhood scale, while on the urban scale it remains far from the center where daily life is active. In this direction, it would be right to consider the station together with all the dynamics of the city and to develop recommendations.

Tourism: There are two of the most popular regions of Eğirdir in terms of tourism. One of them is Yeşilada actively providing accommodation and gastronomic service during the four seasons, while Altinkum Beach offers service especially local tourists during the summer months.

Health: Despite not being used as extensively as in the past, Eğirdir Hospital for bone diseases is still of great importance and it contributes to the economic and social development of the city.

Economic: The city has two main economic regions. While the city center mostly consists of small commercial houses, economic activities based on industry and agriculture are carried out in the bridgehead area.

Education: Besides the fact that there are no major universities in the city, there are some parts of the university located in Isparta. These sections are scattered throughout the city in a very fragmentary manner and a good infrastructure based on education has not been established.

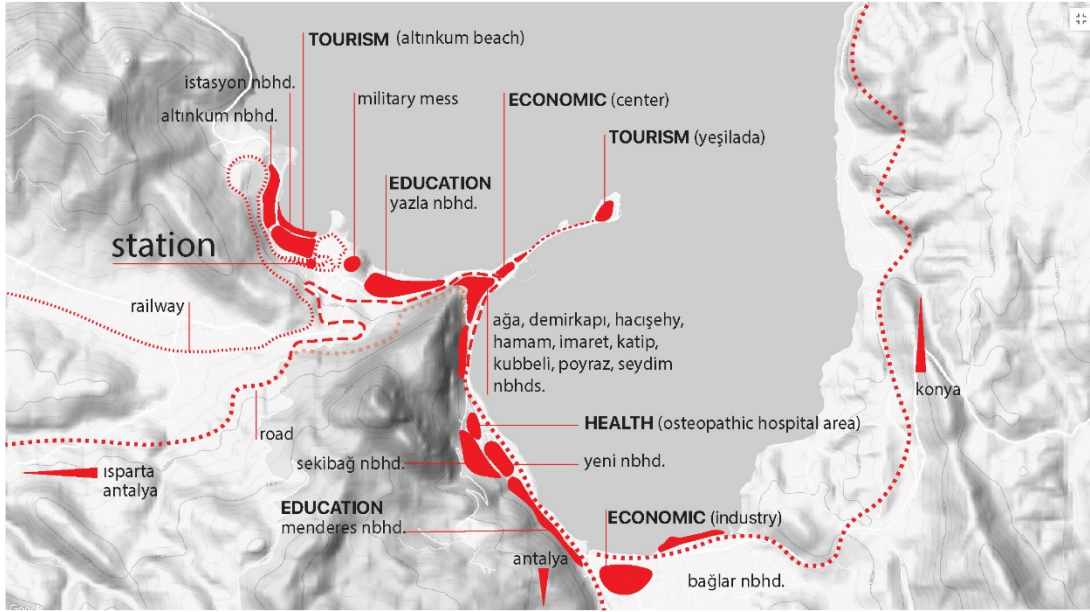


Figure 5.6: Activity zones of Eğirdir (Sinan, 2019)

As can be seen in Figure 5.6, the city dynamics are mainly located in the center and southeast of the city, and in this sense there is a large gap in the station and its surroundings. This gap is very valuable for Eğirdir, which hosts education, health, tourism and economic activities but is not suitable for expansion due to its topographic features. It would not be right to use this value for a single purpose.

Multipurpose space

The Eğirdir Train Station Complex is the “balance tank” of this city, which is stuck in itself, and it is necessary both for the full use of the values and potentials of the complex and for the active and sustainable use of the urban spaces. Although the city provides services such as tourism and health education, it does not have enough equipment to provide these services. For example, there are no conference centers to support tourism and education, local / foreign tourists or festival areas for students, camp centers, museums, libraries. The area will undoubtedly be restored to its former value when taken up again with an infrastructure capable of covering all objectives.

5.5.3. Eğirdir Train Station Complex

The buildings on the complex and its location consist of three main parts. Each of the parts has different values and potentials, but unfortunately they cannot be perceived as a whole. In this respect, the general approach should be to develop conservation strategies while maintaining integrity again.

Area 1: The most important structural element of this area defined by the railway is the steel bridge. It is highly valuable as the context and construction system in which it is located. The face of the bridge facing the city was closed with a residential fabric. However, the bridge holds an important place in the history of Eğirdir and also has a symbolic value. In this respect, the bridge and its surroundings should be treated in a way that people can easily access and spend time with, and it should be designed for the realization of various activities such as celebrations and ceremonies. With the necessary lighting elements and urban accessories, the symbol value should be brought to the fore by making it more accentuated day and night. The rest of the area is quite impressive with its natural features. The natural slope of the railway is curving to the edge of the lake, accompanied by the rare landscape in the world. This route should today be used not only as a railway but also for recreational purposes. In this direction, the area should be used for recreational purposes of the local people during the day as well as for various sports activities such as races, training, camps and infrastructure

requirements should be met. The area should never be damaged by construction and its natural structure should be conserved as much as possible.

Area 2: This area is where the original buildings of the complex are located. As described in Chapter 3, there are unique buildings of the complex and these buildings are in good physical condition. The buildings have different spatial and structural qualities. Thus, being able to support many different programs in terms of architecture is very valuable in terms of usability of the area. Buildings can be small parts of a large organization, or they can serve different organizations separately. The most important point here is that the integrity of the area should not be spoiled programmatically and spatially. In line with this;

Main Station Building: It should be renewed for its original use and continued to be used as a boarding-landing station for the passengers of the Izmir-Eğirdir expedition.

Guesthouse: The guesthouse may not be needed as the station complex is no longer out of town and there is no shortage of accommodation within the city. In this respect, the building can be used as an office unit for the general management of the complex due to its location in the center of the area, its proximity to the entrance of the complex, the presence of many rooms and wet area in the planning building and the relationship it has with the main station building.

Bunkhouse: This building used for railway workers has its own open space and kitchen. Building area alone can be used as a restaurant, office and so on. or it may undertake different programs in other organizational scenarios to be developed over the entire area.

Warehouse Building: It is the building with the widest space of the area. This building, whose bearing system is made of steel, is used for purposes such as storage, and has wide openings as well as entrance and exit doors and areas are very wide. The building can serve wide-ranging organizations and can be easily used with programs such as museums and exhibitions.

Cer Building: This building, which is thought to have been built and used for repair and maintenance of trains, has a long and thin place. The building, which has a door at the beginning and end, is mostly used for exhibitions, museums, libraries and so on, unlike the warehouse building that can be used for wide-range organizations.

Office (Old Wagon): This building, where an old wagon was used as an office, can again continue to be used as a venue used by a small number of people, such as office, reception office, etc.

Water Reservoir, Bridge, Turntable; in addition to the use of other buildings in the area for a program, providing information about the original use of the area is valuable in terms of the reflection of the technology of the period and it is necessary to preserve it.

Considering the areas and buildings as a whole, the area is suitable for use under a single but large organization such as film set, education center, culture conference center, youth center. However, even if it belongs to a single organization, it is open to the use of other dynamics of the city and it should continue to serve as a “balance tank”.

Area 3: This area was first separated from the complex by the Egirdir-Barla road and then used for other administrations and different purposes other than the railway. The area is currently used as a lodging area by the military, while traces of original use, such as the railway, have disappeared. One of the most important features of Egirdir railway complex that makes it unique and valuable is undoubtedly that the water rail reaches the roadside and combines with lake transportation. In this direction, first of all, rail routes should be re-established and their original relations should be re-established.

There are two separate lodging campuses on the coastal and inner parts of the area. Particularly on the lakeside, the lodging is old and damages both the lake's special provisions and the original railway relations in the area. In this sense, the housing on the lakeside must be destroyed. There is no building prior to military lodgings and its

ownership belongs to the railways as a whole. Although it is not an exact document, it can be considered that the site is constructed as the site of the station complex considering the location of the station buildings and the land situation. Examining both the planned areas master regulations and new laws (like special provisions) and the 1980 Eđirdir Master Plan, the area should be used for recreational purposes and the construction should be handled within this framework. Considering the integrity and general building of the complex, it will be true that the necessary construction should occur in this area without interfering with areas 1 and 2 to the extent permitted by the laws and regulations.

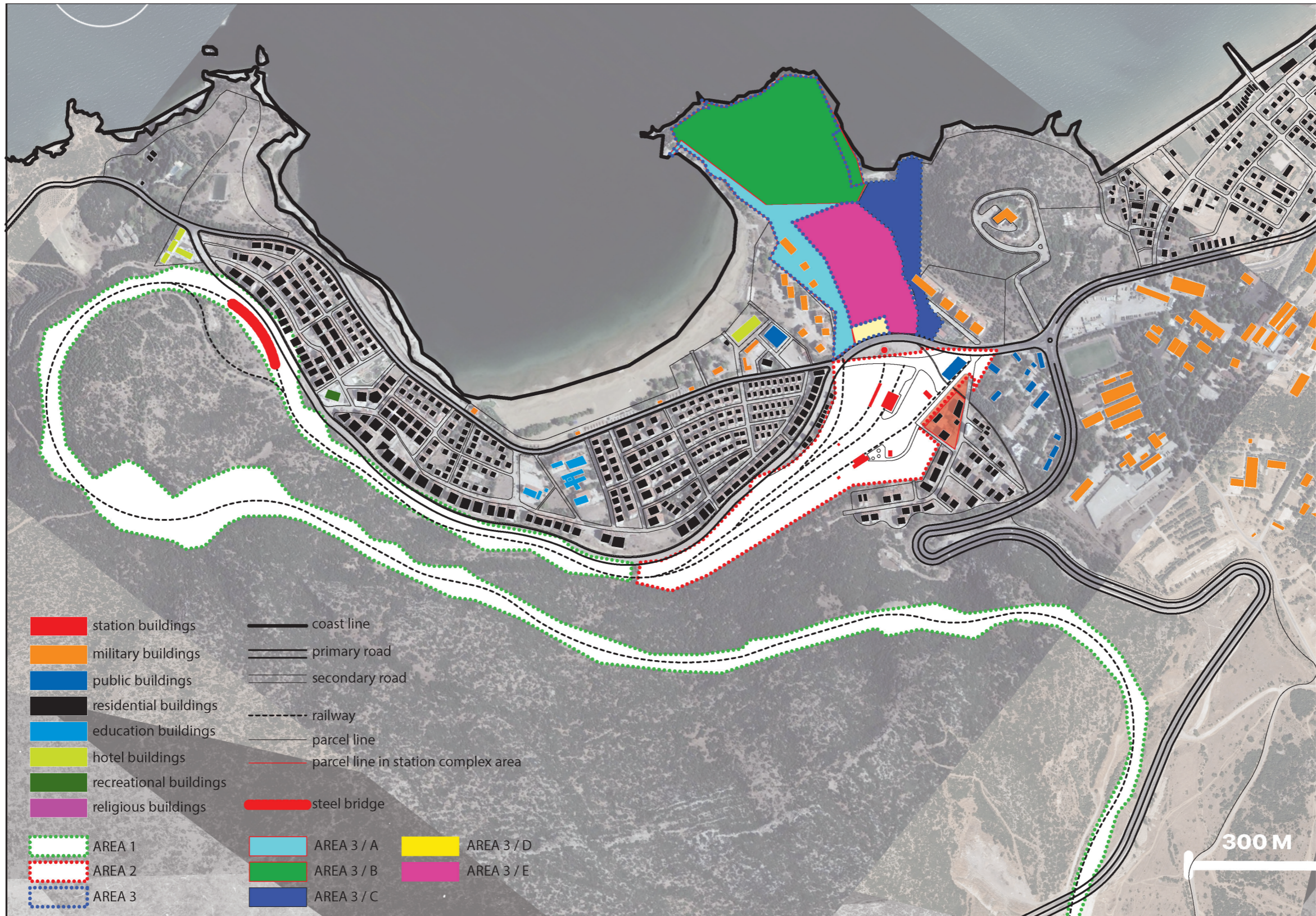


Figure 4.107: Proposal Map (Sinan, 2019)

CHAPTER 6

CONCLUSION

One of the most important inventions that changed the course of history, the railway has affected industry, trade, war, peace, culture, art, literature, in short, almost every subject. The invention of railways symbolizes the birth of the modern age with the development of technology before other life-changing aspects (electricity, telephone, motor vehicles, etc.).

With the use of railways first in England and then all over the world since the 1830s, the mass loads produced by the industrial revolution are brought to far distant places, and it led societies to develop not only in economic terms but also in social and cultural areas. In addition, the railway has become one of the most important elements of gaining the upper hand even in war. Also, railroad has become the most common and powerful of colonial vehicles.

The Ottoman Empire, on the other hand, was unable to keep up with the developments of the industrial revolution, still struggling to gain land or maintain its territorial integrity, and was in a very bad state economically. The European countries such as Britain, France and Alma, which captured the industrial revolution in a good way, took advantage of the Ottoman Empire and brought the railway to the Anatolian territory. The main purpose of these states was to exploit these fertile lands.

The Izmir-Aydın line, which was built in Anatolia and whose last station was Eğirdir, also established for the same purposes. It is thanks to this line that the British merchants were outnumbered and the transport of goods and raw materials from Izmir, a port city, to Europe was made. On the other hand, the cities located in these routes where this line passes have undergone social and economic and cultural change.

During the First World War, the Anatolian railways became unsharable. For the strategically important railways, European giants have fought a separate war between themselves and with the Ottoman Empire.

In the war of liberation initiated by the Turks after the First World War, the railways took on a different role. The railways, which were used by the imperialist states as colonial means until then, were used for independence in the war of liberation. The newly established Republic of Turkey gave priority attention is given to railways and transport policies.

After the 1950s, the importance of railways was not given due to the focus the highways. As mentioned in the beginning of all Anatolia, the railways, which were influenced from different directions, were unfortunately abandoned to their fate. There are several main reasons for this.

First; unfortunately, Turkey's conservation awareness is at the lower levels. This situation can be easily understood when looking at Anatolia, which has hosted many civilizations. The practices that are quite contrary to the conservation awareness and the lack of awareness about the value of the railway heritage present us bad examples. Second, studies on railway heritage started begin carried out in Turkey in the 1990s. Although there are academic studies, there are very few examples applied in practice. In addition, railways in Turkey depend on a single institution, namely Turkish State Railways (TCDD). Within Turkish Railway Stations, there are mainly engineers. However, multi-faceted work is required to conserve the railways.

As stated in Article One of the Venice Charter, the heritage of the railways should be evaluated together with the whole environment. As Burman (1997) said, it is not only the building but all the railway elements that would be evaluated.

In this sense, it would not be appropriate to examine the buildings, rails and open spaces of Eğirdir Train Station Complex separately. It should be noted that The Complex is a small part of a large organization. In this sense, conservation proposals should be ranked from upper scale to lower scale and related to each other.

Within the framework of this study, solutions were sought to revitalize through the mission of Eğirdir Train Station Complex on the İzmir-Aydın route, and suggestions were made to re-associate with the region and city where it is located and to strengthen its position in urban collective memory. However, it should be noted that the railway heritage requires a multifaceted study. In this sense, a possible future study requires a joint study of different disciplines.

As a result; Eğirdir Train Station and Complex;

- is the last stop of the first exemplary railway line in Anatolia and in the region,
- witnessed developments that would have a profound impact on the history of the country and region;
- established an original relationship with the context;
- played an important role in the development of the region;
- Its technical equipment is highly conserved;
- In terms of preserving the integrity of the original material and the original construction system, although the settlements are idle and devastated;

Eğirdir Train Station Complex is our railway heritage with universal criteria that railway complexes, which are accepted as cultural heritage, should have. With its values, it is worth to be conserved as a railway heritage in national and regional sense and it will be active again in the future. This complex should never be privatized and disintegrated. Eğirdir Train Station should be preserved as an important cultural heritage to be left to our future generations and opened to public use to serve the city and the citizens.

In addition, there are many train stations and complexes that still maintain their originality, such as Eğirdir. Each of these complexes constitutes our railway heritage today. To conserve this heritage, Turkey is obliged to develop strategies considering the specifications, laws, national and international declarations.

In this sense, the conservation approach for Eđirdir Train Station Complex, which was transferred to the privatization administration and closed in 2002, constitutes a model for our railway heritage and other 24 stations that were transferred to the privatization administration.

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APPENDICES

- A. All correspondences and decisions about Eđirdir Train Station Complex**
- B. Title deeds of Eđirdir Train Station Complex**
- C. Documents About Cadastral Status of Eđirdir Train Station Complex Area**

APPENDIX A

All correspondences and decisions about Eđirdir Train Station Complex

GAYRİMENKULÜN
MAHALLESİ
YAZLA

ADA NO	PARSEL NO	MİKTARI	YAKI
239	1	-41.2445.42-m2	1997 yılında 5 Ar. Kavga İşletme İ nası ve Lojman lan Tarifi

MALİKLERİ
T.C.D.D.Yelları İşletmesi Gn.Mud.

BEYANLAR
İmar düzenlemesine alınmıştır. 21.6.1988 Yev:527
Askeri güvenlik bülgesi içinde kalmaktadır. 27.7.1998 Yev:488
2942 sayılı Kan. 7.Mad. göre serh. 1.9.1998 Yev:611

YAZLA	239	2	-69619-m2
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MALİKLERİ

T. C.
BELEDİYE BAŞKANLIĞI
EÇİRDİR

04 / 09 / 1997

Sayı : 200

Şube : Pen işleri

Konu : İmar planı temini ve
arazi rayıcı.

D.D.Y.İŞLETMESİ GENEL MÜDÜRLÜĞÜ
(7. Bölge Başkanlığı)

AFYON

İlci : 01.09.1997 Tarih ve 6684 sayılı yazınız.

İlgi yazınızda adı geçen parsellere ilişkin imar paftalarının
2'şer adet özallikleri görevli elemana elden verilmiştir. İmar pafta-
larının Belediye meclisinde onay tarihi 16.2.1981 dir.

Yazla mahallesinde bulunan bu parsellerin 1994 tarihindeki
M2 fiyatı 80.000 TL dir. 1998 yılı için geçerli olmak üzere 1997 yılı
içinde tesbit edilen M2 fiyatında 2.000.000 TL dir.

Bilgilerinize rica ederim.

i.s.

EB

Omer ŞEKER
Belediye Başkanı

I-Dag. Komando okulunun yazısı.

A. Nedim ÖZKESKİN
Tespü Sicil Müdürü

KOD. B. 11.2. DUY 1.788	
RAY : 4249	
11 Eylül 2001	
HAYAT: 19 - XVIII	
EK : 1	

K/587
19/3 Eylül 2001

Sn: Kanar
Garası M 13/7

12/07
S

GAYRİMENKULÜN
MİKTARI
T.C. MİKTARI
DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ
7.Bölge Müdürlüğü

SAYI: B.11.2.DDY.1.07.00.13/16-21/9314
KONU: Bilgi Bankası Çalışmaları
25 ARALIK 1998

BELEDİYE BAŞKANLIĞI
TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞINA
ANKARA

Bölgemiz Alsancak-Eğirdir Hattı Eğirdir Gar'ı içine alan Is-
parta İli, Eğirdir İlçe, Yazla Mahallesi 239 Ada 1, 2, 9 ; 277 Ada
1, 2 ; 415 Ada 2, 3, 6, 7, 15 parseli ait bilgi kartları 23.12.1998
tarihinde bilgisayara yüklenmiş olup kadastral çap, vaziyet planı, ta-
pu senedi, imar planı yazımız ekinde gönderilmektedir.

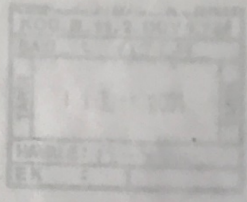
Gereğini arz ederim.

EKLER :

- Ek 1) 2 adet kadastral çap (aslı)
- 2) 2 adet kadastral çap (ozalit)
- 3) 1 adet vaziyet planı (")
- 4) 3 adet tapu senedi
- 5) 1 adet imar planı

[Signature]
Ahmet ÇOBANOĞLU
7.Bölge Müdür Yrd.

Yazan
Tekniker *[Signature]* Ak.
Müd. Yrd.
Müdür *[Signature]*



GAYRİMENKULÜN
MAHALLESİ
YAZLA

ADA NO	PARSEL NO	MİKTARI	VASFI
239	1	-41.2445.42-m2	İçerisinde 5 Ad. Kargir İletme İ nssi ve Lojmanı lan Tarla

MALİKLERİ
T.C.D.D.Yolları İletmesi Gn.Mud.

BEYANLAR
İmar düzenlemesine alınmıştır.21.6.1998 Yev:527
Askeri güvenlik bölgesi içinde kalmaktadır.27.7.1998 Yev:488
2942 sayılı Kan. 7.Mad. göre gerh. 1.9.1998 Yev:611

YAZLA	ADA NO	PARSEL NO	MİKTARI	VASFI
239	2	-69619-m2	Arsa	

MALİKLERİ
T.C.D.D.Yolları İletmesi Gn.Mud.

BEYANLAR
Askeri güvenlik bölgesi içinde kalmaktadır. 27.7.1998 Yev:488

T. C.
BAŞBAKANLIK
APU ve KADASTRO GENEL MÜDÜRLÜĞÜ.
Eğirdir Tapu Sicil Müdürlüğü

SAYI:BO21TKG4323501-1711/692
KONU:Tapu Kaydı.

9.7.2001

DEVLET DEMİRYOLLARI 7.BÖLGE MÜDÜRLÜĞÜ
AFYON

İlgi:25.6.2001 tarih B.11.2.DDY.1.07.00.18/16-11/522/3852 sayılı yazıdır.

İlgi tarih ve sayılı yazınız ile istemiş olduğunuz ilçemiz Yazlı Mahallesi 239 ada 1 ve 2 parselde tapu kayıtları yukarıya çıkarılmış olup; Beyanlardaki Askeri güvenlik bölgesinin gerhinin yazısının fotokopisi çıkartılarak yazımız ekinde gönderilmiştir.

Bilgilerinize arz ederim.

EKLERİ
I-Dağ Komando okulunun yazısı.

A.Nedim ÖZKESKİN
Tapu Sicil Müdürü

KOD B. 11.2. DUY 1.700	
SAYI	4249
TARİH	11 Temmuz 2001
HAVALE	18-XVIII
EK	1

K/587
19/3 Temmuz 2001
Sn. Karan
Garagi
M 13/7

12/07
S

T.C.
KÜLTÜR BAKANLIĞI
ANTALYA KÜLTÜR VE TABİAT VARLIKLARINI
KORUMA KURULU

KARAR

Toplantı Tarihi ve No. : 26.07.2002/290
Karar Tarihi ve No. : 26.07.2002/5530

Toplantı Yeri
ANTALYA

84226.m

Isparta İli, Merkezinde, Devlet Demir Yolları İşletmesine ait 32 ada 5 parseldeki gar binalarının tesciline ilişkin, Devlet Demir Yolları Afyon 7.Bölge Müdürlüğünün 8.10.2001 gün ve 6543 sayılı, 18.4.2002 gün ve 2516 sayılı yazıları, Antalya Koruma Kurulu Müdürlüğü uzmanlarının raporu okundu, ekleri ve dosyası incelendi, yapılan görüşmeler sonucunda:

Isparta İli, Merkezinde, Türkiye Cumhuriyeti ilk dönemlerindeki alt yapı anlayışını yansıtan DDY İşletmesine ait 32 ada 5 parseldeki gar yapılarının, 2863-3386 sayılı yasa kapsamında kalması nedeniyle, Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescil edilmesine, Gar alanının tamamının koruma alanı olarak belirlenmesine karar verildi.



BAŞKAN
Prof.Dr.Yıldız ÖTÜKEN
İMZA

BAŞKAN YARDIMCISI
Doç.Dr.Feral EKE
İMZA

Üye
Prof.Dr.Sencer ŞAHİN
İMZA

Üye
Prof.Dr. Haluk ABBASOĞLU
(BULUNMADI)

Üye
Doç.Dr.Gül ASATEKİN
(BULUNMADI)

Üye
Cengiz AKTAŞ
Isparta Bld.Tem.
İMZA

Üye

Üye

T.C.
KÜLTÜR BAKANLIĞI
Kültür ve Tabiat Varlıklarını Koruma Genel Müdürlüğü
Antalya Kültür ve Tabiat Varlıklarını
Koruma Kurulu Müdürlüğü

SAYI : A.K.T.V.K.K. 32.00.155
KONU: Isparta, Merkez, Isparta
Gari.

ANTALYA

02.08.02 1529

ULAŞTIRMA BAKANLIĞI
(Devlet Demir Yolları Genel Müdürlüğüne)
ANKARA

Özü yukarıda belirtilen konu hakkında Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulu tarafından alınan 26.07.2002 gün ve 5530 sayılı Kurul kararı ekte gönderilmektedir.

2863 sayılı Kültür ve Tabiat Varlıklarını Koruma Kanunu ile Bu Kanunun Bazı Maddelerinin Değiştirilmesi ve Bu Kanuna Bazı Maddeler eklenmesi hakkındaki 3386 sayılı ve 10 Aralık 1987 gün ve 19660 sayılı Resmî Gazete ile 19.8.1989 gün ve 20257 sayılı Resmî Gazetede değişikliği yayımlanan "Korunması Gerekli Kültür ve Tabiat Varlıklarının Tespit-Tescili Hakkındaki Yönetmelik" gereğince;

Tescil edilen korunması gerekli taşınmaz kültür-tabiat varlıklarının Valiliğinizce, Kaymakamlığınızca en geç 3 gün içinde ilan tahtalarına asmak, Belediye hoparlörüyle duyurmak ve Köy Muhtarlığına bildirmek suretiyle ilan tutanağının Müdürlüğümüze gönderilmesi ve tapu kütüğünün beyanlar hanesine, korunması gerekli taşınmaz kültür-tabiat varlıkları olduğuna dair kayıt konulması gerekmektedir.

Bilgilerinizi ve gereğini arz ve rica ederim.

H.Bülent BAYKAL
Müdür

EK: 1-Karar
2-Anıt Fişi
3-

DAĞITIM
Gereği için.

- Ulaştırma Bakanlığı
(Devlet Demir Yolları Gen.Müd.)
ANKARA
- Isparta Valiliği
(İl Kültür Müd.)
- Isparta Kadastro Müd.
- Isparta Tapu-Sicil Müd.
- Devlet Demir Yolları İşletmesi
(Afyon 7.Bölge Müd.)
- Isparta Belediye Başk.

Bügi için:

- Kültür ve Tab.Var.Kor.Gen.Müd.
(Kurullar Dai.Başk.)
- (Araştırma ve Tespit Dai.Başk.)
- Anıtlar ve Müzeler Genel Müd.

TCDD	D.No: 1900006/97
TAŞINMAZ	K.No: 3812
DEĞERLER	6 Ağustos 2002
BAHARCI	
İAŞTIR	

İşaretler:	
Heyate	Eden
6/8	6/8

Adres : Kılıçaslan Mah. Zafer Sokak No.6 Kaleiçi/ANTALYA
Tel : (242) 247 87 61 - 243 21 60 Faks : (242) 248 33 33

Sn. TOKW 7/8pm.

W. Sarıoğlu

T.C.
KÜLTÜR BAKANLIĞI
ANTALYA KÜLTÜR VE TABİAT VARLIKLARINI
KORUMA KURULU

KARAR

Toplantı Tarihi ve No. : 29.08.2002/291
Karar Tarihi ve No. : 29.08.2002/5561

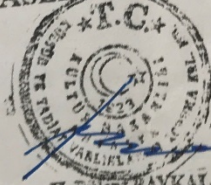
Toplantı Yeri
ANTALYA

Isparta İli, Eğirdir İlçesinde Devlet Demir Yolları İşletmesine ait 239 ada 1 parseldeki gar binalarının tesciline ilişkin, Devlet Demir Yolları Afyon 7.Bölge Müdürlüğünün 8.10.2001 gün ve 6543 sayılı, 18.4.2002 gün ve 2516 sayılı yazıları, Eğirdir Belediye Başkanlığının 2.4.2002 gün ve 78 sayılı yazısı, Antalya Koruma Kurulu Müdürlüğü uzmanlarının 16.7.2002 gün ve 2272 kayıt numaralı raporu, Devlet Demiryolları İşletmesi Genel Müdürlüğü Taşınmaz Mallar Daire Başkanlığının 15.8.2002 gün ve 77-02/3811,3917 sayılı yazısı okundu, ekleri ve dosyası incelendi, yapılan görüşmeler sonucunda;

Isparta İli, Eğirdir İlçesinde, Devlet Demir Yolları İşletmesine ait 239 ada 1 parseldeki Osmanlı İmparatorluğunun son yıllarında yabancıların yaptığı demiryolu yatırımlarının fiziksel belgesi olan ve TC'nin ilk dönemlerindeki altyapı anlayışını yansıtan Gar yapılarının, 2863-3386 sayılı yasa kapsamında kalması nedeniyle Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescil edilmesine, Gar alanının tamamının Koruma Alanı olarak belirlenmesine,

Devlet Demiryolları İşletmesi Genel Müdürlüğü Taşınmaz Mallar Dairesi Başkanlığının ilgi yazısı doğrultusunda, Eğirdir Garı koruma alanıyla birlikte, Kurulumuzun 26.7.2002 gün ve 5530 ve 5531 sayılı kararlarıyla tescil edilen Isparta Garı ile Kuleönü İstasyonu koruma alanlarında, Demiryolu İşletmecilik faaliyetlerinden kaynaklanan teknik altyapı çalışmalarının, Isparta Müze Müdürlüğü ile Koruma Kurulu Müdürlüğüne bilgi verilerek yapılabileceğine karar verildi.

ASLI GİBİDİR



H. BİLGE BAYKAL
Müdür

BAŞKAN
Prof.Dr.Yıldız ÖTÜKEN
İMZA

BAŞKAN YARDIMCISI
Doç.Dr.Feral EKE
İMZA

Üye
Prof.Dr.Sencer ŞAHİN
(BULUNMADI)

Üye
Prof.Dr. Haluk ABBASOĞLU
İMZA

Üye
Doç.Dr.Gül ASATEKİN
İMZA

Üye
Eğirdir Belediye Tem.
(BULUNMADI)

Üye

Üye

T. C.
KÜLTÜR BAKANLIĞI
Kültür ve Tabiat Varlıklarını Koruma Genel Müdürlüğü
Antalya Kültür ve Tabiat Varlıklarını
Koruma Kurulu Müdürlüğü

AKTVK/ 32.02.64
Isparta, Eğirdir Gar Binaları.

05.09.02 1737

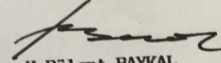
ANTALYA

ULAŞTIRMA BAKANLIĞI
DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ
(Taşınmaz Mallar Dairesi Başkanlığına)

Özü yukarıda belirtilen konu hakkında Antalya Kültür ve Tabiat Varlıklarını Koruma kurulu tarafından alınan
29.08.2002 gün ve 5561

sayılı karar ekte gönderilmektedir.

Bilgilerinizi ve gereğiniarz/rica.... ederim.


H.Bülent BAYKAL
MÜDÜR

EKLİ:

- 1- Karar
- 2- Anıt Fişi
- 3-

DAĞITIM:

GEREĞİ:

- Ulaştırma Bakanlığı
- Devlet Demiryolları İşletmesi Gn.Ml.
- Taşınmaz Mallar Dairesi Başkanlığına
- Isparta Valiliği
(İl Kültür Ml.)
- Isparta Kadastro Ml.
- Isparta Tapu-Sicil Ml.
- Devlet Demiryolları İşletmesi
Afyon 7.Bölge Ml.
- Eğirdir Kaymaklığına
- Eğirdir Bld.Bşk.

BİLGİ:

- Kültür ve Tabiat Var.Kor.Gn.Ml.
- (Kurullar Dai.Bşk.)
- (Araştırma ve Tespit Dai.Bşk.)
- Anıtlar ve Müzeler Gn.Ml.
- Isparta Bld.Bşk.
- Kuleönü Bld.Bşk.

TCDD	8400.06/12
TAŞINMAZ	L.No: 4401
DAİRESİ	13 Eylül 2002
İşler:	
Havale Eden	13/1
Havale Edilen	muon plan

Sn. TOKLU paçpı 16/9/02

Adres : Kaleici Kılıçarslan Mah. Zafer Sok. No. 6 ANTALYA
Fax : 248 35 33

7.MERKEZ İŞLETME MÜDÜRLÜĞÜ
TAŞINMAZ MALLAR MÜDÜRLÜĞÜ

SAYI :B.11.2.DDY.1.07.00.18/16-01/7325
KONU:Eğirdir Gar'ın Tescili.

19 EYLÜL 2002

TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞINA
ANKARA

İLGİ: a) Antalya Kültür Ve Tabiat Varlıklarını Koruma Kurulu Müdürlüğünün 05.09.2002 tarih ve AKTVK./32.02.64-1737 sayılı yazısı.

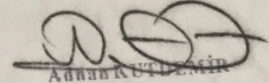
Antalya Kültür Ve Tabiat Varlıklarını Koruma Kurulu Müdürlüğünün ilgede kayıtlı yazısı incelenmiş olup Eğirdir Gar yapılarının 2863-3386 sayılı yasa kapsamında kalması nedeniyle korunması gerekli taşınmaz kültür varlığı olarak tescil edilmesine,gar alanının tamamının koruma alanı olarak belirlenmesine karar verilmiştir.

Ayrıca Koruma Kurulunun 26 Temmuz 2002 tarih ve 5530 ve 5531 sayılı kararları ile tescil edilen Isparta Gar ile Kuleönü İstasyonu koruma alanlarında demiryolu işletmecilik faaliyetlerinden kaynaklanan teknik altyapı çalışmalarının Isparta Müze Müdürlüğü ve Antalya Koruma Kurulu Müdürlüğü'ne bilgi verilerek yapılabileceği kararı verilmiştir.

Eğirdir Gar'da turizme yönelik değerlendirme çalışmalarının yapıldığı ve 239 ada 1 parselin 464+845-470+690 arasında devam ettiği (5845 m),üzerinde 140 m uzunluğunda 8-10 m yüksekliğinde köprünün bulunduğu,yol tamiratu(poz yapımı) ve köprülerin bakımı yapılması gerektiği ayrıca Kuruluşumuzun işletmecilik faaliyetleriyle ilgili yapabileceği onarım,İNŞAAT,kazı,tadilat,plan ve proje değişikliği,ifraz,tevhid,kiraya verme,satış,hibe vb gibi hizmetlerin yapılıp yapılamayacağı hususu ilgi karar ile net olarak belirtilmediğinden devamlı engelle karşılaşılmaması için yapılan tescillere itiraz edilmesi gerekmektedir.

Yapılan tescillere ait belgeler yazımız ekinde gönderilmekte olup konunun Dairenizcede uygun görülmesi halinde,kanuni süresi içerisinde gerekli itirazın yapılmasını arz ederim.

Ek :3 Sayfa Belge


Adnan KUTLU
Taşınmaz Mallar Müdürü

...../200. Memur :.....
...../200. Büro Şefi :.....
9.09.2002 Envy Şef : F. CAŞKIN
14.09.2002 Müdür Yrd : M. E. YASAR
...../200. Müdür :.....

COORDİNE :

...../200.....

TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞI

24 Eylül 2002

SAYI : B.11.2.DDY.0.84.00.06/77-02/4576
KONU : Isparta-Eğirdir'deki tescil kararı

TAŞINMAZ MALLAR SERVİS MÜDÜRLÜĞÜNE
AFYON

İlgi: a) A.K.T.V.K.K. Müd.nün 5/9/2002 t. ve AKTVK./32.02.64/1737 sy. yazısı
b) 15/8/2002 t. ve B.11.2.DDY.0.84.00.06/77-02/3811,3817 sy. yazımız

Isparta İli, Eğirdir İlçesinde, mülkiyeti Kuruluşumuza ait 239 ada 1 parselde yer alan ve Osmanlı İmparatorluğunun son yıllarında yabancıların yaptığı demiryolu yatırımlarının fiziksel belgesi olan ve T.C.'nin ilk dönemlerindeki altyapı anlayışını yansıtan Gar yapılarının 2863-3386 sayılı Yasa kapsamında kalması nedeniyle Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescil edilmesine, Gar alanının tamamının Koruma Alanı olarak belirlenmesine 29.08.2002 t. ve 5561 sayılı Kurul Kararı ile karar verildiği A.K.T.V.K.K. Müdürlüğünün ilgi(a) yazısı ile bildirilmiştir.

Kurulun 26.07.2002 t. ve 5530 ile 5531 sayılı kararlarıyla tescillenen Isparta Gar ve Kuleönü istasyonundaki yapılar için belirlenen koruma alanlarının incelenerek yeniden belirlenmesi ilgi(b) yazımız ile istenmişti. Ancak Kurulun 29.08.2002 t. ve 5561 sayılı kararı incelendiğinde koruma alanlarıyla ilgili herhangi bir değişikliğin yapılmadığı görülmekte olup Isparta Garı ile Kuleönü istasyonu koruma alanlarında, demiryolu işletmecilik faaliyetlerinden kaynaklanan teknik altyapı çalışmalarının, Isparta Müze Müdürlüğü ile Koruma Kurulu Müdürlüğüne bilgi verilerek yapılabileceğine karar verildiği belirtilmektedir.

Müdürlüğünüzce, Eğirdir'de yer alan 239 ada 1 parseldeki tescil kararının Kuruluşumuz işletmecilik faaliyetleri ve Isparta Merkez ve Kuleönü istasyonundaki tescil kararı ile parsellerin tamamının koruma alanı olarak belirlenmiş olması nedeniyle yaptığımız itirazımıza gelen olumsuz cevap da dikkate alınarak incelenmesi ve itirazı gerektirir bir hususun bulunup bulunmadığı yönündeki görüşünüzün bildirilmesi hususunda gereğini rica ederim.

KOD. B. 11. 2. DDY 1.700	
SAYI :	7845
26 Eylül 2002	
TARİH	
HAVALE :	XVIII-018
EK :	

H. Hakan ŞAHİN

H. Hakan ŞAHİN
Taşınmaz Mallar Dairesi
Başkanı

S. Kasnak

K/007

27 EYLÜL 2002

*7/XVIII
26/9/02*

*27.09.2002 - Tarihinde 27/9
imar plan subesinden Kayıt, tamamı geçirildi
19.09.2002 Tarih ve 7825 sayılı yazı ile
aldıkları ayrıca raporla gerek elme depara
bildirdiler.*

T.C.
DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ
Taşınmaz Mallar Dairesi Başkanlığı

7 - Ekim 2002

SAYI : B.11.2.DDY.0.84.00.06/77-02/4596
KONU : Isparta, Eğirdir'deki tescil kararı

**ANTALYA KÜLTÜR VE TABİAT VARLIKLARINI
KORUMA KURULU MÜDÜRLÜĞÜNE**

İlgi: a) 15/08/2002 t. ve B.11.2.DDY.0.84.00.06/77-02/ 3811, 3817 sayılı yazımız
b) 05/09/2002 t. ve AKTVK./32.02.64/1737 sayılı yazınız

Isparta İli, Eğirdir İlçesi, Yazla Mahallesi, 19, 26, 32 pafta, 239 ada 1 parselde bulunan ve Osmanlı İmparatorluğunun son yıllarında yabancıların yaptığı demiryolu yatırımlarının fiziksel belgesi olan ve TC.'nin ilk dönemlerindeki altyapı anlayışını yansıtan Gar yapılarının, 2863-3386 sayılı Yasa kapsamında kalması nedeniyle Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescil edilmesine, Gar alanının tamamının Koruma Alanı olarak belirlenmesine, Eğirdir Garı koruma alanında ve ilgi(a) yazımız ile yaptığımız itiraz doğrultusunda Kurulunuzun 26.07.2002 t. ve 5530 ile 5531 sayılı kararlarıyla daha önce tescil edilen Isparta Garı ile Kuleönü istasyonu koruma alanlarında işletmecilik faaliyetlerimiz ile ilgili yapılacak teknik ve altyapı çalışmalarının Isparta Müze Müdürlüğü ile Koruma Kurulu Müdürlüğüne bilgi verilerek yapılabileceğine 29.08.2002 t. ve 5561 sayılı Kurul Kararı ile karar verildiği ilgi(b) yazınız ile bildirilmektedir.

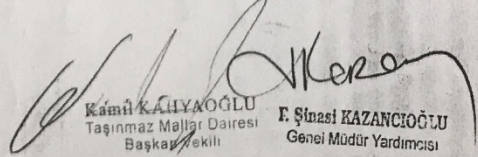
Kuruluşumuz mülkiyetinde olan 412.445,42 m2 yüzölçümlü Eğirdir Gar sahası, Eğirdir Belediye Meclisince onaylı bulunan imar planına göre birden çok imar adasına bölünmüştür. Kurulunuzun 29.08.2002 t. ve 5561 sayılı kararı ile tesciline karar verilen yapılar ekli planda görüleceği üzere aynı imar adası üzerine isabet etmektedir.

Kuruluşumuzca, atıl durumda bulunan taşınmazlarımızın değerlendirilmesi kapsamında, Eğirdir Gar'ın da turizme yönelik değerlendirilmesi düşünülmektedir. Ancak Kurulunuzca alınan tescil kararı ile tüm parselin koruma alanı olarak belirlenmesi nedeniyle söz konusu değerlendirme çalışmaları ile ilgili yapılacak işlemlerde çeşitli zorluklarla karşılaşılacaktır.

239 ada 1 parselin çok büyük bir alanı kapsamı nedeniyle tescilli yapılara ait koruma alanının ekli planda önerildiği şekli ile üzerine isabet ettiği imar adası sınırları dahilinde(ekli planda mavi boyalı) belirlenmesinin daha uygun olacağı düşünülmektedir.

Bu nedenle, Kurulunuzun 29.08.2002 t. ve 5561 sayılı kararının yeniden değerlendirilerek tesciline karar verilen yapılar için gerekli koruma alanının üzerinde buldukları imar adası(ekli planda mavi boyalı) olarak belirlenmesi ve sonucundan Kuruluşumuza bilgi verilmesi hususunda gereğini arz ederiz.

Ek: Plan (1 adet)
Müh. : F.TOKLU ^{14.2/10}
Şb.Müd.Yrd. : S. ÇINAR ^{14.2/10}
Şb.Müd. : R. MUMCU ^{14.2/10}
Bşk.Yrd. : K. KAHYAĞLU


Kamal KAHYAĞLU
Taşınmaz Mallar Dairesi
Başkan Yardımcısı

F. Şinasi KAZANCIÖZÜ
Genel Müdür Yardımcısı

Adres : TCDD Genel Müdürlüğü 06330 Gar /Ankara Telefon : 3241416 Faks : 3241330 Telex : 44390 TCDD - TR

16 Ekim 2002

TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞI

SAYI : B.11.2.DDY.0.84.00.06/77-02/4945
KONU : Isparta, Eğirdir'deki tescil kararı

TAŞINMAZ MALLAR MÜDÜRLÜĞÜNE
AFYON

İlgi: 19/09/2002 t. ve B.11.2.DDY.1.07.00.18/16-01/7325 sayılı yazınız

Isparta İli, Eğirdir İlçesi, Yazla Mahallesi, 19, 26, 32 pafta, 239 ada 1 parselde bulunan Gar yapılarının Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescil edilmesine, Gar alanının tamamının Koruma Alanı olarak belirlenmesine ve bu alanda işletmecilik faaliyetlerimiz ile ilgili yapılacak teknik ve altyapı çalışmalarının Isparta Müze Müdürlüğü ile Koruma Kurulu Müdürlüğüne bilgi verilerek yapılabileceğine A.K.T.V.K.Kurulunun 29.08.2002 t. ve 5561 sayılı kararı ile karar verilmişti.

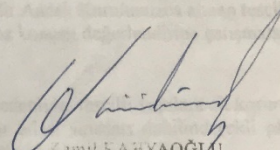
Eğirdir Gar'da turizme yönelik değerlendirme çalışmalarının yapıldığı ve Kuruluşumuzun işletmecilik faaliyetleri ile ilgili yapılacak çalışmalarında devamlı engelle karşılaşılmaması için yapılan tescil işlemine itiraz edilmesi gerektiği ilgi yazınız ile bildirilmişti.

Dairemizce, 412.445,42 m2. miktartlı 239 ada 1 parselin birden çok imar adasına bölünmüş olması ve 29/08/2002 t. ve 5561 sayılı karar ile tesciline karar verilen yapıların aynı imar adasına isabet etmesi nedeniyle koruma alanının üzerinde buldukları imar adası olarak belirlenmesi fotokopisi ekli 7/10/2002 t. ve B.11.2.DDY.0.84.00.06/77-02/4596 sayılı yazımız ile A.K.T.V.K.K Müdürlüğünden istenmiştir.

Bilgi alınması ve gelişmelerden Dairemize bilgi verilmesi hususunda gereğini rica ederim.

Ek: Yazı fot. (1 adet)

KOD: B.11.2.DDY.1.746	
SAYI: 8346	
17 Ekim 2002	
MAHALLE: XVIII-013	1
EK	


Kani KAYAĞI
Taşınmaz Mallar Dairesi
Başkan Vekili

1024
17-EKİM 2002

S.Kasnak
Konunun tatib edilerek gelişmelerde
bilgi verilmesi
K18/10
Sn: Coşkun
tali'bi ve gelişmelerden b'f'i
verilmesi 18/10 mds.

T.C.
KÜLTÜR BAKANLIĞI
ANTALYA KÜLTÜR VE TABİAT VARLIKLARINI
KORUMA KURULU

KARAR

Toplantı Tarihi ve No. : 25.10.2002/295
Karar Tarihi ve No. : 25.10.2002/5647

Toplantı Yeri
ANTALYA

Isparta İli, Eğirdir İlçesi, Yazla Mahallesinde Devlet Demir Yolları İşletmesine ait, 239 ada 1 parselde Antalya Koruma Kurulunun 29.8.2002 gün ve 5561 sayılı kararıyla tescil edilen gar binalarının, koruma alanının yeniden düzenlenmesine ilişkin, Devlet Demir Yolları İşletmesi Genel Müdürlüğü Taşınmaz Mallar Dairesi Başkanlığının 7.10.2002 gün ve 4596 sayılı yazısı, Antalya Koruma Kurulu Müdürlüğü uzmanlarının 23.10.2002 gün ve 3222 kayıt numaralı raporu okundu, ekleri ve dosyası incelendi, yapılan görüşmeler sonucunda;

Isparta İli, Eğirdir İlçesi, Yazla Mahallesinde Devlet Demir Yolları İşletmesine ait, 239 ada 1 parselde Kurulumuzun 29.8.2002 gün ve 5561 sayılı kararıyla tescil edilen gar binalarının, koruma alanının Devlet Demir Yolları İşletme Genel Müdürlüğü'nün ve Antalya Koruma Kurulu Müdürlüğü uzmanlarının önerileri doğrultusunda kararımız eki paftada gösterildiği şekilde yeniden belirlenmesine,

İmar planında, koruma alanı içinden geçirilen yaya yolunun, gar bütünlüğünü olumsuz etkilemesi nedeniyle iptal edilmesine,

Bu alanda, Kurulumuzun 29.8.2002 gün ve 5561 sayılı kararında geçen teknik altyapı çalışmaları dışında yapılacak uygulamalar için Kurulumuzdan izin alınması gerektiğine karar verildi.

ASLI GİBİDİR



BAŞKAN
Prof.Dr.Yıldız ÖTÜKEN
İMZA

BAŞKAN YARDIMCISI
Doç.Dr.Feral EKE
(BULUNMADI)

Üye
Prof.Dr.Sencer ŞAHİN
İMZA

Üye
Prof.Dr. Haluk ABBASOĞLU
(BULUNMADI)

Üye
Doç.Dr.Gül ASATEKİN
İMZA

Üye
İsa SABAH
Eğirdir Bld.Tem.
İMZA

Üye

Üye

T. C.
KÜLTÜR BAKANLIĞI
Kültür ve Tabiat Varlıklarını Koruma Genel Müdürlüğü
Antalya Kültür ve Tabiat Varlıklarını
Koruma Kurulu Müdürlüğü

AYI : AKTVK/ 32.02.64
KONU : Isparta, Eğirdir, Gar Binaları
Koruma Alanı

ANTALYA

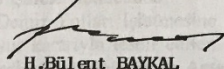
05.11.02 2156

ULAŞTIRMA BAKANLIĞI
Devlet Demiryolları İşletmesi Genel Müdürlüğü
Taşınmaz Mallar Dairesi Başkanlığına

Özü yukarıda belirtilen konu hakkında Antalya Kültür ve Tabiat Varlıklarını Koruma kurulu tarafından alınan
25.10.2002 gün ve 5647

sayılı karar ekte gönderilmektedir.

Bilgilerinizi ve gereğiniarz/rica..... ederim.


H. Bülent BAYKAL
MÜDÜR

EKİ:

- 1- Karar
- 2- Pafta
- 3-

DAĞITIM:

GEREĞİ:

- Ulaştırma Bakanlığı
- Devlet Demiryolları İşletmesi Gn.Ml.
- Taşınmaz Mallar Dai.Bşk.
- Isparta Valiliği
(İl Kültür Ml.)
- Isparta Kadastro Ml.
- Isparta Tapu-Sicil Ml.
- Devlet Demiryolları İşletmesi
Afyon 7.Bölge Ml.
- Eğirdir Kaymakamlığına
- Eğirdir Bld.Bşk.

BİLGİ:

- Kültür ve Tabiat Var.Kor.Gn.Ml.
- (Kurullar Dai.Bşk.)
- (Araştırma ve Tespit Dai.Bşk.)
- Anıtlar ve Muzeler Gn.Ml.

TCDD	D.No: 840006/2702
TAŞINMAZ	K.No: 5548
MALLAR	
DAİRESİ	21 Kasım 2002
BAŞLIĞI	
İşaretler:	
Havale	Havale
Ekon	21/11

2) Arge
Bilgi alınmıştır. 14.11.02

Sn. Tokdemir için v/mem.

Adres : Kaleiçi Kılıçarslan Mah. Zafer Sok. No. 6 ANTALYA
Tel. : 0 (242) 247 87 61 - 243 21 60 Fax : 248 35 33

TAŞINMAZ MALLAR MÜDÜRLÜĞÜ

SAYI : B.11.2.DDY.1.07.00.18/16-01/1074/8346
KONU : Eğirdir'deki tescil ve koruma alanı.

25 KASIM 2002

TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞINA
ANKARA

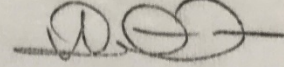
İLGİ : a-) Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulu Müdürlüğü'nün
05.11.2002 Tarih ve AKTVK./32,0264-2156 sayılı yazısı.
b-) 16.10.2002 Tarih ve B.11.2.DDY.0.84.00.06 /77-02/ 4945 sayılı yazımız

Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulu Müdürlüğü'nün ilgi a-) da kayıtlı yazısı incelenmiş olup Dairemizin önerileri doğrultusunda Eğirdir Gar'da tescil edilen binalara ait koruma alanının yeniden belirlenmesine karar verildiği anlaşılmıştır .

İlgi b-) de kayıtlı yazımız ile gelişmelerden bilgi verilmesi istenildiğinden söz konusu karara ait belge ve plan fotokopileri yazımız ekinde gönderilmektedir.

Gereğini arz ederim.

EK : 2 Adet yazı (fotokopi)
1 Adet plan (fotokopi)



Adnan KUTDEMİR
Taşınmaz Mallar Müdürü

TAŞINMAZ MALLAR MÜDÜRLÜĞÜ

SAYI :B.11.2.DDY.1.07.00.18/16-01/1074 /8346
KONU : Eğirdir Gar'daki tescil.

26 KASIM 2002

YOL MÜDÜRLÜĞÜNE
AFYON


Kuruluşumuz mülkiyetinde bulunan Isparta Eğirdir İlçesi Yazla Mahallesi 239 Ada 1 taşınmazın tamamı koruma alanı olarak belirlenmiştir.

Kuruluşumuzca yapılan itiraz ve yeni koruma alanı önerisi Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulu tarafından da benimsenerek koruma alanı ekte planda gösterilen alanla sınırlandırılmıştır.

Önceki Koruma Kurulu Kararı'nda bahsedilen teknik alt yapı çalışmaları dışında , yapılacak uygulamalar için Kuruldan izin alınması gerektiğini belirtir Karar ve üst yazısı ile plan ekte sunulmuştur.

Bilgi ve gereğini arz ederim.

EK : 2 Adet belge .
1 Adet plan .


Adnan KUTDEMİR
Taşınmaz Mallar Müdürü

26.11.2002
2 EN 370
F. COSKUN

LEKORDINE :
.....

TAŞINMAZ MALLAR DAİRESİ BAŞKANLIĞI

2 - Aralık 2002

SAYI : B.11.2.DDY.0.84.00.06/77-02/5548

KONU : Isparta, Eğirdir'deki tescil kararı ve koruma alanı

TAŞINMAZ MALLAR MÜDÜRLÜĞÜNE / AFYON

Isparta İli, Eğirdir İlçesi, Yazla Mahallesi, 19, 26, 32 pafta, 239 ada 1 parselde bulunan ve Osmanlı İmparatorluğunun son yıllarında yabancıların yaptığı demiryolu yatırımlarının fiziksel belgesi olan ve TC.'nin ilk dönemlerindeki altyapı anlayışını yansıtan Gar yapıları, 2863-3386 sayılı Yasa kapsamında kaldığından Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulu Müdürlüğünün 29.08.2002 t. ve 5561 sayılı kararı ile Korunması Gerekli Taşınmaz Kültür Varlığı olarak tescillenmiş ve Gar alanının tamamının Koruma Alanı olarak belirlenmesine karar verilmiştir.

Dairemizce yapılan incelemede, alınan tescil kararı ile tüm parselin koruma alanı olarak belirlenmesi nedeniyle taşınmazımızın turizme yönelik değerlendirilmesi çalışmalarında yapılacak işlemlerde çeşitli zorluklarla karşılaşılacağı belirtilerek taşınmazın çok büyük bir alanı kapsamaması nedeniyle tescilli yapılar için gerekli olan koruma alanının yeniden belirlenmesi fotokopisi ekli 7.10.2002 t. ve B.11.2.DDY.0.84.00.06/77-02/4596 sayılı yazımız ile A.K.T.V.K.K.Müdürlüğünden istenmiştir.

Konu ile ilgili olarak A.K.T.V.K.K.Müdürlüğünden alınan fotokopisi ekli 5.11.2002 t. ve AKTVK/32.02.64/2156 sayılı yazıda Kurulun 25.10.2002 t. ve 5647 sayılı kararında koruma alanının yeniden belirlendiği, Garın bütünlüğünü olumsuz etkileyen yaya yolunun iptal edildiği ve Kuruluşumuzun teknik altyapı çalışmaları dışında yapacağı uygulamalar için Kuruldan izin alınması gerektiğine karar verildiği bildirilmektedir.

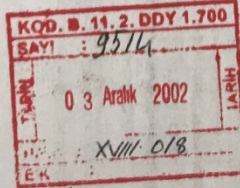
Bilgi alınması ve tesciline karar verilen söz konusu alanda yapılacak işlemlerde Isparta Müze Müdürlüğü ve Koruma Kurulu Müdürlüğüne bilgi verilmesi hususunda bilgi ve gereğini arz ve rica ederim.

Ek:Yazı fot.(3 adet)
Plan (1 adet)

DAĞITIM:

Gereği:
I, II, V, XVI Dai. Bşk.
Taş. Mal. Müd.(Afyon)

H. Hakan ŞAHİN
Taşınmaz Mallar Dairesi
Başkanı



4/12/02
109 ARALIK 2002

Sn: Albayrak Neşe 16/12
Kerem
Coskun
Bilgi alınması ve buna kararın
uygulanması için gereği

S. Kasnak
S. Aydın

Sn: Çoşkun
Bilgi alınması için gereğini
mektupla yaptım. 9/12/02

09/12

BEYANLAR :
Tarih Veriye Açıklamalar
27/07/1998 488 ASKERİ GÜVENLİK BÜLESİ İÇİNDİRİLMİKTİR.

MAHALLESİ...: YAZILI
MEVKİİ...: İSTİFAYON
CİNSİ...: PARSEL
YÜZÖLÇÜMÜ...: 307,30 m²
ADA...: 1415
PARSEL...: 17
CİLT...: 16
SAYFA...: 175B

ROL NO/SU: 180001 HİSSESİ EDİME NEDENİ EDİN.TAR. YEV.
T.C.D.B.YOLLARI İZLETİŞİ GENEL NO.: 107/500 İHR 10/10/1988 765
333/500 İHR 10/10/1988 765

MAHALLESİ...: YAZILI
MEVKİİ...: İSTİFAYON
CİNSİ...: PARSEL
YÜZÖLÇÜMÜ...: 298,235 m²
ADA...: 1415
PARSEL...: 15
CİLT...: 16
SAYFA...: 176B

ROL NO/SU: 180001 HİSSESİ EDİME NEDENİ EDİN.TAR. YEV.
T.C.D.B.YOLLARI İZLETİŞİ GENEL NO.: İHR 10/10/1988 765

BEYANLAR :
Tarih Veriye Açıklamalar
27/07/1998 488 ASKERİ GÜVENLİK BÜLESİ İÇİNDİRİLMİKTİR.

T.C.
BAŞBAKANLIK
TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ
EMİRDİR TAPU SİCİL MÜDÜRLÜĞÜ

Sayı : 8021TKG4223501-110 Tarih : 05/02/2003

TASINMAZ MALLAR MÜDÜRLÜĞÜNE
AFYON

İlgi : 04/02/2003 tarih ve 362 sayılı yazınız.
İlgi yazınızla istenilen olduğunuz tapu kayıtları yukarıya
çıkartılmış olup, bilgilerinize arz olunur.

AHMET NURİ ÖZKIBAN
TAPU BAŞKAN YERİNE

K/154
10 SUBAT 2003

T.C.D.D. İSLETMESİ GENEL NO.:	TARİH	HÜKMEYİ (BAŞ. KARARI)	18/03/1987 459
BEYANLAR :			
Tarih	Tevziye Açıklamaları		
27/07/1998	488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.		
MAHALLESİ...: YAZLA	ADA...: 415		
MEVKİİ...: İSTASYON	PARSEL...: 3		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 367,15 m ²	SAYFA...: 1753		
ADI SÖYADİ : BABAADI	HİSSESİ	EDİNE NEDENİ	EDİM TAR. YEV.
T.C.D.D. YOLLARI İSLETMESİ GENEL NO.:	TARİH	İMAR	10/10/1988 765
27/07/1998	488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.		
MAHALLESİ...: YAZLA	ADA...: 415		
MEVKİİ...: İSTASYON	PARSEL...: 3		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 352,39 m ²	SAYFA...: 1754		
ADI SÖYADİ : BABAADI	HİSSESİ	EDİNE NEDENİ	EDİM TAR. YEV.
T.C.D.D. YOLLARI İSLETMESİ GENEL NO.:	TARİH	İMAR	10/10/1988 765
BEYANLAR :			
Tarih	Tevziye Açıklamaları		
27/07/1998	488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.		
MAHALLESİ...: YAZLA	ADA...: 415		
MEVKİİ...: İSTASYON	PARSEL...: 6		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 260,29 m ²	SAYFA...: 1757		
ADI SÖYADİ : BABAADI	HİSSESİ	EDİNE NEDENİ	EDİM TAR. YEV.
T.C.D.D. YOLLARI İSLETMESİ GENEL NO.:	TARİH	İMAR	10/10/1988 765
BEYANLAR :			
Tarih	Tevziye Açıklamaları		
27/07/1998	488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.		
MAHALLESİ...: YAZLA	ADA...: 415		
MEVKİİ...: İSTASYON	PARSEL...: 7		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 307,30 m ²	SAYFA...: 1758		
ADI SÖYADİ : BABAADI	HİSSESİ	EDİNE NEDENİ	EDİM TAR. YEV.
T.C.D.D. YOLLARI İSLETMESİ GENEL NO.:	107/500	İMAR	10/10/1988 765
T.C. SEKER FABRİKALARI A.Ş.:	333/500	İMAR	10/10/1988 765
MAHALLESİ...: YAZLA	ADA...: 415		

ADI SOYADI : BABAOĞI	HISSESİ	EDİME NEDENİ	EDİM TAR. / YET.
T.C.D.D.YOLLARI İŞLETİŞİ GENEL NO.:	TAN	İNAR	10/10/1988 765
BEYANLAR :			
Tarih : Yevmiye Açıklamaları			
27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.			
MAHALLESİ...: YAZLA	ADA.....: 277		
MEVKİİ...: KUMLUK	PARSEL...: 1		
CİNSİ...: KARĞER SU DEPOSU VE ARSASI	CİLT...: 5		
YÜZÖLÇÜMÜ...: 170.00 m ²	SAYFA...: 420		
ADI SOYADI : BABAOĞI	HISSESİ	EDİME NEDENİ	EDİM TAR. / YET.
T.C.D.D. İŞLETİŞİ GENEL NO.:	TAN	HAKMUKİYAT KARARI	04/11/1987 460
BEYANLAR :			
Tarih : Yevmiye Açıklamaları			
27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.			
MAHALLESİ...: YAZLA	ADA.....: 277		
MEVKİİ...: KUMLUK	PARSEL...: 2		
ÜRÜN...: KARĞER SU DEPOSU VE ARSASI	CİLT...: 5		
YÜZÖLÇÜMÜ...: 152.30 m ²	SAYFA...: 421		
ADI SOYADI : BABAOĞI	HISSESİ	EDİME NEDENİ	EDİM TAR. / YET.
T.C.D.D. İŞLETİŞİ GENEL NO.:	TAN	HAKMUKİYAT KARARI	18/03/1987 459
BEYANLAR :			
Tarih : Yevmiye Açıklamaları			
27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.			
MAHALLESİ...: YAZLA	ADA.....: 415		
MEVKİİ...: İSTASYON	PARSEL...: 3		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 387.15 m ²	SAYFA...: 753		
ADI SOYADI : BABAOĞI	HISSESİ	EDİME NEDENİ	EDİM TAR. / YET.
T.C.D.D.YOLLARI İŞLETİŞİ GENEL NO.:	TAN	İNAR	10/10/1988 765
BEYANLAR :			
Tarih : Yevmiye Açıklamaları			
27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTADIR.			
MAHALLESİ...: YAZLA	ADA.....: 415		
MEVKİİ...: İSTASYON	PARSEL...: 3		
CİNSİ...: ARSA	CİLT...: 8		
YÜZÖLÇÜMÜ...: 352.39 m ²	SAYFA...: 754		
ADI SOYADI : BABAOĞI	HISSESİ	EDİME NEDENİ	EDİM TAR. / YET.
T.C.D.D.YOLLARI İŞLETİŞİ GENEL NO.:	TAN	İNAR	10/10/1988 765
BEYANLAR :			
Tarih : Yevmiye Açıklamaları			

MAHALLESİ...:YAZLA
MEVKİİ...:İSTASYON
CİNSİ...:İGE.5 AD. KAR. İSLT. Bİ. LOJ. TARLA
YÜZÖLÇÜMÜ...: 412445.42 m²

ADA...:239
PARSEL...:1
CİLT...:3
SAYFA...:291

ADI SOYADI : BABAADI

HİSSESİ

EDİLMİŞ NEDENİ

EDİM TAR. YEV.

T.C.D.D.YOLLARI İŞLETMESİ GENEL MD.:

TAK

KADASTRO

11/02/1986 0

BEYANLAR :

Tarih Tevziye Açıklamalar

21/06/1998 527 İMAR DÜZENLEMESİNE ALINMIŞTIR.
27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTİR.
01/09/1998 611 2942 SAY.KAN.7.MAD.GAHE SERH.

*Not: Parsel kâğıdı ve Tabiat varlıklarını
foruma kurulanın 25/10-2002 tarihli 285 sayılı
fazlası görüşme bu parsellerin içinde yapı
lı olan değişiklikler için işlemlerin
gerçekleştirilmesi.*

MAHALLESİ...:YAZLA
MEVKİİ...:KUMLUK
CİNSİ...:ARSAN
YÜZÖLÇÜMÜ...: 69619.00 m²

ADA...:239
PARSEL...:2
CİLT...:3
SAYFA...:292

ADI SOYADI : BABAADI

HİSSESİ

EDİLMİŞ NEDENİ

EDİM TAR. YEV.

T.C.D.D.YOLLARI İŞLETMESİ GENEL MD.:

TAK

KADASTRO

20/05/1988 461

BEYANLAR :

Tarih Tevziye Açıklamalar

27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTİR.

MAHALLESİ...:YAZLA
MEVKİİ...:İSTASYON
CİNSİ...:ARSAN
YÜZÖLÇÜMÜ...: 283.82 m²

ADA...:239
PARSEL...:9
CİLT...:7
SAYFA...:686

ADI SOYADI : BABAADI

HİSSESİ

EDİLMİŞ NEDENİ

EDİM TAR. YEV.

T.C.D.D.YOLLARI İŞLETMESİ GENEL MD.:

TAK

İRAK

10/10/1988 763

BEYANLAR :

Tarih Tevziye Açıklamalar

27/07/1998 488 ASKERİ GÜVENLİK BÖLGESİ İÇİNDEKİLMİKTİR.

MAHALLESİ...:YAZLA
MEVKİİ...:KUMLUK
CİNSİ...:İKRSR SU DEPOSU VE ARSASI
YÜZÖLÇÜMÜ...: 170.00 m²

ADA...:277
PARSEL...:11
CİLT...:5
SAYFA...:420

TAŞINMAZ MALLAR MÜDÜRLÜĞÜ

SAYI : B.11.2.DDY.1.07.00.18/16-01/1074/8346
KONU : Eğirdir'deki tescil ve koruma alanı.

25 KASIM 2002

ANKARA
T.C
DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ
7.Bölge Müdürlüğü
Afyon

SAYI : B.11.2.DDY.1.07.00.18/16-23/ 6035
KONU : Tapu kaydı.

22.07.2004

TAPU SİCİL MÜDÜRLÜĞÜNE
EĞİRDİR/İSPARTA

Kuruluşumuza ait Isparta İli, Eğirdir İlçesi, Yazla Mahallesi 239 ada 1 parsel, 3 cilt, 291 sayfada kayıtlı 412445.42.m2 alanlı taşınmaza ait, tapu Kütüğünün beyanlar hanesindeki bilgilerin (Koruma Kurulu tescilinin grubu ile birlikte) çıkarılarak Bölgemize gönderilmesi hususunda gereğini arz ederiz.

...../200. Memur
...../200. Büro Şefi
20/7/2004 Eny Şefi : F. COSKUN
20/7/2004 Müdür Yrd : M. C. KASIM
...../200. Müdür

Abderrahman GENC
Taşınmaz Mallar Müdürü

Abderrahman GENC
7. Bölge Müdür Yardımcısı

KOORDİNE :

...../200.....

...../200.....

T.C.
İSPARTA VALİLİĞİ
EĞİRDİR TAPU SİCİL MÜDÜRLÜĞÜ

SAYI: B091TKG4323501-1711/809
KONU: Tapu kaydı.

EĞİRDİR
02.08.2004

DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ
7.BÖLGE MÜDÜRLÜĞÜNE
A F Y O N

İlgi: 22.7.2004 tarih B.11.2.DDY.1.07.00.18/16-23/6035 sayılı yazınız.

İlgi yazınızda istenilen İlçemiz Yazla Mahallesi 239 ada 1 parsel sayılı taşınmazın ait tapu kaydı tüm tekyidatları ile birlikte çıkarılarak Koruma Kurulunun yazı ve kararıyla birlikte ilgilikte sunulmuştur. Bilgilerinize arz ederim.

BKİ: 3

Gülçin ÖZDEMİR
Tapu Sicil Müdürü V.

877
EĞİRDİR 2004

Snr. Karsnel
Emlak Bırosu
uf 9/8
9/8

19 Kasım 2002
104

DAĞITIM:

GEREĞİ:

- Ulaştırma Bakanlığı
- Devlet Demiryolları İşletmesi Gn.Md.
- Taşınmaz Mallar Dai.Bşk.
- İsparta Valiliği
(İl Kültür Md.)
- İsparta Kadastro Md.
- İsparta Tapu-Sicil Md.
- Devlet Demiryolları İşletmesi
Afyon 7.Bölge Md.
- Eğirdir Kaymakamlığına
- Eğirdir Bld.Bşk.

BİLGİ:

- Kültür ve Tabiat Var.Kor.Gn.Md.
- (Kurullar Dai.Bşk.)
- (Araştırma ve Tespit Dai.Bşk.)
- Anıtlar ve Mizeler Gn.Md.

TAPU SİCİL MÜDÜRLÜĞÜ	
Keşid No :	2331
Tarih:	11.11.2002
Genel Not:	10.4.2004
Parasız	

EĞİRDİR
24.8.2004

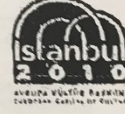
Cengiz Solmazel



T.C.

KÜLTÜR VE TURİZM BAKANLIĞI

Kültür Varlıkları ve Müzeler Genel Müdürlüğü

SAYI
KONUB 16.0.KVM.0.10.02.00/25/493 - 54611
Devir

D.G.Y.Y.

İLGİ: a) Devlet Demiryolları İşletmesi Genel Müdürlüğünün (Tasınmaz Mallar Dairesi Başkanlığı) 05.12.2008 gün ve 18157 sayılı yazısı.

b) Antalya Kültür ve Tabiat Varlıklarını Koruma Bölge Kurulu Müdürlüğünün 02.02.2009 gün ve 408 sayılı yazısı.

c) Antalya Kültür ve Tabiat Varlıklarını Koruma Bölge Kurulu Müdürlüğünün 03.03.2009 gün ve 800 sayılı yazısı.

Isparta İli, Eğirdir İlçesi, Yazla Mahallesi'ndeki Devlet Demiryolları İşletmesi Genel Müdürlüğünün mülkiyetindeki 239 ada, 1 parselin 56.750 m2'lik kısmı, 239 ada, 2 parsel, 277 ada, 1 ve 2 parsel nolu taşınmazların tamamının Milli Savunma Bakanlığına devri talebine ilişkin ilgi(a),(b),(c) yazılar ve ekleri incelenmiştir.

Konuya ilişkin ilgi(b) yazı ekindeki rapor ve ilgi(c) yazıda 239 ada, 1 parselde bulunan gar yapılarının tescilli olduğu ve koruma alanı sınırlarının belirlendiği, talebe konu parsellerin tamamının ve 239 ada, 1 parselin 56.750 m2'lik kısmının koruma alanı sınırları ve tescil edilen alanlar dışında kaldığı belirtilmiştir.

Söz konusu tescilli 239 ada, 1 parselin 56.750 m2'lik kısmının yapılacak uygulamalar öncesi ilgili koruma bölge kurulundan izin alınması ve bu hususta tapu kaydına şerh düşülmesi koşuluyla devrinde 2863 sayılı Kanunun 13.maddesi kapsamında Bakanlığımızca sakınca bulunmamakta olup 239 ada, 2 parsel, 277 ada, 1 ve 2 parsel nolu taşınmazların 2863 sayılı Kanun kapsamında kalmadıklarından devirleri konusunda Bakanlığımızca yapılacak bir işlem bulunmamaktadır.

Bilgilerinizi ve gereğini rica ederim.

Ulku SOLMAZEL SAYGILI

Genel Müdür Yardımcısı

DAĞITIM

Gereği

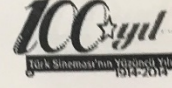
-Devlet Demir Yolları İşletmesi Genel Müdürlüğü
(Tasınmaz Mallar Dairesi Başkanlığı)

Bilgi

-Antalya Kültür ve Tabiat Varlıklarını
Koruma Bölge Kurulu Müdürlüğüne16/3/2009 Pz. Mm.
19/3/2009 Şb. Md.
19/3/2009 De. Bşk.B. TİMUR
Y. SÜYDEMİR
M. GÜRÜLT.C. B.M.M. Ulus 06543/ Ankara
www.kultur.gov.tr E-Posta: bilgi@kvm.gov.tr

Tel: 309 02 60 (9 hat)

Faks: 310 91 12



KÜLTÜR VE TURİZM BAKANLIĞI

Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü

SAYI : 79545921/32.02.64

ANTALYA

KONU: Isparta, Eğirdir İlçesi, Gar Yapıları.

10.03.2014 * 0930

DEVLET DEMİRYOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜNE
(Emlak ve İnşaat Dairesi Başkanlığı)

- İLGİ: a) ATN İmar İnş. Harita Pro.Tur.San.A.Ş.'nin 01.12.2011 tarih ve R.32.04/04 sayılı yazısı.
b) 15.12.2011 tarih ve 1105 sayılı yazımız.
c) Kültür Varlıkları ve Müzeler Genel Müdürlüğünün 11.02.2014 tarih ve 57914681/25/29331 sayılı yazısı.
d) D.D.Y. İşletmesi Genel Müdürlüğü Emlak ve İnşaat Dairesi Başkanlığı 11.02.2014 tarih ve 3572 sayılı yazısı.
e) Kültür Varlıkları ve Müzeler Genel Müdürlüğünün 25.02.2014 tarih ve 57914681/25-38938 sayılı yazısı.

Isparta İli, Eğirdir İlçesi, Yazla Mahallesinde 239 ada 1 parselde bulunan mülkiyeti Devlet Demiryolları İşletmesine ait tescilli Eğirdir Gar Binalarının satış işlemine ilişkin ilgi (c), (d) ve (e) yazılar incelenmiştir.

Eğirdir İlçesi Yazla Mahallesinde mülkiyeti Devlet Demiryolları İşletmesine ait 239 ada 1 parselde yer alan gar binaları Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulunun 29.08.2002 tarih ve 5561 sayılı kararı ile korunması gerekli taşınmaz kültür varlığı olarak tescil edilmiş; Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulunun 25.10.2002 tarih ve 5647 sayılı kararı ile de karar eki paftada sınırları gösterildiği şekilde koruma alanı sınırları belirlenmiştir.

Özelleştirme İdaresi Başkanlığına yetkilendirilen firma tarafından ilgi(a) yazı ile Isparta İli, Eğirdir İlçesi Merkezinde 4046 Sayılı Özelleştirme Uygulamaları Hakkında Kanun kapsam ve programında olan Yazla Mahallesinde yer alan mülkiyeti TCDD'ye ait 239 ada 9 ve 11 parseller, 415 ada 2,3,6,7 ve 15 parsellere ilişkin araştırma ve planlama çalışmalarında kullanılmak üzere bilgi ve belgeler istenmiştir.

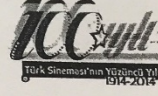
İlgi(a) yazı doğrultusunda konu Müdürlüğümüz uzmanlarınca yerinde incelenerek alanda yapılan yeni kadastral düzenleme ile tescilli gar yapıları ile koruma alanını içeren parselin yeni kadastral durumunun 239 ada 11 parsel olduğu anlaşılmış; Müdürlüğümüzün ilgi(b) yazısı ile de 239 ada 11 parselde Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulunun 29.08.2002 tarih ve 5561 sayılı kararı ile korunması gerekli taşınmaz kültür varlığı olarak tescil edilen yapıların yer aldığı ve bu parselin Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulunun 25.10.2002 tarih ve 5647 sayılı kararı ile karar eki paftada sınırları gösterildiği şekilde belirlenen koruma alanı sınırları içerisinde kalmakta olduğu; bunun dışındaki 239 ada 9 parsel ile 415 ada 2,3,6,7 ve 15 nolu parsellerin ise anılan karar ile belirlenen koruma alanı sınırlarının güneyinde ve dışında kaldığı, ayrıca bu parseller üzerinde 2863 sayılı yasa kapsamında değerlendirilebilecek herhangi bir taşınmaz kültür varlığı bulunmadığının tespit edildiği ve 2863 sayılı yasanın 8. Maddesinde; "korunması gerekli kültür ve tabiat varlıklarının korunma alanlarının tesbiti ve bu alanlar içinde inşaat ve tesisat yapıp yapılamayacağı konusunda karar alma yetkisi Koruma Kurullarına aittir." hükmü bulunduğu belirtilerek, söz konusu gar yapılarının tescili ile koruma alanının belirlenmesine yönelik Antalya Kültür ve Tabiat Varlıklarını Koruma Kurulunca alınan kararlar ile koruma alanına ilişkin pafta ilgi(b) yazımız ekinde gönderilmiştir.

..

Kılıçaslan Mahallesi Zafer Sokak No:6 Kaleiçi / ANTALYA
Tel: 0 (242) 247 87 61-243 21 60 Faks: 0 (242) 248 35 33
e-posta:antalyakurul@kulturturizm.gov.tr

Ayrıntılı Bilgi İçin İrtibat: U.ÖZGÜZ Mimar

Elektronik Ağ: www.kulturturizm.gov.tr



KÜLTÜR VE TURİZM BAKANLIĞI

Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü

SAYI : 79545921/32.02.64

KONU: Isparta, Eğirdir İlçesi, Gar Yapıları.

ANTALYA

-2-

10.03.2014 * 0930

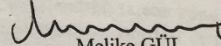
İlgi(e) yazı ekinde iletilen Başbakanlık Özelleştirme İdaresi Başkanlığının 20.02.2014 tarih ve 1501 sayılı yazısında 239 ada 11 parselin satışa konu edilmediği satışa konu olan parsellerin 415 ada 2,3,6,7 ve 15 nolu parseller olduğunun belirtildiği görülmüştür.

2863 sayılı yasanın 13. Maddesinde "Hazineye ve diğer kamu kurum ve kuruluşlarına ait olup, usulüne göre tescil ve ilan olunan, her çeşit korunması gerekli taşınmaz kültür ve tabiat varlığı ile bunlara ait korunma sınırları dahilindeki taşınmazlar, Kültür ve Turizm Bakanlığının izni olmadan, gerçek ve tüzelkişilere satılamaz, hibe edilemez.;"

Aynı yasanın 14. Maddesinde ise; "Korunması gerekli taşınmaz kültür ve tabiat varlıklarının intifa haklarının, belirli sürelerle kamu hizmetlerinde kullanılmak üzere, Devlet dairelerine, kamu kurum ve kuruluşlarına, kamu menfaatine yararlı milli derneklere bırakılması veya gerçek ve tüzelkişilere kiraya verilmesi, Kültür ve Turizm Bakanlığının iznine tabidir." hükmü bulunmaktadır.

Buna göre; ilgi(e) yazı eki Başbakanlık Özelleştirme İdaresi Başkanlığının 20.02.2014 tarih ve 1501 sayılı yazısından 239 ada 11 parselde yer alan korunması gerekli taşınmaz kültür varlığı olarak tescilli "Eğirdir Gar Binaları"nın satışına ilişkin herhangi bir işlem bulunmadığı anlaşılmakta olup anılan parselde yönelik herhangi bir satış ya da kiralama işlemi söz konusu olduğu takdirde 2863 sayılı yasanın 13. ve 14. maddeleri ve buna bağlı yönetmelik hükümleri gereği Müdürlüğümüzden izin alınması gerektiği hususunda;

Gereğini ve bilgilerinizi arz ederim.


Melike GÜL
Bölge Kurulu Müdürü

DAĞITIM:

Gereği:
-Başbakanlık
(Özelleştirme İdaresi Başkanlığı)

Bilgi:
-Kültür Varlıkları ve Müzeler Genel Müdürlüğü
(Emlak Dairesi Başkanlığı)
(Ek: Yazışmalar)
-Devlet Demiryolları İşletmesi Genel Müdürlüğü
(Emlak ve İnşaat Dairesi Başkanlığı)

EMLAK VE İNŞAAT DAİRESİ BAŞKANLIĞI

Sayı : 75330335-769/58-07-20424
Konu : Eğirdir Gar Sahası

18/03/2014

7. BÖLGE MÜDÜRLÜĞÜNE

İlgi : Kültür ve Turizm Bakanlığı Kültür ve Tabiat Varlıklarını Koruma Genel Müd.lüğünün 10.03.2014 tarihli ve /0930 sayılı yazısı

Isparta İli, Eğirdir İlçesi, Yazla Mahallesi, Teşekkülümüz mülkiyetinde bulunan, korunması gerekli taşınmaz kültür varlığı olarak tescilli Eğirdir Gar Binalarını da içeren 239 Ada 11 Parselin Özelleştirme İdaresi Başkanlığı'na satışına ilişkin herhangi bir işlemin bulunmadığı, Özelleştirme İdaresi Başkanlığı'na satışa konu parsellerin 239 Ada 11 Parselin güneyinde yer alan 415 Ada 2,3,6,7 ve 15 parseller olduğundan bahisle;

Eğirdir Gar Sahasında Teşekkülümüz mülkiyetinde bulunan 239 Ada 11 parsel yönelik herhangi bir satış yada kiralama işlemi söz konusu olduğu takdirde 2863 Sayılı Yasanın 13 ve 14.maddeleri ve buna bağlı yönetmelik hükümleri gereği Müdürlüklerinden izin alınması gerektiği Kültür ve Turizm Bakanlığı Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü'nün ilgi yazısı ile bildirilmektedir.

Bu nedenle Eğirdir Gar Sahasında Teşekkülümüzce yapılacak işe ve işlemlerde Kültür ve Turizm Bakanlığı Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü'nün ilgi yazısında belirtilen hususların dikkate alınması hususunda gereğini rica ederim.

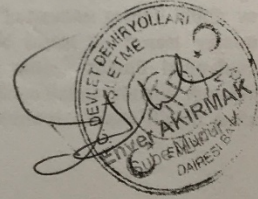
Mustafa ÇAVUŞOĞLU
Genel Müdür Yardımcısı

EKLER :

Yazı fot (2 adet)

DAĞITIM:

YOL DAİRESİ BAŞKANLIĞI
CER DAİRESİ BAŞKANLIĞI
YÜK DAİRESİ BAŞKANLIĞI
DEMİRYOLU YAPIM DAİRESİ
BAŞKANLIĞI
APK DAİRESİ BAŞKANLIĞI
TRAFİK DAİRESİ BAŞKANLIĞI
TESİSLER DAİRESİ BAŞKANLIĞI
ETÜT PROJE ve YATIRIM DAİRESİ
BAŞKANLIĞI
7. BÖLGE MÜDÜRLÜĞÜ



5070 Sayılı Elektronik imza Kanunu'na uygun olarak nitelikli elektronik sertifika ile imzalanmıştır.

T.C.
KÜLTÜR VE TURİZM BAKANLIĞI
ANTALYA KÜLTÜR VARLIKLARINI KORUMA BÖLGE KURULU
KARAR

Toplantı Tarihi ve No. : 24.01.2017/167
Karar Tarihi ve No. : 24.01.2017/5779

Toplantı Yeri
ANTALYA

Isparta İli, Eğirdir İlçesi, Yazla Mahallesinde yer alan, mülkiyeti D.D.Y. İşletmesine ait Eğirdir Gar Sahası koruma alanı içerisinde, plaktorna alanında yapılan izinsiz uygulamaya ilişkin Antalya Kültür Varlıklarını Koruma Bölge Kurulunun 19.08.2016 tarih ve 5240 sayılı kararı, plaktornanın kesilen kısımlarının yeniden birleştirilmesi ve alanın rehabilitasyonu için hazırlanan teknik raporun iletildiği T.C.D.D. 7. Bölge Müdürlüğü Yol Servis Müdürlüğü'nün 14.12.2016 tarih ve 596742 sayılı ve Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü uzmanlarınca hazırlanan 20.01.2017 tarih ve 419 kayıt nolu rapor okundu; ekleri ve dosyası incelendi, yapılan görüşmeler sonucunda;

Isparta İli, Eğirdir İlçesi, Yazla Mahallesinde yer alan, mülkiyeti D.D.Y. İşletmesine ait Eğirdir Gar Sahası koruma alanı içerisindeki, izinsiz müdahale ile kesilerek parçalanmış plaktornanın yeniden birleştirilmesi ve alanın rehabilitasyonu için hazırlanan teknik raporun uygun bulunduğu (olumlu); uygulamanın T.C.D.D. teknik personeli denetiminde yapılarak uygun bulduğuna ilişkin rapor ve fotoğrafik belgelerin Kurul Müdürlüğüne gönderilmesine;

Söz konusu plaktorna alanındaki uygulamanın geri dönüşümünün mümkün olduğu teknik rapordan anlaşıldığından izinsiz müdahale ile ilgili suç duyurusunda bulunmaya gerek görülmediğine, ancak koruma alanı içerisinde Kurulumuzda izin alınmadan uygulamada bulunulmaması konusunda ilgili kurumların uyarılmasına;

Ayrıca mevcut karayolunun gar sahası bütünlüğünü bozacak şekilde geçtiği anlaşıldığından karayolunun imar planındaki yerine çekilerek tescilli gar sahası bütünlüğünün sağlanması için gerekli çalışmaların yapılmasının ilgili kurumlara tavsiyesine karar verildi.

ASLI GİBİDİR



BAŞKAN
Yrd.Doç.Dr.İbrahim BAKIR
İMZA

BAŞKAN YARDIMCISI
Prof.Dr.H.Sabri ALANYALI
İMZA

Üye
Prof.Dr. Ziya GENÇEL
(BULUNMADI)

Üye
Prof.Dr.Bilal SÖĞÜT
İMZA

Üye
Esin SERTTAŞ YAREN
İMZA

Üye
Emel BOYACIOĞLU
İMZA

Üye
Ömer ŞENGÖL
Eğirdir Bld.Bşk.
İMZA

Üye
Seher HOŞGÖR
Isparta Müze Md.V.
İMZA



T.C.
KÜLTÜR VE TURİZM BAKANLIĞI
Antalya Kültür Varlıklarını Koruma Bölge Kurulu Müdürlüğü

sayılı.
0258 851537

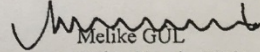
SAYI : 79545921/32.02.64
KONU: Isparta, Eğirdir, Gar Sahası
Plaktorna Alanı.

ANTALYA

31.01.2017 0411

DEVLET DEMİRYOLLARI İŞLETMESİ 7. BÖLGE MÜDÜRLÜĞÜNE
Aliçetinkaya Mah.Gar 03030 AFYONKARAHİSAR

Özü yukarıda belirtilen konu hakkında Antalya Kültür Varlıklarını Koruma Bölge Kurulu tarafından alınan 24.01.2017 gün ve 5779 sayılı kurul kararı ekte gönderilmektedir.
Bilgilerinizi ve gereğini arz ve rica ederim.


Melike GÖL
Bölge Kurulu Müdür V.

EK:

- 1- Karar
- 2- Teknik Rapor

DAĞITIM:

Gereği için:

- Isparta Valiliği
(İl Kültür ve Turizm Md.)
- Eğirdir Bld.Bşk.
- Devlet Demiryolları İşletmesi 7. Bölge Müdürlüğü
Aliçetinkaya Mah.Gar 03030 AFYONKARAHİSAR

Bilgi için:

- Kültür Varlıkları ve Müzeler Gn.Md.
(Kurullar Dai.Başk.) (Ek 2 konmadı)
- Eğirdir Kaymakamlığı
(Ek 2 konmadı)

Kılıçaslan Mahallesi Zafer Sokak No:6 Kaleiçi / ANTALYA
Tel: 0 (242) 247 87 61-243 21 60 Faks: 0 (242) 248 35 33
e-posta:antalyakurul@kulturturizm.gov.tr

Ayrıntılı Bilgi İçin İrtibat: U.OĞUZ Mimar

Elektronik Ağ: www.kulturturizm.gov.tr

APPENDIX B

Title deeds of Eđirdir Train Station Complex

İli	Isparta		Türkiye Cumhuriyeti TAPU SENEDİ				FOTOĞRAF		
İlçesi	Eğirdir								
Bucacı									
Mahallesi	Yazla								
Köyü									
Sokağı									
Mevkii	İstasyon								
Verginin Genel No.	G. Safi iradı	Vergi Kıymeti	Satış Bedeli	Pafta No.	Ada No.	Parsel No.	Yüz Ölçümü		
Özel No. Eski Yeni				1911a 26-32	239	1	He.	M ²	Dm ²
							(41)	5912	-
GAYRİMENKULÜN	Vasfı	İçerisinde 5 Adet Kargir İşletme Binası ve Lojmanı olan Tarla							
	Sınırı	Paftasında							
	İktisabı	Kadastro							
		BEYANLAR : İmar düzenlenmesine alınmıştır.21.6.1988 Yev:527							
Sahibi	T.C.D.D. YOLLARI İŞLETMESİ GENEL MÜDÜRLÜĞÜ								
GELDİSİ		Yevmiye No.	Cilt No.	Sahife No.	Sıra No.	Tarihi	GİTTİSİ		
Cilt No.		-	3	291	-	11.2.1986	Cilt No.		
Sayfa No.		19.9.1988 Siciline uygundur ASLI GİBİDİR MEVZU: 5.20100						Sayfa No.	
Sıra No.		NOT: Mülkiyetin gayri ayni haklar ile serhler için tapu kütüğüne müracaat edilmelidir.						Sıra No.	
Tarihi								Tarihi	

NOT: Arka sayfada yazılacak yazıların çerçeve dışına çıkmaması gerekir.

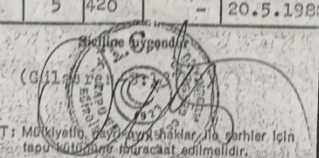
Ambar Stok No.: 129

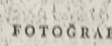
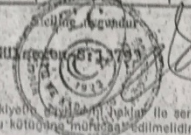
Askeriye
Kampanya

PARSEL BİLGİ KARTI

İl	İzmir							FOTOĞRAF		
İlçe	Yıldırım									
Bucak	-									
Mahalle	Yazın									
Köy	-									
Sokak	-									
Mevki	Kampanya	Türkiye Cumhuriyeti						TAPU SENEDİ		
Verginin		G. Safi	Vergi	Şatıs	Pafta	Ada	Parsel	Yüz Ölçümü		
Genel No.	Özel No.	İradı	Kıymeti	Bedeli	No.	No.	No.	He.	M ²	Dm ²
				-	19-20	239	2	6-	9619	-
Vasfı		Arsa								
Sınırı		Plânında								
İktisabı		Mümen								
Sahibi		T.C. Devlet Demiryolları İşletmesi Genel Müdürlüğü								
GELDISİ			Yevmiye No.	Cilt No.	Sahife No.	Sıra No.	Tarihi	GİTTİSİ		
Cilt No.			461	3	292	-	20.5.1988	Cilt No.		
Sayfa No.								Sayfa No.		
Sıra No.								Sıra No.		
Tarihi								Tarihi		
NOT: Mülkiyetin devri için tapu kütüphanesindeki tapu senedi ile beraber için tapu kütüphanesindeki tapu senedi esastir.										

ARABİ SİGORTA NO: 1129

İl		İlçe		Bucak		Mahalle		Köy		Sokak		Mevki		Müluk		FOTOĞRAF	
Türkiye Cumhuriyeti TAPU SENEDİ																	
Verginin		G. Safi		Vergi		Şatıs		Pafta		Ada		Parsel		Yüz Ölçümü			
Genel No		Özel No		İradı		Bedeli		No		No		No		He.		M ²	
		Eski Yeni				yeni		20		277		1		-		(170)	
GAYRİMENKULÜN		Vasfı Kargir su deposu ve arsası.															
		Sınırı Planında.															
		İktisabı Eski Asliye Hukuk Mahkemesinin 4.11.1987 tarih ve 1986/33 esas-1987/68 sayılı kararı uyarınca yeniden tescil edilmiştir....															
		Sahibi E.C.D.D. İşletmesi Genel Müdürlüğü															
GELDİSİ		Yevmiye No.		Cilt No.		Sahife No.		Sıra No.		Tarih		GİTTİSİ					
Cilt No.		460		5		420		-		20.5.1988		Cilt No.					
Sayfa No.												Sayfa No.					
Sıra No.												Sıra No.					
Tarih												Tarih					
 <p>NOT: Mülkiyetle ilgili aykırılıklar ile vakıflar için tapu kütüphanesi buracatı edilmelidir.</p>																	
ASLI GÜBÜR																	
Ambar Sıra No. 129																	

İl		Tepavada		 <p style="text-align: center;">Türkiye Cumhuriyeti TAPU SENEDİ</p>							
İlçe		Etiler									
Bucak											
Mahallesi		Yasla									
Köyü											
Sokağı											
Mevki		Kırsal									
Verginin		G. Safi		Vergi	Şatıs	Pafta	Ada	Parsel	Yüz Ölçümü		
Genel No.	Özel No.	İradı	Kıymeti	Bedeli	No.	No.	No.	No.	He.	M ²	Dm ²
				-	20	277	2	-		(1523)	-
Yaslı		Kırık, Su deposu ve Araç.									
Sınırı		Paftasında.									
İktisabı		BİRİRDİR Asliye Hukuk Mahkemesinin 18.3.1987 Tarih ve 1986/34 Esas 1987/69 Kararı sayılı ilama uyarınca teşkil edilmiştir.									
Sahibi		T.C.D.D.İşletmeni Genel Müdürlüğü									
GELDİSİ		Yevmiye No.	Çift No.	Sahifs No.	Sıra No.	Tarih	GİTTİSİ				
Çift No.		459	5	421		20.5.1988	Çift No.				
Sayfa No.							Sayfa No.				
Sıra No.							Sıra No.				
Tarih							Tarih				
NOT: Mülkiyetin tapu senedi ile kanıtlanabilir. Tapu kütüğüne tescil edilmiştir.											
ASLİ GİTTİR											

Ambar Stok No: 1 129

APPENDIX C

Documents About Cadastral Status of Eđirdir Train Station Complex Area

- City Block 239 / Parcels 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 15, 16, 17, 18, 19
- City Block 277 / Parcels 1, 2
- City Block 415 / Parcels 1, 2, 3, 4, 5, 6, 7, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
il	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	2	69,619.00	Arsa	Kumlu	19-20	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:21:24		




239 ada 3 parsel hareket listesi





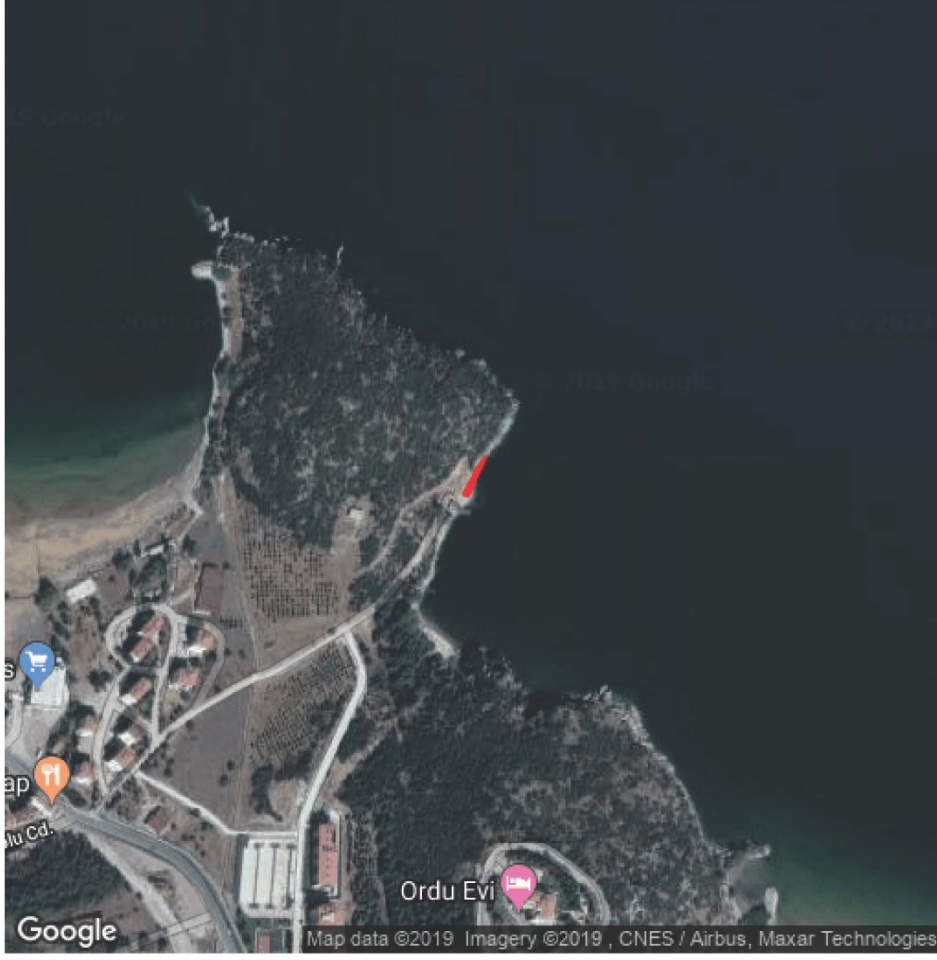
Tablo



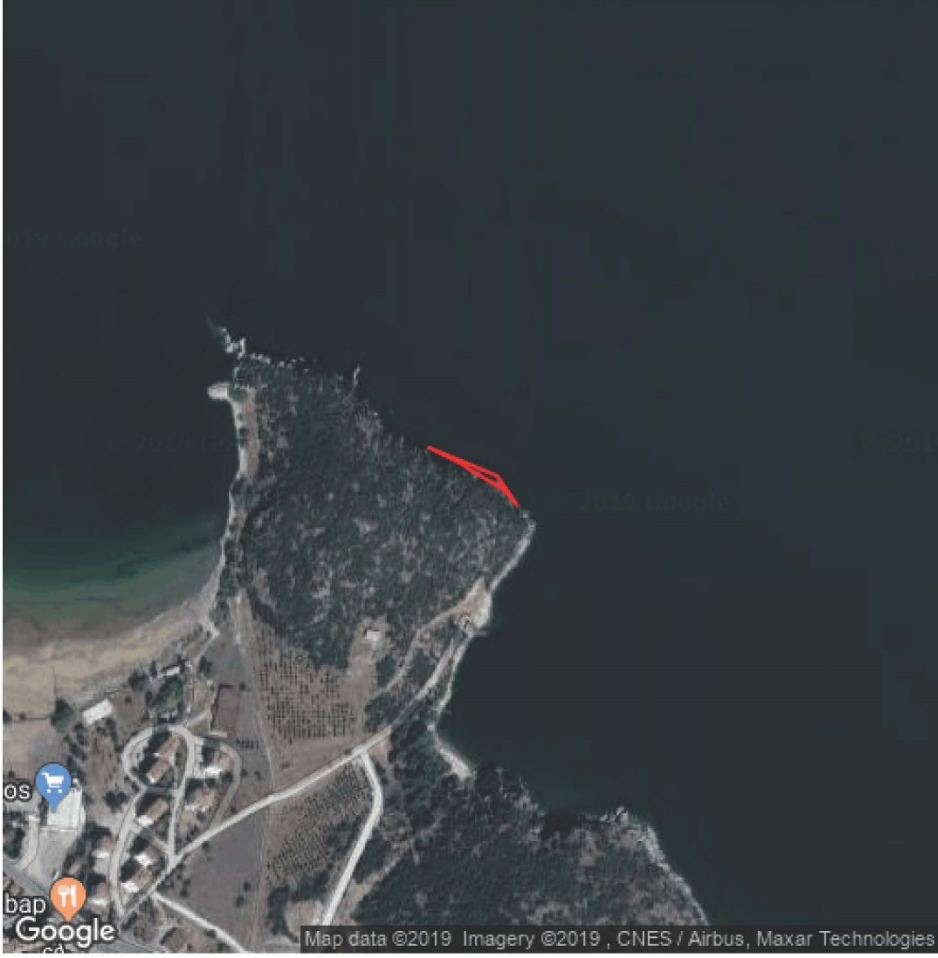
Grafik



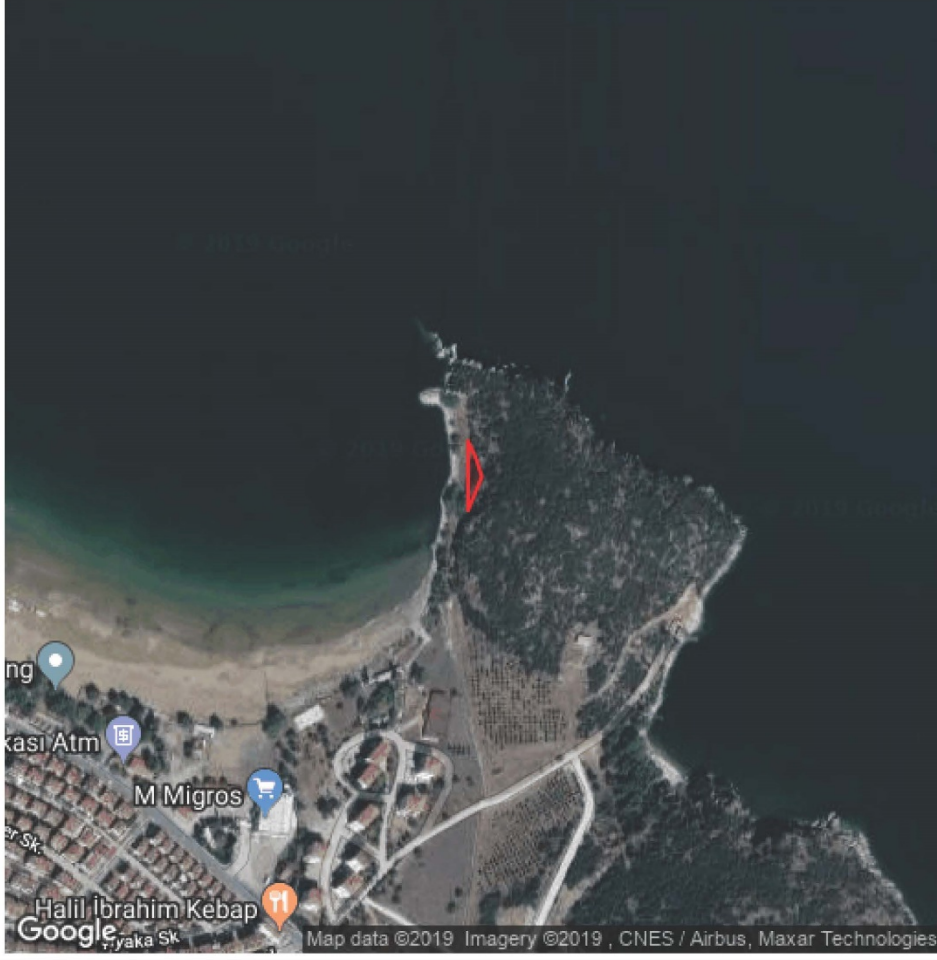
Açıklama	Durum	Taşınmaz No	Parsel
▼ 19/02/2013 tarihli ifraz işlemi (tsm)	Pasif	47738050	239/3
	Aktif	47738050	239/12
	Aktif	47738050	239/13
	Aktif	47738050	239/14
	Aktif	47738050	239/15

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	4	42,540.00	Arsa	İstasyon	18-19-21	
						
BİLGİLENDİRME - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:33:43		

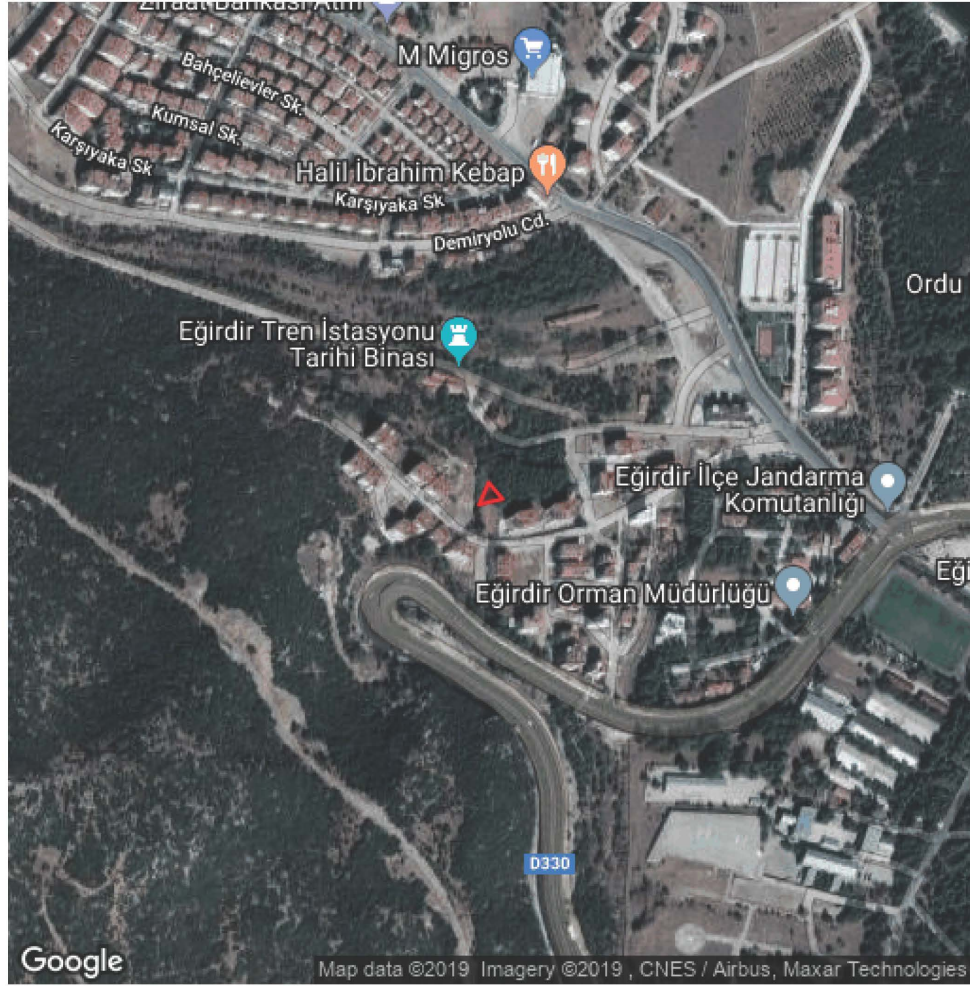
		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	5	600.00	Arsa	Kumluk	19-20	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:38:07		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	6	85.00	Arsa	Kumlu	19-20	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		6 Aralık 2019 Cuma 01:25:07		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	7	392.00	Arsa	Kumluk	19-20	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:42:09		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	8	524.00	Arsa	Kumluk	19-20	
						
BİLGİLENDİRME - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:46:22		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ			
İl	İlçe				Mahalle
Isparta	Eğirdir				Yazla
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta
239	9	283.82	Arsa	İstasyon	19



BİLGİLENDİRME

- Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir.
- Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır.
- Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir.
- Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.

127.0.0.1

MEGSİS (Mekansal Gayrimenkul Sistemi)

5 Aralık 2019 Perşembe 22:51:04

239 ada 10 parsel hareket listesi



Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 25/06/2019 tarihli ifraz işlemi (tsm)	Pasif	47757986	239/10
	Aktif	47757986	239/16
	Aktif	47757986	239/17
	Aktif	47757986	239/18
	Aktif	47757986	239/19
	Aktif	47757986	239/20

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
		PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	11	359,827.67	İçerisinde Beş Adet Kargir İşletme Binası Ve Lojmanı Olan Tarla	İstasyon	19-26-32	



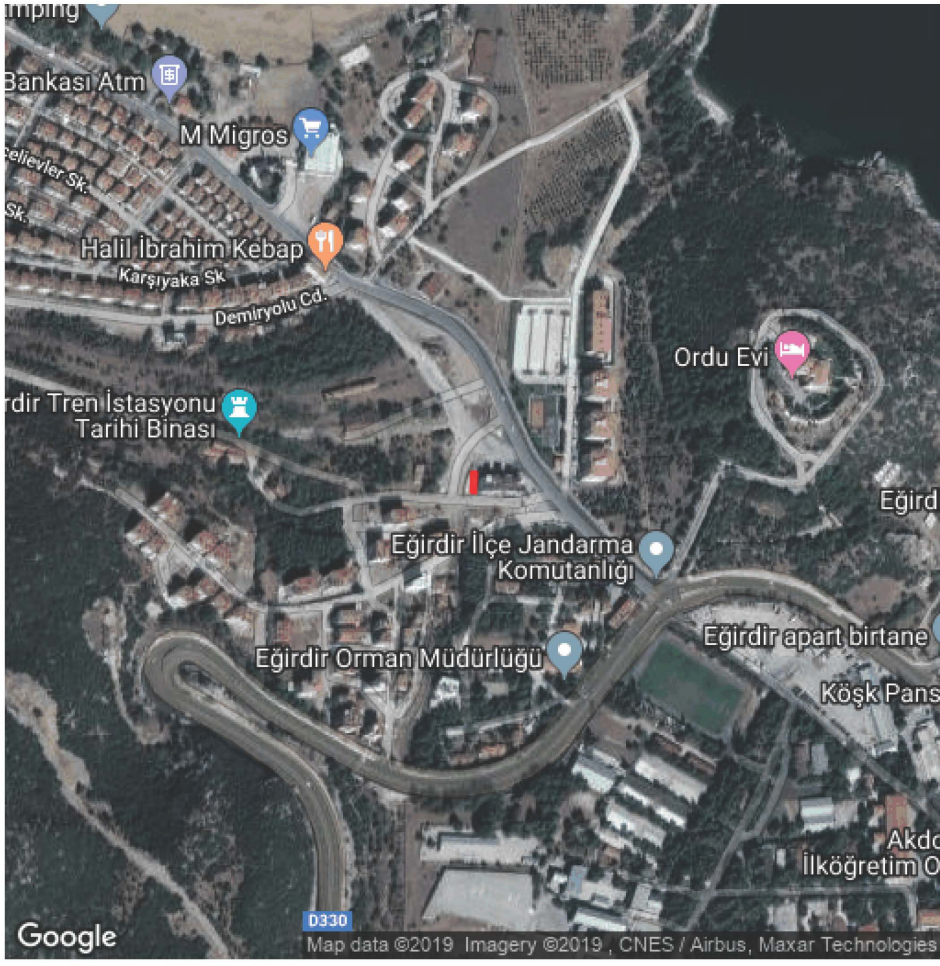

BİLGİLENDİRME



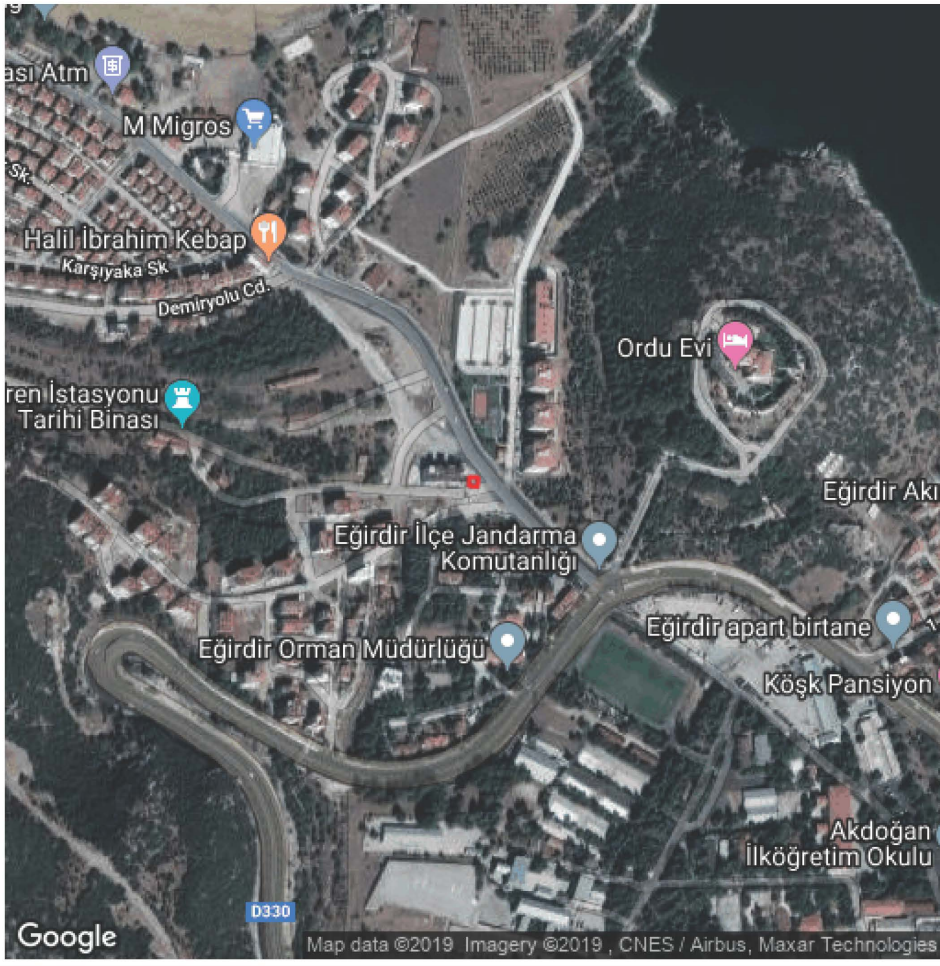
- Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir.
- Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır.
- Amacı dışında kullanılması halinde doğacak her türlü hukuki cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir.
- Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.



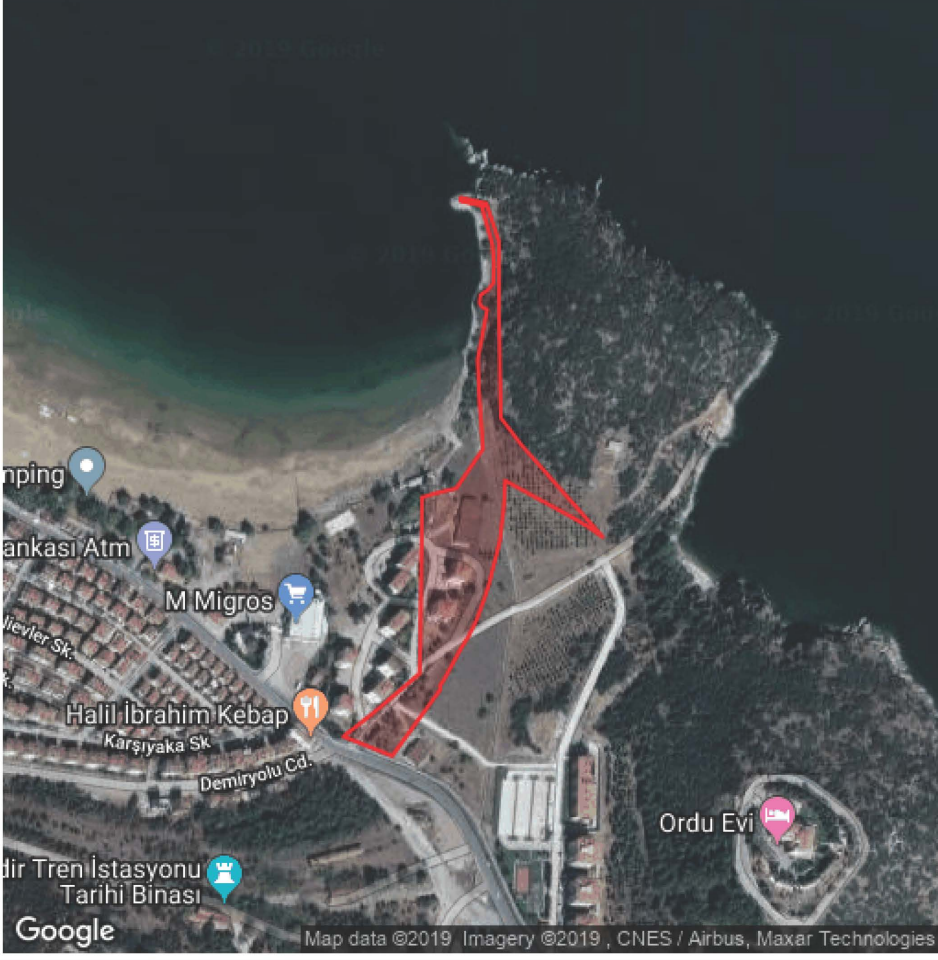
127.0.0.1



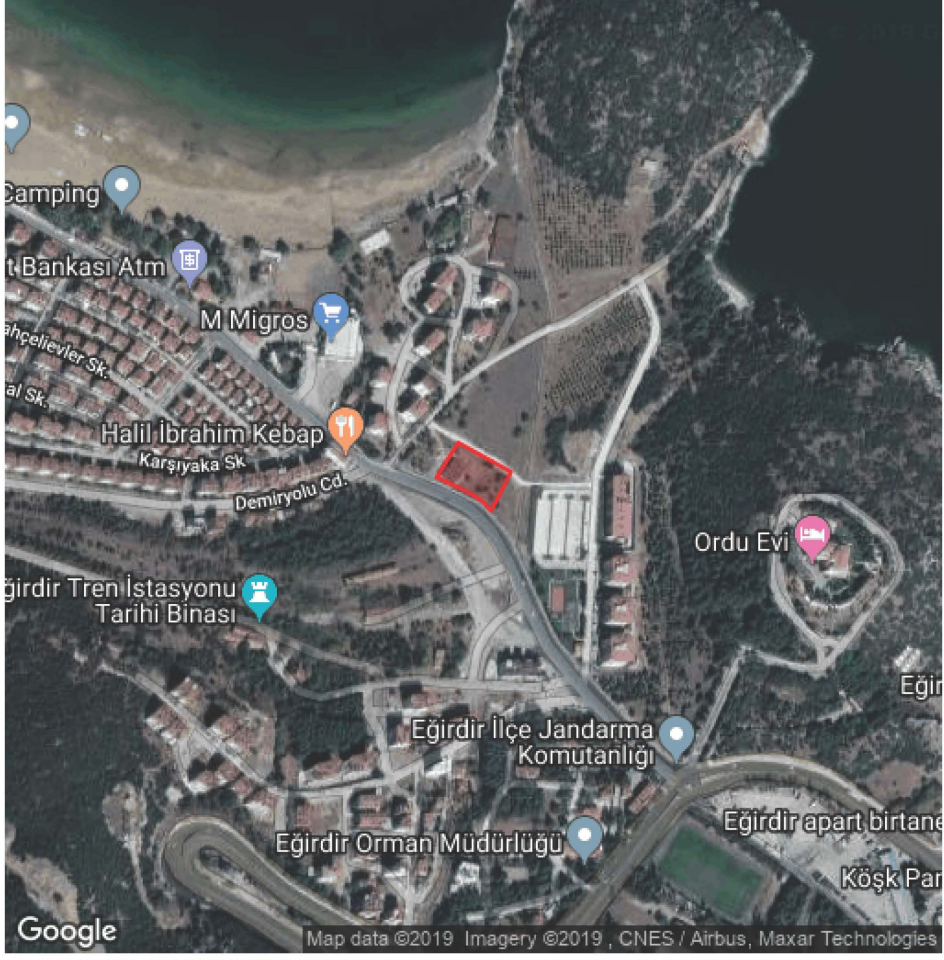
MEGSİS (Mekansal Gayrimenkul Sistemi)

5 Aralık 2019 Perşembe 22:59:56



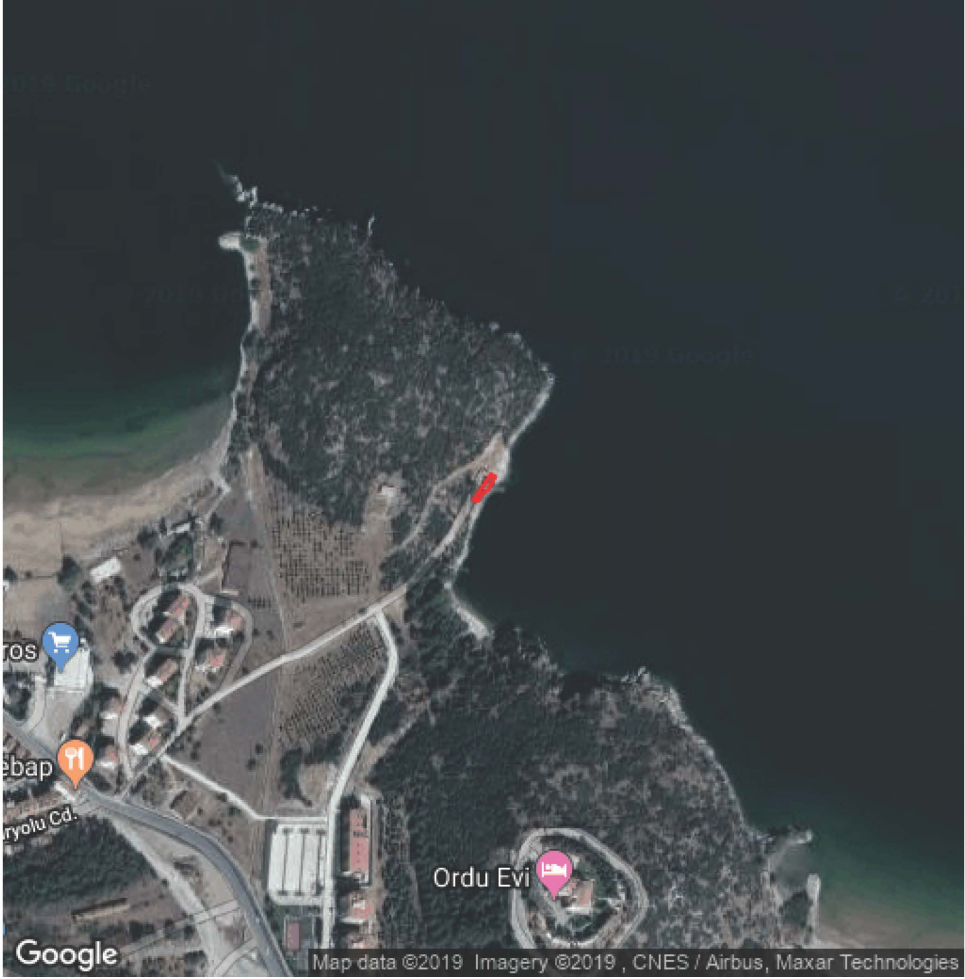
	TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
	PARSEL SORGULAMA BİLGİLERİ				
İl	İlçe				Mahalle
Isparta	Eğirdir				Yazla
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta
239	12	99.07	Arsa	İstasyon	19
					
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 					
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:59:19	



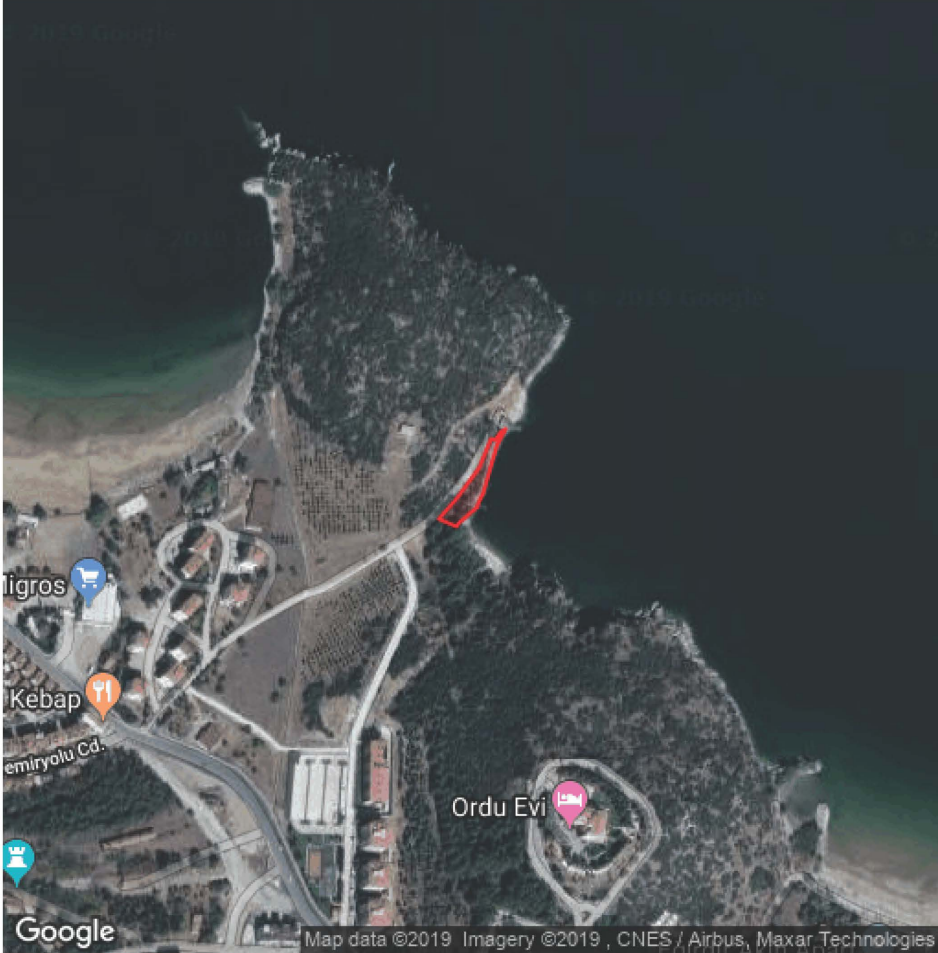
		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	15	87.56	Arsa	İstasyon	19	
						
BİLGİLENDİRME - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 23:02:18		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	16	24,204.67	Arsa	İstasyon	19-26-32	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 23:06:00		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	17	2,672.93	Arsa	İstasyon	19-26-32	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 23:07:10		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
239	18	19,684.66	Arsa	İstasyon	19-26-32	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 23:08:35		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ			
İl	İlçe				Mahalle
Isparta	Eğirdir				Yazla
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta
277	1	170.00	Kargir Su Deposu Ve Arsası	Kumluk	20
					
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 					
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:11:34	

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
il	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
277	2	1,523.00	Kargir Su Deposu Ve Arsası	Kumluk	20	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 22:12:28		

415 ada 1 parsel hareket listesi



Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738492	415/1
	Aktif	47738492	415/19

415 ada 2 parsel hareket listesi



Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738493	415/2
	Aktif	47738493	415/19



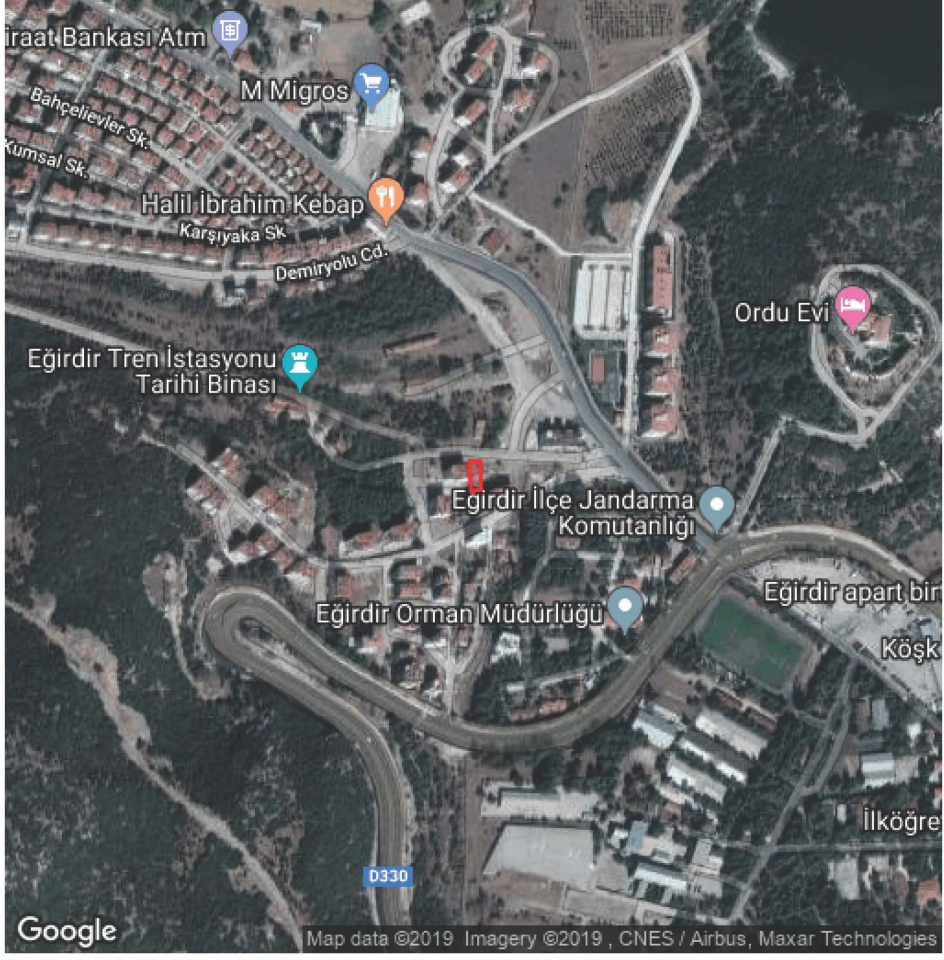
415 ada 3 parsel hareket listesi



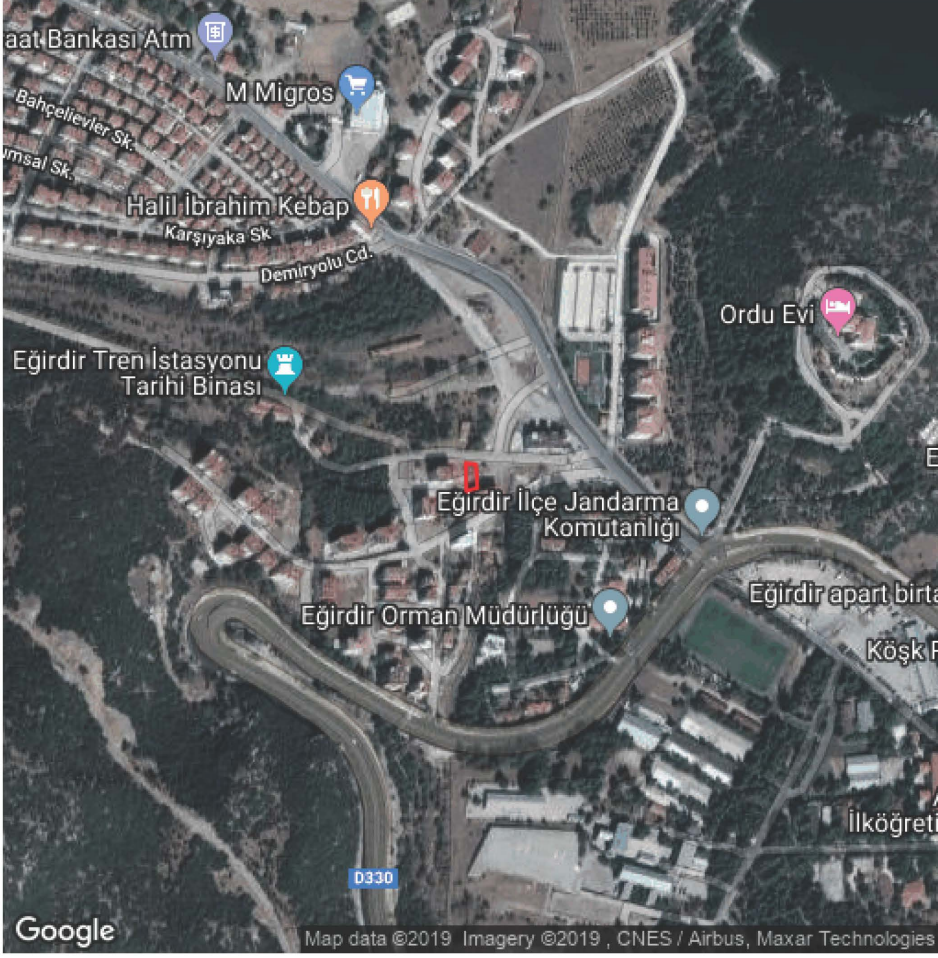


Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738494	415/3
	Aktif	47738494	415/19

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
		PARSEL SORGULAMA BİLGİLERİ				
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	4	322.87	Arsa	İstasyon	18-22	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 21:15:58		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
		PARSEL SORGULAMA BİLGİLERİ				
İl	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	5	291.38	Arsa	İstasyon	18-22	
						
BİLGİLENDİRME - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 21:13:27		

415 ada 6 parsel hareket listesi



Tablo Grafik



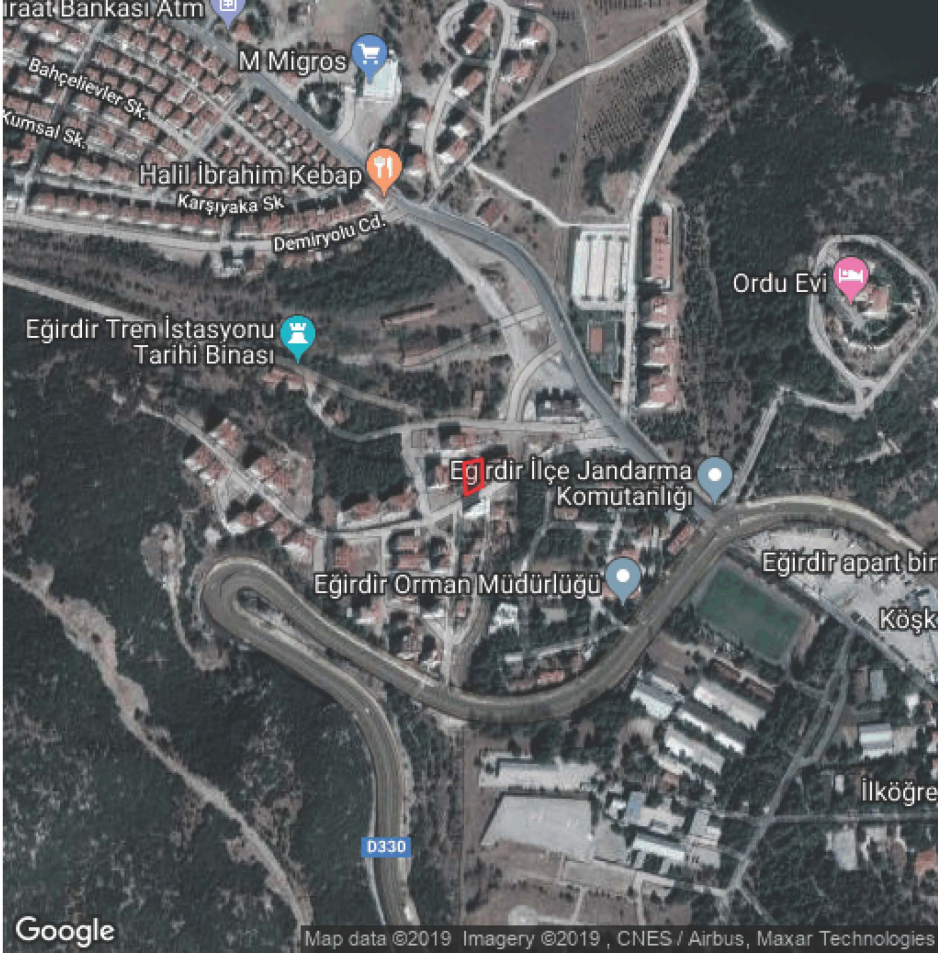
Açıklama	Durum	Taşınmaz No	Parsel
▼ 16/04/2014 tarihli tevhit işlemi (tsm)	Pasif	47738497	415/6
	Aktif	47738497	415/18



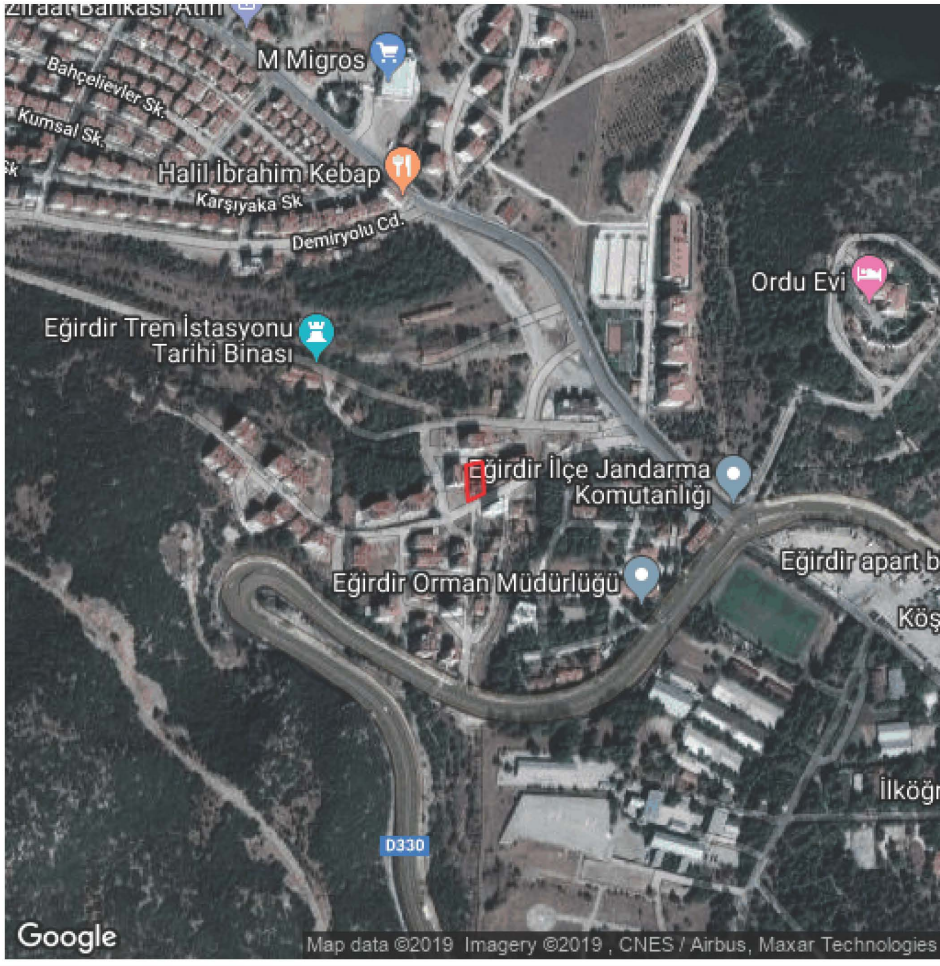
415 ada 7 parsel hareket listesi



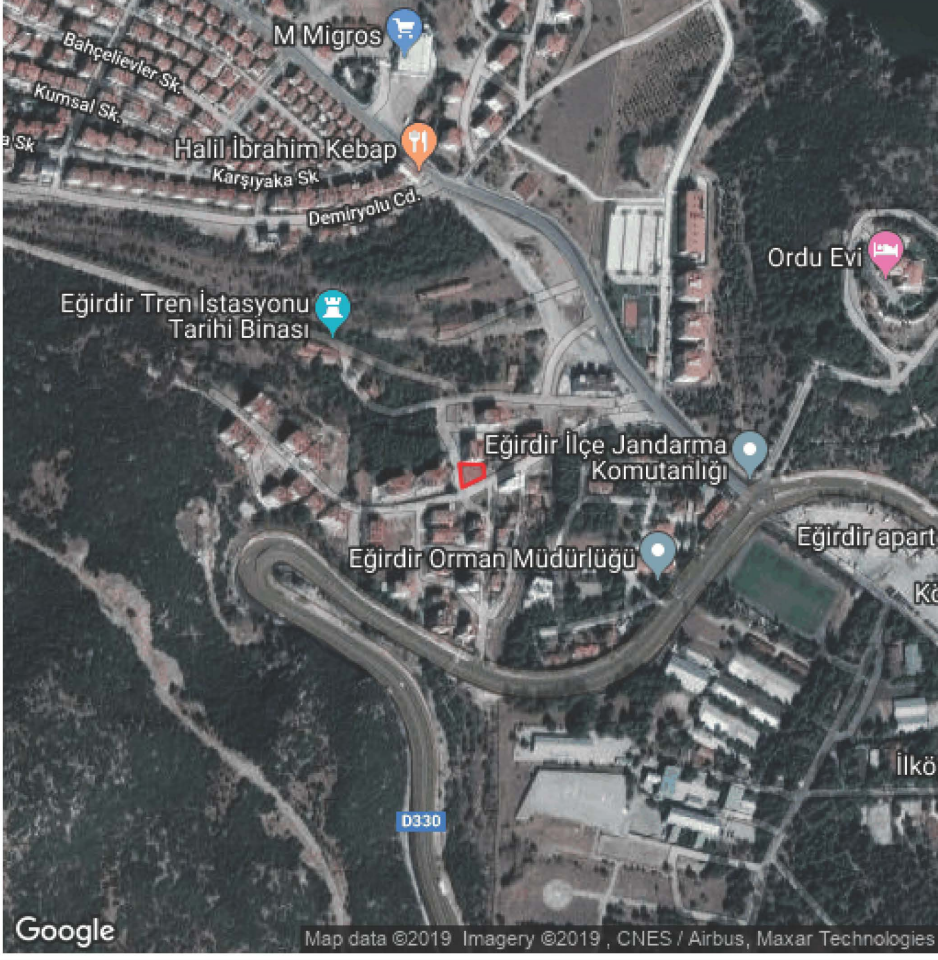


Tablo Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 16/04/2014 tarihli tevhit işlemi (tsm)	Pasif	47738498	415/7
	Aktif	47738498	415/18

	TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ					
	PARSEL SORGULAMA BİLGİLERİ					
İl	İlçe					Mahalle
Isparta	Eğirdir					Yazla
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	10	543.77	Bahçeli Ahşap Ev	İstasyon	18-22	
						
BİLGİLENDİRME						
<ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)			5 Aralık 2019 Perşembe 21:18:37	

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	11	580.20	Ahşap Ev	İstasyon	18-22	
						
BİLGİLENDİRME - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 21:20:45		

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
il	İlçe				Mahalle	
Isparta	Eğirdir				Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	12	450.12	Arsa	İstasyon	18-22	
 <p>Map data ©2019 Imagery ©2019 , CNES / Airbus, Maxar Technologies</p>						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 21:21:46		

415 ada 13 parsel hareket listesi



Tablo Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738502	415/13
	Aktif	47738502	415/19

415 ada 14 parsel hareket listesi



Tablo Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738503	415/14
	Aktif	47738503	415/19

415 ada 15 parsel hareket listesi



Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738504	415/15
	Aktif	47738504	415/19



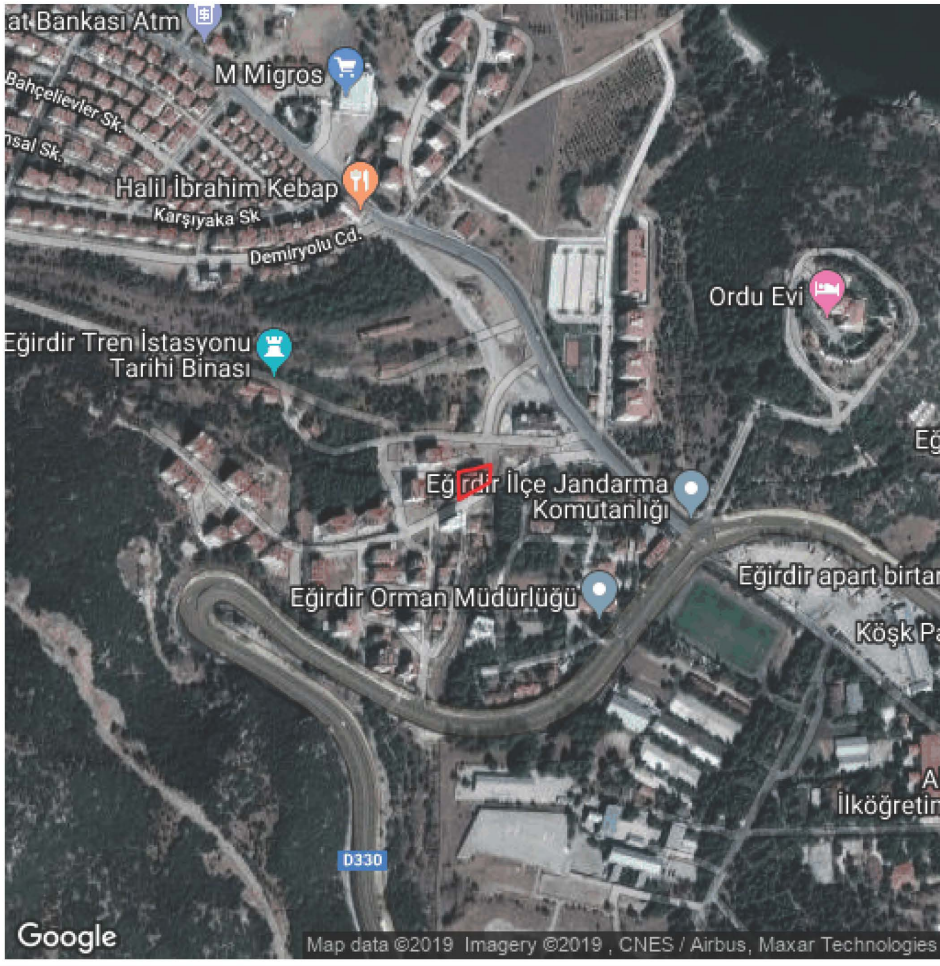
415 ada 16 parsel hareket listesi



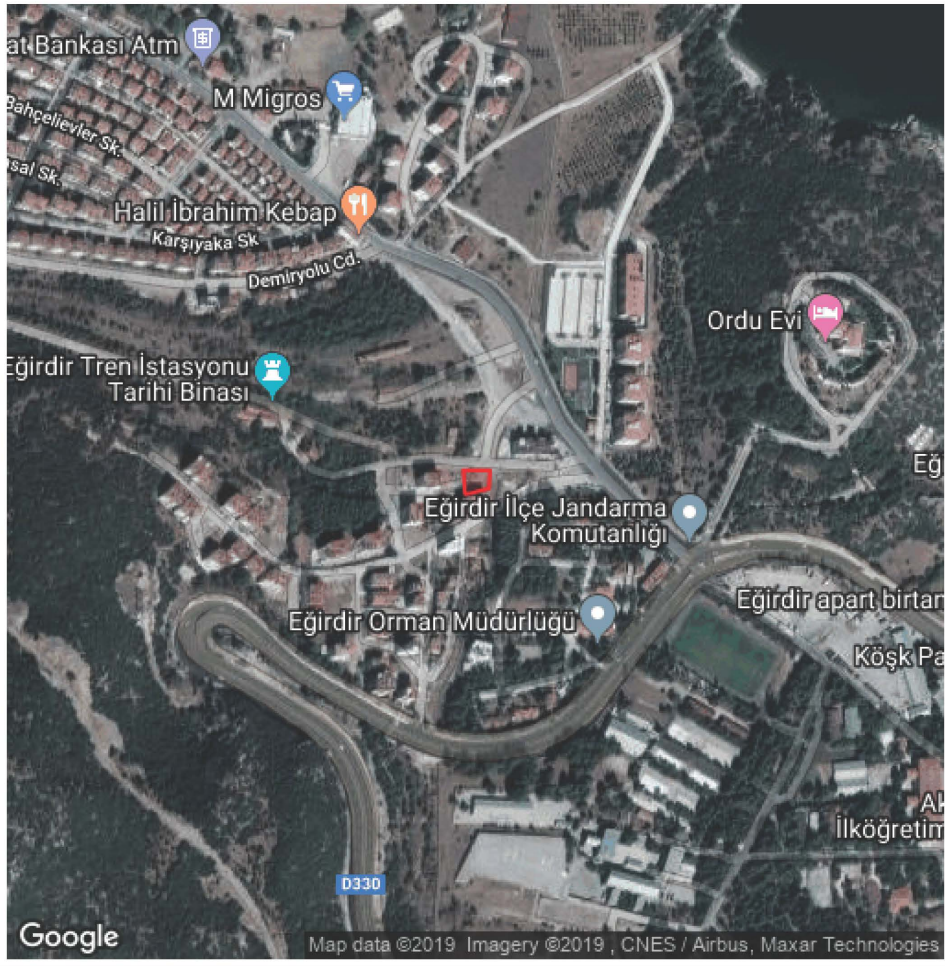


Tablo

Grafik

Açıklama	Durum	Taşınmaz No	Parsel
▼ 02/03/2015 tarihli tevhit işlemi (tsm)	Pasif	47738505	415/16
	Aktif	47738505	415/19

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ				
PARSEL SORGULAMA BİLGİLERİ						
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	17	748.20	Arsa	İstasyon	19	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)		5 Aralık 2019 Perşembe 21:26:22		

	TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ					
	PARSEL SORGULAMA BİLGİLERİ					
il	İlçe					Mahalle
Isparta	Eğirdir					Yazla
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	18	567.59	Arsa	İstasyon	19	
						
BİLGİLENDİRME <ul style="list-style-type: none"> - Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir. - Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır. - Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir. - Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz. 						
127.0.0.1		MEGSİS (Mekansal Gayrimenkul Sistemi)			5 Aralık 2019 Perşembe 21:27:29	

		TAPU VE KADASTRO GENEL MÜDÜRLÜĞÜ PARSEL SORGULAMA BİLGİLERİ				
İl		İlçe			Mahalle	
Isparta		Eğirdir			Yazla	
Ada	Parsel	Tapu Alanı (m2)	Nitelik	Mevki	Pafta	
415	19	2,255.54	A Blok 6 Katlı B Blok 5 Katlı Betonarme Mesken Ve Arsası	İstasyon	18-22	


BİLGİLENDİRME

- Tüm hakları Tapu Ve Kadastro Genel Müdürlüğüne Aittir.
- Sorgulama sonucu sunulan veriler bilgilendirme amaçlı olup, ticari amaçla kullanılması yasaktır.
- Amacı dışında kullanılması halinde doğacak her türlü hukuki, cezai ve mali sorumluluk uygunsuz kullanan kişilere aittir.
- Görüntülenen kayıtlar ile elinizde bulunan bilgiler arasında uyumsuzluk olması halinde ilgili tapu ve/veya kadastro müdürlüğüne müracaat ediniz.

127.0.0.1

MEGSİS (Mekansal Gayrimenkul Sistemi)

5 Aralık 2019 Perşembe 21:28:46